



AGENDA

VALLEJO PLANNING COMMISSION SPECIAL MEETING – 7:00 P.M

Kent Peterman, Chairperson
Landis Graden
Dennis Klimisch
Marvin Kinney
Roberto Cortez
Anthony Adams
Karimah Karah

City Hall
555 Santa Clara Street
Vallejo, CA 94590

APRIL 29, 2013

This AGENDA contains a brief general description of each item to be considered. The posting of the recommended actions does not indicate what action may be taken. If comments come to the Planning Commission without prior notice and are not listed on the AGENDA, no specific answers or response should be expected at this meeting per State law.

Agenda Items: Those wishing to address the Commission on a scheduled agenda item should fill out a speaker card and give it to the Secretary. Speaker time limits for scheduled agenda items are five minutes for designated spokespersons for a group and three minutes for individuals.

Notice of Availability of Public Records: All public records relating to an open session item, which are not exempt from disclosure pursuant to the Public Records Act, that are distributed to a majority of the Planning Commission will be available for public inspection at City Hall, 555 Santa Clara St., 2nd Floor, or the Vallejo Public Library, 505 Santa Clara St. at the same time that the public records are distributed or made available to the Planning Commission. Such documents may also be available on the City of Vallejo website at www.ci.vallejo.ca.us subject to staff's ability to post the documents prior to the meeting.

Disclosure Requirements: Government Code Section 84308 (d) sets forth disclosure requirements which apply to persons who actively support or oppose projects in which they have a "financial interest", as that term is defined by the Political Reform Act of 1974. If you fall within that category, and if you (or your agent) have made a contribution of \$250 or more to any commissioner within the last twelve months to be used in a federal, state or local election, you must disclose the fact of that contribution in a statement to the Commission.

Appeal Rights: The applicant or any party adversely affected by the decision of the Planning Commission may, within ten days after the rendition of the decision of the Planning Commission, appeal in writing to the City Council by filing a written appeal with the City Clerk. Such written appeal shall state the reason or reasons for the appeal and why the applicant believes he or she is adversely affected by the decision of the Planning Commission. Such appeal shall not be timely filed unless it is actually received by the City Clerk or designee no later than the close of business on the tenth calendar day after the rendition of the decision of the Planning Commission. If such date falls on a weekend or City holiday, then the deadline shall be extended until the next regular business day.

Notice of the appeal, including the date and time of the City Council's consideration of the appeal, shall be sent by the City Clerk to all property owners within two hundred or five hundred feet of the project boundary, whichever was the original notification boundary.

The Council may affirm, reverse or modify any decision of the Planning Commission which is appealed. The Council may summarily reject any appeal upon determination that the appellant is not adversely affected by a decision under appeal.

If any party challenges the Planning Commission's actions on any of the following items, they may be limited to raising only those issues they or someone else raised at the public hearing described in this agenda or in written correspondence delivered to the Secretary of the Planning Commission.

	Vallejo City Council Chambers is ADA compliant. Devices for the hearing impaired are available from the City Clerk. Requests for disability related modifications or accommodations, aids or services may be made by a person with a disability to the City Clerk's office no less than 72 hours prior to the meeting as required by Section 202 of the Americans with Disabilities Act of 1990 and the federal rules and regulations adopted in implementation thereof.
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If you have any questions regarding any of the following agenda items, please call the assigned planner or project manager at (707) 648-4326.

1. CALL TO ORDER

2. PLEDGE OF ALLEGIANCE

3. ROLL CALL

4. APPROVAL OF THE MINUTES

None

5. WRITTEN COMMUNICATIONS

6. REPORT OF THE SECRETARY

A. Upcoming Meetings:

May 6, 2013 – Plan Amendments regarding beer and wine manufacturing facilities with on-site tasting rooms and brewpubs (Tentative); Code Text Amendment regarding Fast Food establishments (Tentative)

7. CITY ATTORNEY REPORT

8. REPORT OF THE PRESIDING OFFICER AND MEMBERS OF THE PLANNING COMMISSION AND LIAISON REPORTS

A. Report of the Presiding Officer and Members of the Planning Commission

B. Council Liaison to Planning Commission

C. Planning Commission Liaison to City Council

9. COMMUNITY FORUM

Anyone wishing to address the Commission on any matter for which another opportunity to speak is not provided on the agenda, and which is within the jurisdiction of the Commission to resolve, is requested to submit a completed speaker card to the Secretary. When called upon, each speaker should step to the podium, state his/her name and address for the record. The conduct of the community forum shall be limited to a maximum of fifteen (15) minutes, with each speaker limited to three minutes pursuant to Vallejo Municipal Code Section 2.20.300. The Commission may take information but may not take action on any item not on the agenda.

10. CONSENT CALENDAR AND APPROVAL OF THE AGENDA

Consent Calendar items appear below, with the Secretary's or City Attorney's designation as such. Members of the public wishing to address the Commission on Consent Calendar items are asked to address the Secretary and submit a completed speaker card prior to the approval of the agenda. Such requests shall be granted, and items will be addressed in the order in which they appear in the agenda. After making any changes to the agenda, the agenda shall be approved.

All matters are approved under one motion unless requested to be removed for discussion by a commissioner or any member of the public.

11. PUBLIC HEARINGS

- A. Project Title: **Specific Plan (SPA) #10-0001
General Plan Amendment (GPA) #10-0001
Zoning Map Amendment (ZMA) #10-0001
Code Text Amendment (CTA) #10-0004
Development Agreement (DA) #13-0001
Tentative Map (TM) #13-0001**
- Applicant: County of Solano, Solano County Fair Association and City of Vallejo
- Location: Solano County Fairgrounds, 900 Fairgrounds Drive
- Project Description: The County of Solano, in collaboration with the Solano County Fair Association and the City of Vallejo, has developed the Solano360 Specific Plan, a long-term plan for redevelopment of the Solano County Fairgrounds. The Specific Plan establishes land uses and a comprehensive set of infrastructure and development standards for the property while retaining the ability to respond to market conditions and opportunities for private development. The proposed General Plan and Zoning Amendments are required for plan consistency, the Development Agreement provides assurance to the County that the project may proceed in accordance with existing policies, rules and regulations subject to certain conditions of approval, and the proposed Tentative Map provides design improvements of the land to facilitate leasing and financing of future development projects.
- Environmental Determination: Pursuant to the California Environmental Quality Act (CEQA), an Environmental Impact Report (EIR) for the project was prepared, circulated for public review and certified by the Solano County Board of Supervisors as the lead agency on February 26, 2013.

Staff Recommendation:

Adopt three Resolutions per the following:

- (1) Approve the Solano360 Specific Plan, General Plan Amendment and Zoning Amendments and recommend forwarding to City Council for adoption;
- (2) Approve the Development Agreement between the County of Solano and City of Vallejo for the Solano360 Project and recommend forwarding to City Council for execution; and
- (3) Approve the Solano360 Tentative Map, subject to the conditions of approval.

Project Manager:

Michelle Hightower, (707) 648-4506,
mhightower@ci.vallejo.ca.us

12. ADJOURNMENT

Staff Reports



PLANNING COMMISSION STAFF REPORT

DATE OF MEETING: April 29, 2013 **Item No. 11-A**

PROJECT NUMBERS: Specific Plan (SPA) #10-0001
General Plan Amendment (GPA) #10-0001
Zoning Map Amendment (ZMA) #10-0001
Code Text Amendment (CTA) #10-0004
Development Agreement (DA) #13-0001
Tentative Map (TM) #13-0001

PROJECT LOCATION: 900 Fairgrounds Drive, Vallejo, CA 94589
APN 0052-240-550, 560

PROJECT

DESCRIPTION:

The County of Solano, in collaboration with the Solano County Fair Association and the City of Vallejo, has developed the Solano360 Specific Plan, a long-term plan for redevelopment of the Solano County Fairgrounds. The Specific Plan establishes land uses and a comprehensive set of infrastructure and development standards for the property while retaining the ability to respond to market conditions and opportunities for private development. The proposed General Plan and Zoning Amendments are required for plan consistency, the Development Agreement provides assurance to the County that the project may proceed in accordance with existing policies, rules and regulations subject to certain conditions of approval, and the proposed Tentative Map provides design improvements of the land to facilitate leasing and financing of future development projects.

PROPOSED

ENVIRONMENTAL

DETERMINATION:

Pursuant to the California Environmental Quality Act (CEQA), an Environmental Impact Report (EIR) for the project was prepared, circulated for public review and certified by the Solano County Board of Supervisors as the lead agency on February 26, 2013.

RECOMMENDATION:

Adopt three Resolutions per the following:

- (1) Approve the Solano360 Specific Plan, General Plan Amendment and Zoning Amendments and recommend forwarding to City Council for adoption;
- (2) Approve the Development Agreement between the County of Solano and City of Vallejo for the Solano360 Project and recommend forwarding to City Council for execution; and

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- (3) Approve the Solano360 Tentative Map, subject to the conditions of approval.

PROJECT DATA

SUMMARY:

Name of Applicant:	County of Solano, Solano County Fair Association and City of Vallejo
General Plan Designation:	Existing – Open Space-Community Park Proposed - Commercial Recreation
Zoning Designation:	Existing - Public and Quasi-Public Facilities (PF) Proposed – Mixed Use Planned Development (MUPD)
Surrounding Land Use:	North: State Route/Highway 37 Freeway South: Residential (Mobile Home Park) East: Interstate 80 Freeway West: Commercial (Amusement Park) and Lake (Chabot)
Project Area:	149.1 Acres

PROJECT DESCRIPTION

Background

The Solano360 Specific Plan (“the Plan”) was developed as a joint effort by the County of Solano (“County”), City of Vallejo (“City”), and the Solano County Fair Association (“Fair Association”), with SWA Group serving as the lead consultant, to facilitate the revitalization of the Solano County Fairgrounds. The Fair Association has operated the Solano County Fair on the project site annually during mid-summer since 1949, and has expressed interest in upgrading the Fairgrounds to create “the Fair of the Future” to address the decline in fair attendance over the past several years and to increase revenue to support the Fair. As a result, the Fair Association, County and City engaged in a planning effort to guide the future use of the property, including activities that would take place throughout the year.

Phase I of the planning effort involved a visioning process which took place from Spring 2007 to September 2008. The culmination of the first phase was acceptance by the City Council and Board of Supervisors of the Solano360 Vision Report dated May 28, 2009. The Vision Report was based on the following Guiding Principles:

- Generate revenues for Solano County and City of Vallejo, create jobs and ensure long-term economic sustainability.
- Establish a unique place with an unmistakable identity that serves as a destination for visitors as well as a pedestrian-friendly community gathering place.

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- Explore a mix of complementary land uses, including retail, commercial, hospitality, recreational, residential, and family and youth oriented, educational and civic uses that seamlessly integrate with the “Fair of the Future”.
 - Explore increased physical connectivity and synergy with Six Flags Discovery Kingdom, downtown Vallejo, the waterfront and other existing commercial operations.
 - Provide pedestrian, bicycle, vehicular and transit facilities that foster access to, from and within the site.
 - Incorporate sustainable and green principles in all aspects of the development.

The Vision Report identified a concept plan which integrated a new Public Entertainment Zone with the Fair of the Future Zone on the Fairgrounds. The Public Entertainment Zone is envisioned to provide an active, multi-use district with a restored waterway, waterside trails, and a wide variety of restaurants, public art, shops, terraced seating and other attractions. Other private uses considered in the Vision Report included offices, hotel/hospitality, and retail. The Fair of the Future included the traditional annual County Fair, while also proposing year-round supporting uses such as a demonstration garden, a new exhibition hall and arena, a satellite wagering facility, multi-purpose fields, and other entertainment and retail activities.

The Vision Report led to Phase II of the planning process, which commenced in 2009. During Phase II, the Specific Plan and associated Environmental Impact Report (EIR), a Public Facilities Financing Plan (PFFP) and the Fiscal Impact Analysis (FIA) were prepared. County and City staff worked collaboratively on these documents and presented them to the Solano360 Committee, which is comprised of the Mayor and two City Council members, two County Supervisors, and three Directors from the Fair Association Board of Directors. The Solano360 Committee was established to provide oversight of the planning process during the visioning process and throughout the development of the Specific Plan. These processes have also benefited from extensive community outreach and participation, including workshops and meetings held by the Solano360 Committee that began in early 2009. More recently, a joint City Council/Planning Commission study session took place in January 2013, and a follow-up public meeting was held in February 2013 to specifically address the project’s financial analysis.

Project Location and Context

The Plan area encompasses approximately 149 acres of County-owned land located in the southwest corner of the Interstate 80/State Route 37 junction. The site is generally bounded by Fairgrounds Drive to the west, Coach Lane to the south, Interstate 80 to the east, and State Route 37 and Sage Street to the north. The property currently contains several buildings, parking lots, open space areas and other facilities associated with the Solano County Fairgrounds primarily within the northeast portion of the site. The area also includes an inactive equestrian racetrack facility adjacent to Interstate 80, a nine-hole public golf course located in the infield of the racetrack, and vacant stables at the south end of the site. The western portion of the property is largely unimproved and primarily used as a parking area for the Fair and as an overflow parking lot for the adjacent Six Flags Discovery Kingdom amusement park located on Fairgrounds Drive.

Specific Plan Contents

The Plan provides the planning policies and land use and development regulations for the Solano360 Project. As mentioned, the Plan builds upon the 2009 Vision Report and provides a blueprint to guide land use and infrastructure improvements, coordinate public investments, facilitate private investment, and support long-term, phased revitalization of the property over the next 25 years. It is expected that development will occur in three major phases with Phase 1 to develop over a 5-year period, and Phases 2 and 3 to develop over sequential 10-year periods. To address both the Fair of the Future improvements to be undertaken by the Fair Association and the County, public improvements throughout the Plan Area, and future private development on the remainder of the site, the Plan area is subdivided as "Public Purpose Areas" and "Private Purpose Areas" as shown in Figure 1 below and Figure 1.2 in the Plan document. The "Public Purpose Areas," which are to be used for Fair-related purposes, are exempt from City land use authority. The "Private Purpose Areas" are intended for future private development and will be subject to the City's land use regulations and procedures. Accordingly, this Staff report focuses on the Private Purpose Areas which will be subject to City regulatory authority.

The Plan serves two functions. It serves as a Specific Plan/Master Plan of the Private Purpose Areas for the City. As such, it is consistent with California State law (Government Code §65450-65457) and the City's Zoning Ordinance (Title 16, Chapter 16.114 of the Vallejo Municipal Code (VMC)), and provides flexible planning and design criteria for mixed-use private development on the Project site.

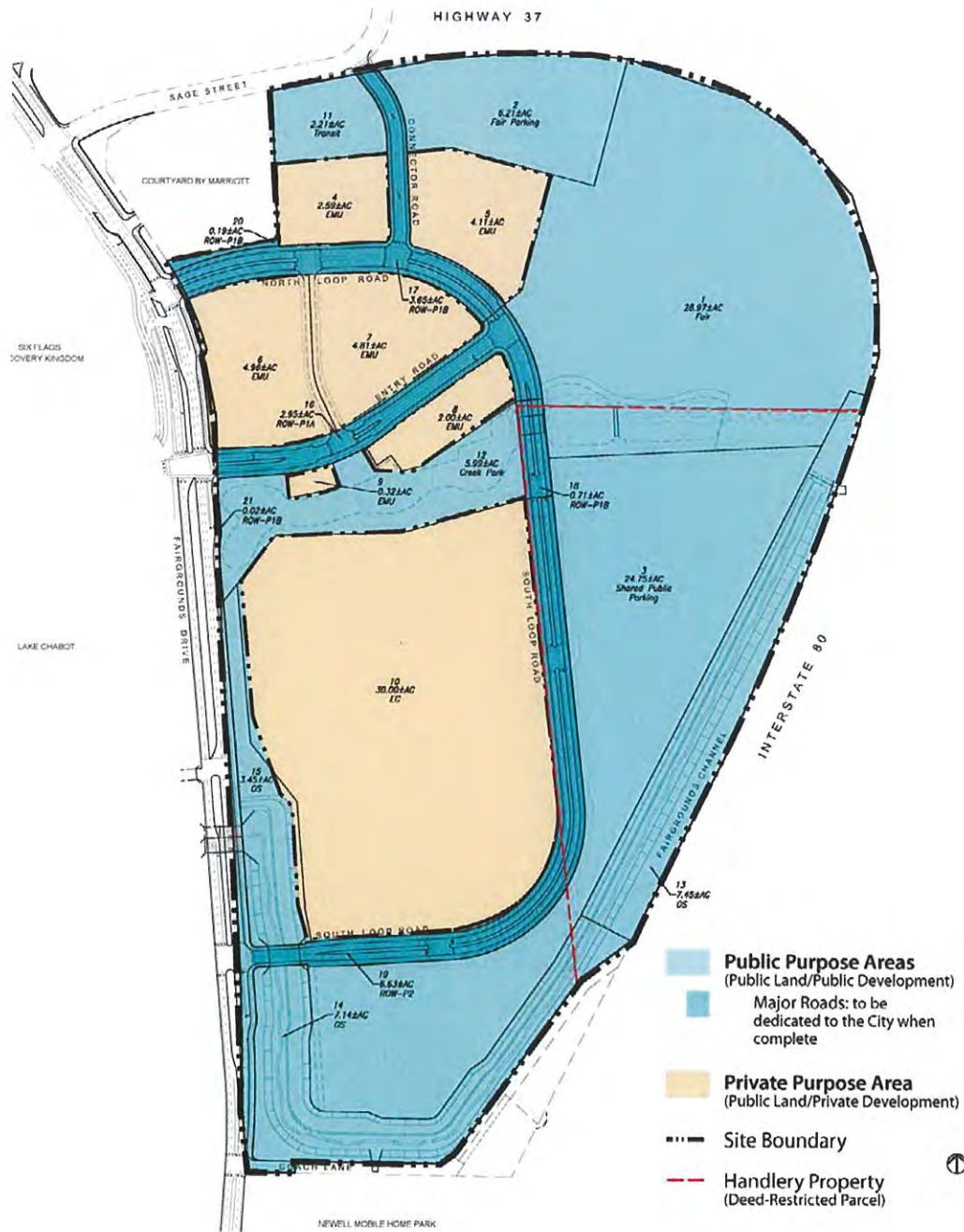
The Plan also serves as a "master plan" for the County, proposing a new "Fair of the Future" with new fairground facilities and Public Purpose Area improvements that continue and enhance the 60-year heritage of the Solano County Fair. The County Board of Supervisors approved the Plan as its master plan on February 26, 2013.

The Plan document, which can be accessed through the City's web site at www.ci.vallejo.ca.us, and provided as Exhibit A to Attachment 1 of this report, contains an Executive Summary, seven chapters, and six Appendices providing technical and supporting data. The primary components of each chapter are briefly described below.

Chapter 1 - Introduction: The Introduction defines the purpose of the Plan, the Solano360 Vision Report and overall planning process. It further describes the entitlements needed for the project which include (1) approval of the Plan as the master plan for the Fairgrounds and certification of the EIR by the County Board of Supervisors; (2) recommendation for adoption of the Plan and General Plan and Zoning Amendments by the Planning Commission; and (3) adoption of the Plan and General Plan and Zoning Amendments by the City Council.

The Introduction also references the financial and fiscal impact analyses prepared for the Project. The Public Facilities Financing Plan identifies potential funding sources and mechanisms for the infrastructure improvements required for the development of the Project.

Figure 1 – Public and Private Purpose Areas



The FIA assesses the City, County and Fair revenues expected to be received as a result of development of the Project, in addition to the operating costs that will be incurred by the City, County and Fair. Although the details of the PFFP and the FIA are not discussed in the Plan, the documents are provided as appendices. The PFFP contemplates that the Project will be primarily funded through the issuance of bonds by the County. In addition, a Community Facilities District (CFD) may be established by the County or the City to issue bonds to pay for

the Private Purpose Area's share of infrastructure. For the City, the Project is projected to generate net fiscal surpluses at build-out of each of the three phases, and generate an annual fiscal surplus for all remaining years of the 50-year analysis.

Chapter 2 - Site and Context: This chapter provides a description of the project area and existing project area conditions. Detailed information regarding property ownership, the area's natural features and access are also provided. A key aspect of property ownership is that although the County owns and controls the entire property, approximately 24 acres of land located on the east side of the site along I-80 and identified as the Handlery Property, is deed restricted and can only be used for public or "fair-related" purposes.

Chapter 2 also discusses the market study conducted on the Vision Report to assess the economic and financial feasibility of the major private uses and the public fairground. The study concluded that the amount and type of office, retail, and hotel uses initially proposed in the Vision Report may not be feasible at this time, and recommended instead that the Plan reduce the land area for these uses and create synergy with the adjacent Six Flags Discovery Kingdom facility by encouraging a mix of experiential entertainment, shopping and food uses. These recommendations are incorporated into the Specific Plan.

Chapter 3 - Land Use: Relying on the recommendations of the Vision Report and associated market study, the Land Use Plan involves the development of Entertainment Mixed Use ("EMU") venues and facilities that were determined to be more feasible in the near term, and creation of a larger parcel for a future Entertainment Commercial ("EC") use as a new, major anchor or entertainment "gateway", as shown in Figure 1 above. To facilitate this phased development, a new internal street system would be created to include new roadways with access from Fairgrounds Drive (North and South Loop Road and Entry Road) and from Sage Street (Connector Road).

The Plan designates land use areas as follows:

- Entertainment Mixed Use (EMU): 18.8 acres for entertainment-oriented commercial uses such as "Family Entertainment Centers" which are businesses that combine eating, entertainment, small amusement park, gaming, and similar uses within buildings and/or as outdoor venues, restaurant and retail activities. Up to 50 residential units above ground floor retail/commercial are allowed in this area.
- Entertainment Commercial (EC): 30 acres for a major entertainment use that requires a large undivided site. The Plan also allows for smaller attractions for flexibility.
- Fair: 35 acres for the revitalized Solano County Fairgrounds area or Fair of the Future, including built and open space venues and parking.
- Transit/North Parking Center: 2.2 acres for a transit and parking facility in the northwest area of site with surface parking in Phase 1, and a parking structure constructed in Phase 2.
- Parking and Roads: 24.7 acres for major roadways and shared public parking to support the continuing viability of entertainment uses within and the near the Plan Area.

- Open Space: 6 acres for Creek Park and its water feature that form the spine of the "Public Entertainment Core" connecting the Fair of the Future with mixed use development areas and major roadways.

The entertainment project area would authorize up to 327,571 square feet of retail, commercial, entertainment and office space, and up to 30 acres of theme park-type uses. Three parking structures would also be constructed on the site as well as several surface parking areas. The fairgrounds portion of the site would include up to 149,500 square feet of new building space at the time of full build-out, including a new exposition hall and concert arena/grandstand cover.

A summary of the land use program is provided in Table 1 below.

Table 1 – Land Use Program

Land Uses	Acres	Building SF	Housing Units	Parking Stalls
Public Development Areas	35.2			
Fairgrounds	1.1	149,500		775
Transit/North Parking Center Bus Docking	1.1			
Shared Public Parking Structure	5.0	121,600		380
Shared Public Surface Parking	19.7	800,000		2,500
Creek Park (w/water feature)	6.0			1,980
Fairgrounds Channel	17.9			
Major roads	14.3			
Subtotal	100.3	1,071,100		5,708
Private Development Areas				
Entertainment Mixed Use (EMU)	18.8	327,571		804
EMU Parking Structure (included in EMU)		320,000		
Residential (included in EMU area)			50	
Entertainment Commercial (EC)	30			750
Subtotal	48.8	647,571	50	2,554
TOTALS	149.1	1,718,671	50	8,262

A list of permitted land uses and uses permitted with a Conditional Use Permit is provided in Chapter 3. Also included are interim uses that would be allowed prior to full build-out of the property.

The illustrative land use plan is shown in Figure 2 below.

Figure 2 – Illustrative Land Use Plan



Project phasing is also discussed in this chapter. As summarized below, the phasing involves both the land use and infrastructure programming:

- Phase 1a/1b (Years 1-5 after bond issuance)

Land Uses: Upgrading and expansion of the Fairgrounds including the demolition of the existing Exposition Hall, and construction of Phase 1 of the new Exposition Hall (approximately 72,000 square feet); new outdoor venues, and public amenities in the Entertainment Core; creation of EMU venues and facilities that may be feasible in the near term.

Infrastructure: 1a - Minor perimeter road improvements; Entry Road; Interim North and South Fair Parking, and mass grading; 1b – Northern segments of Loop Road; Sage-Loop Connector Road, and Creek Park including the water feature, trails, and open space within the public and private development areas.

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- Phase 2 (Years 6-16 after bond issuance)
Land Uses: Creation of a larger parcel for a major EC user and additional EMU development; build-out of the Plan Area at a density supported by surface parking.
Infrastructure: Completion of South Loop Road and the intersection with Fairgrounds Drive, channel improvements and all shared public parking areas.
 - Phase 3 (Years 16-25 after bond issuance)
Land Uses: Further intensification of the Fairgrounds venues including completion of the Exposition Hall to approximately 144,000 square feet; further EMU and EC development.
Infrastructure: Construction of the South Parking Garage.

Parcels designated as Private Purpose Area will be governed by the Plan based on the overall land use policies, policies for the three major categories including Entertainment Mixed Use, Entertainment Commercial, and Residential, as well as policies related to the project phasing.

Chapter 4 - Urban Design and Guidelines: Chapter 4 of the Plan provides design guidelines and standards for future development within the Plan Area. The Draft Plan document states that "more detailed design guidelines would be included in the Development Agreement/Implementation Agreement between the County and the City". However, following the release of the Draft Plan in November 2012, County and City staff acknowledged the need to incorporate all design guidelines as part of the Plan to be adopted. As such, County Planning staff in collaboration with City Planning staff developed more detailed design guidelines to replace Chapter 4 in the November 2012 document, which includes the deletion of the above-mentioned statement. The Revised Chapter 4 is included in the "Adopted Revisions of the Solano360 Draft Specific Plan" document attached to this staff report as Exhibit B to Attachment 1.

The revised Guidelines address the proposed urban design concepts, landscape plan, access and circulation, signage and lighting, streetscape and entries, and other design-related elements. Most notably, the urban design concepts are intended to create a seamless integration of public and private uses that addresses the following:

- A Public Entertainment Core with a Creek Park, walkways, promenades, plazas and bridges; a central water feature that connects public and private areas and provides a visual amenity as well as onsite storm water hydromodification; a thematic "Main Street" or Entry Road aligned with Creek Park, terminating at the new Exposition Hall and offering wide urban sidewalks and a pedestrian friendly frontage for restaurants, retail associated with entertainment uses, and gathering areas; and within the Fair, a major Arrival Plaza at the entrance to the Exposition Hall;
- Indoor and outdoor venues for the Fair fostering year-round programming for the facility;
- Strong relationships with nearby entertainment uses via the roadway and pedestrian connections;
- Pedestrian, bicycle and transit connections integrated into the streets and open space systems;
- Creation of a Rindler Creek drainage and an adjacent buffer along the boundaries of the site to alleviate flooding issues;

The Guidelines include character and design principles for the Private Purpose Areas that will be used to assess future development with regard to urban design, building massing, rooflines, scale and articulation, site design and entries, and building lighting. The Guidelines also provide detailed standards regarding the development of the Fair of the Future; however, the regulatory oversight, design review and permitting of the Public Purpose Areas will be provided by the County.

Chapter 5 - Transportation: The Transportation chapter provides detailed information, such as road sections, regarding the proposed on-site and off-site roadways and improvements that will be needed to support the project. It also includes policies associated with transportation, parking and circulation systems for vehicular, pedestrian, bicycle and transit travel within the Plan Area.

As mentioned, new roadways will be constructed to facilitate circulation throughout the Plan Area. This includes North and South Loop Road, a high capacity roadway connecting Fairgrounds Drive at two intersections; Entry Road, a lower-capacity but highly visible secondary access from Fairgrounds Drive; and Connector Road designed primarily for service and transit vehicles and local traffic from Sage Drive. The roadway design includes 11-foot lane widths and highly visible crosswalks intended to control vehicular speed, enhance the pedestrian environment and promote safety. The circulation plan provides a network of pedestrian and bicycle facilities including multi-use paths, controlled crossings and bicycle lanes.

Parking facilities are also described in Chapter 5 and include surface lots, on-street parking, and structured parking garages to meet the demands of the increased development and parking for the Fair and Six Flags Discovery Kingdom. On-street parking is proposed along both sides of Entry Road only. A 24-acre surface parking lot is proposed on the east side of South Loop Road, and three parking structures are proposed at the Transit/North area, the southern end of the property, and near the North Loop Road/Fairgrounds Drive intersection. In addition to the parking facilities, the project allows for the use of shuttle connections to the parking facilities, Six Flags Discovery Kingdom, and Plan Area destinations. Similar to development of the primary land uses and infrastructure, parking facilities are programmed to be developed within three phases as defined in Table 5.1 in the Specific Plan. Currently, there are 3,950 spaces on the site. Based on the parking supply provided for all three project phases, a cumulative total of 8,262 spaces would be provided through the proposed parking facilities.

Lastly, Chapter 5 addresses the off-site transportation improvements required as part of the future Redwood Parkway/Fairgrounds Drive Improvement Project initiated by the Solano Transportation Authority (STA). This includes the requirement that the County or its developer contribute its proportionate fair share funding for future traffic improvements at the Redwood Parkway/I-80 Westbound Ramps/Fairgrounds Drive intersection, the Fairgrounds Drive/State Route 37 ramp intersections, and widening the Fairgrounds Drive/State Route 37 off-ramps at various locations. Both the City and the County recognize that construction of Phase 2 or 3 is predicated on the construction of the Redwood Parkway/Fairgrounds Drive improvement project at the two interchanges as identified in STA's Comprehensive Transportation Plan. Although that project is recommended for funding in the MTC Regional Transportation Plan, a funding source has not been identified.

Chapter 6 - Public Infrastructure and Services: Chapter 6 defines the infrastructure and services needed to meet City standards and integrate with the existing and planned facilities for the Project without impacting services to existing residents or businesses within the City. The chapter provides an overview of the major infrastructure improvements to serve full build-out of the Plan Area with regard to storm drainage, potable and non-potable water, wastewater, electricity, natural gas, telecommunications, wireless communications and waste management services. The primary elements include removing the Plan Area from the flood plain which is caused by high off-site flows from the east and south, and constructing the Creek Park to manage and reduce peak discharges from the Plan Area. Removing the area from the flood plain will require widening and deepening the existing Fairgrounds Channel which will also improve the flooding conditions for the adjacent Newell Mobile Home Park.

Chapter 7 - Implementation: Planned strategies and actions to be undertaken by the County and City are provided in Chapter 7. The overall development strategy assumes the County will have property owner responsibilities in addition to compliance with any conditions of approval set forth in future entitlements. The following represents the key elements for project implementation:

- The County and City will enter into a Development Agreement (“DA”)/Implementation Agreement.
- The County may issue a Request for Proposals (RFP) for a single Developer or multiple Developers for the site. Such agreement(s) may include a ground lease of land.
- The County, or its Developer(s), will have responsibility for constructing all “horizontal development” (including grading, roads, and utilities) necessary to serve the Plan Area. Major roads will be built by the County and dedicated to the City of Vallejo once constructed to City standards.
- The County, or its Developer(s), will have responsibility for the preparation of finished pads for the EMU and EC parcels.
- The County, or its Developer(s), may seek others to develop the vertical buildings on the EMU and/or EC parcels, or may “build to suit” (develop, maintain and manage).
- The County, or its Developer(s), will sub-lease the EC and EMU parcels.
- EC and EMU end-users will build vertical improvements, or the County, on its own or through its Developer(s), will build-to-suit.

City staff shall review all development within the Private Purpose Areas. In general, all new construction consistent with the proposed Plan will require a Planned Development Unit Plan (“Unit Plan”) acted upon by the Planning Division. Certain uses in the EMU and EC Districts, as noted in Section 3.5.2 of the Specific Plan, may be approved with a Minor Use Permit. Performance of all mitigation measures required by the FEIR will also be required as a condition of approval.

Additional future actions may include tentative and parcel maps, lot line adjustments, grading and building permits, improvement plans, landscape and irrigation plans, Will-serve letters for potable water, Sign Permits, Administrative Permits and any other permits or approvals as required by the Vallejo Municipal Code (VMC). Other permits from outside agencies such as the U.S. Fish and Wildlife Service or U.S. Army Corps of Engineers may also be required.

General Plan Amendment

Amendments to the General Plan are proposed in conjunction with the Plan to ensure consistency. The amendments consist of the incorporation of the Plan through a new land use designation, "Commercial Recreation", and the change of the Plan Area from "Open Space-Community Park" to that designation. As proposed, the Land Use Element is amended to include the following designation for the Plan Area:

- Commercial Recreation – The purpose of the Commercial Recreation designation for the Solano360 Specific Plan Area is to create and establish regulations for a mixed use district in which recreational land uses such as the fair, entertainment, commercial and/or office uses are developed as an integral unit. All uses shall complement and enhance each other and their diversity shall be unified by an overall design concept. The intent of this district is to implement policies of the Vallejo General Plan that call for the establishment of specific areas where flexibility of design and development of diverse land uses is appropriate for the benefit of the city as a whole.

Amendments to relevant sections of the General Plan document and individual elements, as described in Appendix C of the Plan and attached to this document as Exhibit C to Attachment 1 are included.

Zoning Amendments

Concurrent with the General Plan Amendment, amendments to the VMC Zoning Ordinance and Zoning Map are also necessary. Similar to other areas of the City that are regulated by a specific plan, the proposal is to rezone the property as Mixed Use Planned Development (MUPD) from Public/Quasi-Public Facilities (PF). Per Section 16.116, development within the Private Purpose Areas is accomplished through implementation of the Plan's land use designations, urban design guidelines, and development standards.

Specific text changes to the Zoning Ordinance acknowledge the adoption of the Plan and address future project implementation. This includes the requirement for a staff-level Unit Plan permit for all future construction projects that are consistent with the Plan. The proposed text changes also address the role of the Solano360 Committee as an Implementation Committee. As proposed, the Solano360 Implementation Committee will be comprised of two members from the Solano County Board of Supervisors and two members from the Vallejo City Council. The scope and authority of the Implementation Committee is specifically provided for in the Development Agreement. In the event of a dispute arising from the interpretation of the Plan or related documents, or to report on implementation progress, City and County Staff will first work collaboratively, and report to the City Manager and County Administrator as necessary. Should a dispute continue after staff collaboration, the issue will be submitted to the Implementation Committee for resolution. In cases where the Implementation Committee is not able to resolve a dispute regarding the Private Purpose Areas, decisions of staff or the Implementation Committee's decision may be appealed to City Council. To make certain that public input is allowed, Implementation Committee meetings will be held in accordance with the Brown Act.

TENTATIVE MAP

To facilitate leasing and future development, the County has submitted an application for a Vesting Tentative Map prepared by MacKay and Soms to subdivide the Plan Area. (See Attachment 3) As proposed, the subdivision contains 23 parcels to accommodate public and private uses in addition to the transit center, open space areas and street right-of-way. The details are summarized below.

- Parcels 1-4: Public Purpose - Fair and Shared Public Parking Lots (6.2 acres to 19 acres)
- Parcels 5-12: Private Purpose – EMU (.3 acres to 4.1 acres)
- Parcels 13-16: Private Purpose – EC (Four 7.5 acre parcels)
The Plan calls for one large parcel to accommodate a major entertainment anchor; however, the proposed map includes four parcels to provide flexibility in the event smaller venues are proposed.
- Parcel 17: Transit/Parking Facility (2.2 acres)
- Parcel 18: Creek Park (6 acres)
- Parcels 19, 20, 23: Open space and Fairgrounds Channel (14.6 acres, 2.4 acres and 1.0 acres)
- Parcel 21, 22: Right-of-Way (.2 acres and .02 acres)

The remaining 13.9 acres is comprised of street right-of-way.

Street and Sidewalk Design: As illustrated in the proposed tentative map, the planned street pattern contains an internal circulation system consisting of three new public roadways to serve the project area. This system is consistent with the Plan, as described in Chapter 5. Entry Road and North and South Loop Road are intended to provide primary vehicular access to the site from Fairgrounds Drive, while Connector Road will generally serve transit, service vehicles and local traffic from Sage Street.

The proposed street widths and design are also consistent with Plan. Entry Road at Fairgrounds Drive will have a right-of-way width of 96 feet comprised of sidewalks with tree grates/wells, parking lanes, and bike lanes, two travel lanes for two-way traffic, and a center median up to the mid-point of the street. North Loop Road at Fairgrounds Drive is designed with a right-of-way width of 102 feet and contains a similar design as Entry Road with four travel lanes, a parkway or landscape strip separating the sidewalk from the street, no on-street parking and a drop-off lane at Connector Road. South Loop Road has a right-of-way width ranging from 76 to 92 feet and includes a multi-use path for bicycles and pedestrians, has no on-street parking and no median along the bridge area that crosses over Creek Park. Connector Road provides access from Sage Street and is designed with a right-of-way width of 68 feet that contains two travel lanes, with bike lanes, sidewalks and parkways on both sides of the roadway.

The following table provides the proposed street design.

Table 2 – Proposed Street Design

Street Name	ROW Width	Dimensions
Entry Road at Fairgrounds	96'	12' Center Median, (2) 11' Travel Lanes, 5' Bike Lane (both sides), 8' Parking Lane (both sides), 18' Sidewalk with tree grates/wells (both sides)
North Loop Road at Fairgrounds Drive	102'	12' Center Median, (4) 11' Travel Lanes, 5' Bike Lanes (both sides) 8' Parkway/Landscape Strip (both sides), 10' Sidewalk (both sides)
South Loop Road	92'	12' Center Median, (4) 11' Travel Lanes, 8' Parkway/Landscape Strip (both sides), 10' Multi-purpose Path (both sides)
Connector Road	68'	(2) 11' Travel Lanes, 5' Bike Lanes (both sides), 7' Parkway/Landscape Strip (both sides), 5' Sidewalk (both sides)

Other minor improvements to Sage Street and along Coach Lane are also included as conditions of approval.

Although not part of the subject Plan, it is expected that Fairgrounds Drive will be widened to accommodate future traffic volumes generated from cumulative growth in the area. Through the environmental analysis and required mitigation measures, the County and/or developers will contribute a proportionate share of funding for this project.

Utility Design: The proposed map provides the site and utility plan to address future water, sewer, storm water, electrical, gas, cable and telephone facilities for the site. (See Sheet 5 of the Tentative Map.) The major public "backbone" infrastructure improvements are planned to provide services to the entire Plan Area. The existing utilities within the Fairgrounds area would remain in place but will be connected to new "backbone" infrastructure along the new roadways. To ensure continued water service on the property until all of the streets are constructed, the tentative map application includes a detailed phasing plan for the water lines. As a condition of project approval, all utility lines must be located underground.

With regard to storm water and drainage, the majority of the Plan Area is designed to drain to the Creek Park water feature. The storm water will be treated upstream prior to discharging into the water feature per MRP Provision C.3 requirements, and the water feature is proposed to discharge into the existing concrete pipe located in Fairgrounds Drive. Each development parcel will be required to address water quality "onsite" prior to discharging into the Project's backbone storm drain system. In addition, future "onsite" water quality improvements are anticipated to consist of bio-treatment through landscaping. Further details regarding the storm water treatment and drainage are provided in Sheet 3 of the Tentative Map.

Tree Removal and Grading: Extensive tree removal will be needed as part of the mass grading of the site for project implementation. As required, the County has provided a Tree Inventory Report prepared by HortScience, Inc. dated February 2013. Based on the report, the site contains a total of 1,000 trees that are 6 inches in diameter or greater, and a large number of the trees are in good condition. The inventory report provides recommendations for preservation and protection during construction, and approximately 130 trees will be preserved. New trees will be planted on the site as the property is developed.

Due to the phased development of the Project, multiple final maps may be filed. The Planning Commission has final review authority for the Vesting Tentative Maps and the City Council will consider all Final Maps.

DEVELOPMENT AGREEMENT

Pursuant to Section 65864 et seq. of the California Government Code and Chapter 7 of the Plan, the County and City have agreed to enter into a DA. City and County staff have met regularly for the past three months to develop the DA provided as an Attachment to this report. The primary elements of the DA are as follows:

- The DA vests the County's rights to uses, densities and intensities, and maximum height and size of buildings, and other development and design standards as described in the Plan, and clarifies that the Public Purpose Areas are exempt from City land use authority.
- The DA contractually obligates the County to develop the Plan during the term of the Agreement, which is 50 years, and establishes a schedule for commencement of construction of infrastructure improvements.
- As described above, the DA establishes the Solano360 Implementation Committee, comprised of two City Council members and two members of the Board of Supervisors to provide policy oversight and dispute resolution to the respective staff throughout the implementation of the Project.
- The City agrees to work with the County to clear title to the property of the City's reversionary interest. In the event that there is no building permit issued on any portion of the Project within 15 years from the date the City executes a quit claim deed for this reversionary interest, the County agrees to grant to City its reversionary interest.
- The DA establishes a revenue sharing agreement between the City and the County, whereby the City agrees to share a portion of its revenues from the project area in order to contribute to the costs of on-site and off-site infrastructure improvements to the Project and to reimburse the County for a portion of the costs incurred since 2008 for the preparation of the Project Vision and the Plan.
- The intent of the DA is to ensure that the City's costs of providing services to the Project be covered and that the City contributes only a portion of its net revenues from the Project.
- The DA requires the County to indemnify the City for any claims and/or litigation arising out of the Project, existing and future Project approvals, or related decisions, including the processing and adoption of the EIR, which relate to the approvals.
- During the life of the DA, the City will be obligated to process development applications and charge fees in accordance with the City's development-related requirements (e.g., laws related to permitted uses, density or intensity of use, the maximum height and size

of buildings, impact fees, exactions) that exist as of the date the DA goes into effect, with limited exceptions.

- The DA reserves the City's right to modify and apply: 1) regulations governing construction standards, including building codes, plumbing codes, mechanical codes, electrical codes, fire codes, grading codes and other uniform construction codes applicable at the time of permit application; 2) any procedural rules that are uniformly applied on a City-wide basis to all substantially similar types of development projects and properties; 3) new City laws necessary to protect persons or property from dangerous or hazardous conditions.
- Finally, the DA provides for an annual review of the Project.

GENERAL PLAN CONSISTENCY

Several General Plan Amendments are required to gain Plan Area consistency with the Vallejo General Plan. As described above, the proposed amendments involve establishing a new Commercial Recreation land use designation for the Plan area to replace the existing Community Park designation, as well as amendments to relevant sections of the General Plan document and individual elements described in Appendix C of the Plan and attached to this report as Exhibit B to Attachment 1. Through these text and map changes, the Plan as adopted will be consistent with the General Plan.

The Plan is consistent with the following existing General Plan Goals and/or Policies:

- Urban Design Goal 1: To establish a strong city identity.
As proposed, the Plan will create a mixed use development with regional exposure at the crossroads of two freeways and the northern entrance to the City. The successful development at this location will enhance the city identity.
- Urban Design Goal 3: To have attractive, exciting shopping areas.
The land use plan includes retail shopping uses and the urban design and guidelines that will facilitate the development of an attractive, exciting shopping area.
- Land Use and Compatibility Policy 3: Promote the development of a pedestrian-oriented environment by – Reserving ground level floors for retail type uses and upper floors for retail shops, office and residential uses.
The land use plan allows up to 50 residential uses above ground floor retail or commercial space.
- Plan Circulation and Transportation, Non-Motorized Transportation Goal 1: To have facilities that encourage greater use of bicycle for recreation, commuting and shopping.
The circulation plan includes bicycle lanes and multi-use paths within the internal street system.

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- Circulation and Transportation, Non-Motorized Transportation Goal 2: To have safe and pleasant access for pedestrians throughout the community.
The circulation plan also includes wide sidewalks and multi-use paths along the proposed roadways.
 - Public Facilities and Other Services Policy 1: Encourage infilling, that is, development within the urban area already sewer, drainage, and water lines, and streets.
The Plan Area is located within an area that contains existing facilities.
 - Public Facilities and Other Services Policy 2: New development should bear the costs to extend or upgrade public facilities to serve the new development proportionately to the demand generated by the new development.
The Plan includes an infrastructure program that will ultimately be funded by the development.
 - Water Service: Landscaping of public facilities should feature drought tolerant species.
Appendix E of the Plan provides a list of recommended plants for the project area that includes drought tolerant species.
 - Economic Development Goal 5, Policy 4: Support expanded and new festivals and special events, particularly multiple day events, which draw upon the rich diversity of Vallejo's population and physical landscape.
The Plan involves an expansion of the week-long Solano County Fair and development of facilities to support special events.
 - Economic Development Goal 6, Policy 2: Enhance quality of-life amenities such as recreation, arts and culture, and entertainment.
As proposed, the mixed-use development planned for the area includes venues to accommodate recreation, arts and culture and entertainment.

In addition to the above policies and goals, the Plan is consistent with the proposed General Plan goals and policies as provided in Appendix C.

CONFORMANCE WITH ZONING REGULATIONS

As proposed, the site will be rezoned from PF to MUPD, allowing the Plan to serve as the Master Plan for the project area per Section 16.116 of the VMC. The Plan provides regulations, standards, and guidelines that are unique to the site and development envisioned for the area. Various text changes are also proposed to the VMC Zoning Ordinance that acknowledge the development of the Plan and provide a process for addressing future development on the site, including the continued role of the Solano360 Committee.

The Plan is consistent with Section 16.104 Specific Planning Procedure in that:

- A. The Plan is forwarded to the Planning Commission for consideration.
- B. The Plan will be reviewed by the Planning Commission and City Council in the same manner as the General Plan and its amendments.

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- C. No development permits will be approved unless they are consistent with the Plan.
- D. The Plan includes the following elements:
1. The distribution, location, extent, and density of land uses, including open spaces, within the specific planning area;
 2. The proposed distribution, location, extent and intensity of public and private infrastructure (transportation, sewage, water, drainage, disposal, energy, etc.) to be located within the specific planning area and needed to support the land uses described in the plan;
 3. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
 4. Program of implementation measures including regulations, programs, public improvements and works, and financing necessary to carry out the development described in the plan; and
 5. Statement of the relationship of the Specific Plan to each and every relevant provision of the Vallejo General Plan.
- E. Adoption of the proposed Plan would not be in conflict with the zoning regulations and provisions set forth in the VMC Zoning Ordinance.

As provided for in the DA, new City laws adopted in conformance with, and in furtherance of, the Solano360 Specific Plan/Master Plan as part of the City's proposed comprehensive General Plan and Zoning Code Update will be developed in consultation with the County and agreed upon in advance by both the City and the County. The City agrees to negotiate in good faith with the County in adopting regulations to implement the Solano360 Specific Plan/Master Plan.

CONFORMANCE WITH SUBDIVISION REGULATIONS

The County has filed a Vesting Tentative Map to subdivide the 149.1-acre property into 23 parcels. This involves the provision of separate parcels for public and private uses in addition to the transit center, open space areas and street right-of-way. Given the proposed phasing of the Project, multiple final maps may be filed. This proposal has been reviewed and found to be in compliance with Title 15, Subdivisions, of the VMC.

ENVIRONMENTAL DETERMINATION

Pursuant to CEQA, an Environmental Impact Report (EIR) was prepared to assess the impacts of the Plan. The Draft EIR was prepared for Solano County by Michael Brandman Associates as a project-level environmental document that analyzes all phases of the proposed entertainment area, and all phases of the proposed fairgrounds improvements. As the Lead Agency, the County commissioned the preparation of the Draft EIR to disclose the potential environmental effects that could result from the construction and operation of the project, in accordance with the requirements of CEQA. As the jurisdiction with land use authority over the private purpose areas, the City served as a "Responsible Agency" as defined by CEQA for the Draft EIR. In this capacity, the City assisted the County in preparing the Draft EIR, commented on it, and assisted the County in preparing the public Response to Comments.

Water Supply Assessment: California Water Code Section 10910 and Section 15155(b) of the CEQA Guidelines require a water utility/supplier to prepare a water supply assessment for any

development project that, among other criteria, includes more than 500,000 square feet of retail space, 250,000 square feet of office space, or a mixed use development. Given that the proposed Plan involves a mixed use development project on 149.1 acres, the combination of land uses and associated water demands on the project site warranted the completion of a water assessment as part of the EIR for the project.

The County, as the Lead Agency, commissioned Wagner & Bonsignore Consulting Civil Engineers under the direction of Michael Brandman and Associates to complete the Water Supply Assessment (WSA) for the Plan, which is provided as Appendix J of the Draft EIR. The City as the water supplier was required by State statute to make a determination whether sufficient water supplies are available to meet the project water demands of the Plan through the WSA. Based on the WSA, the City's water utility has sufficient water supplies to meet the projected water demand for the Plan during normal, single dry years. On February 13, 2013, the City Council adopted a resolution to approve the WSA.

Draft EIR Conclusions: The Draft EIR concluded that all impacts could be mitigated to a less-than-significant level, with the exception of impacts to air quality, freeway traffic and cumulative freeway traffic, intersection operations and cumulative intersection operation as listed below:

- Air Quality Plan Consistency - The Project will exceed the Bay Area Air Quality Management District's significance thresholds during operation.
- Freeway Traffic and Cumulative Freeway Traffic: 1-80 Segment south of Redwood Parkway
- Intersection Operations and Cumulative Intersection Operations: Fairgrounds Drive/SR-37 EB and WB Ramps, and Redwood Street/1-80 WB Ramps.

The Draft EIR was distributed in November 2012 for public comment with a comment period ending on January 10, 2013. A Final EIR containing the public comments received on the Draft EIR, responses to those comments, and corrections to the Draft EIR document was prepared for the County Board of Supervisors' consideration. On February 26, 2013, the County Board of Supervisors certified the Draft EIR which includes the approved WSA, and adopted Findings of Fact, a Statement of Overriding Considerations for the significant unavoidable impacts, and a Mitigation Monitoring and Reporting Program (MMRP). As a Responsible Agency, the City will play a vital role in implementing the MMRP through its authority to require conditions of approval. These documents can be accessed via the City of Vallejo's web site.

PUBLIC COMMENT

The City and County received considerable input from Vallejo citizens throughout the Plan and EIR process. Many of the comments addressed the need for a transparent and comprehensive review process on behalf of City staff and elected officials. All Solano360 Committee meetings were noticed to the public and public comments were received and compiled by County staff and the consultants preparing the documents.

The City Council and Planning Commission held a joint session on the Draft Plan on January 7, 2013. Public comments received during the meeting primarily focused on the financial feasibility of the project and potential impact of public improvements to the City's General Fund. This meeting prompted a subsequent public meeting held on February 7, 2013 to more fully explore and address comments regarding the project's financial analysis. Similar comments were made

regarding the financial feasibility of the project; however, the consultants hired to prepare the PFFP provided evidence indicating that the financial strategy of using a Community Facilities District (CFD) to fund the improvements would be at no cost to the City's General Fund, and that the City would only serve as the conduit by which the CFD would be formed.

Two long-standing concerns raised by the public relate to the land use plan outlined in both the Vision Report and Plan with respect to recreational uses and the feasibility of a large anchor entertainment attraction adjacent to Six Flags Discovery Kingdom. Several members of the public commented that more consideration should be given to develop large-scale recreational type uses such as sports fields on the property. Although the financial analysis did not directly consider this use as a key component of the Plan, the Plan does include "indoor and outdoor participant sports facilities" as a permitted use. Additionally, the plan to designate the EC private purpose area for a large entertainment anchor is in response to the market study recommendation on the Vision Report which concluded that the adjacent amusement park and Fairgrounds provides an opportunity to create an Entertainment Zone on the property.

As of the public release of the staff report on April 17, 2013, staff has not received any correspondence on the proposed Plan outside of comments received regarding the Draft EIR which are included in the Final EIR document. Any correspondence submitted after this date will be presented to the Planning Commission at the April 29, 2013 public hearing.

STAFF ANALYSIS

The County, City and Fair Association have undertaken an extensive planning process to facilitate the redevelopment of the Solano County Fairgrounds site. This unique collaboration represents a six-year effort initiated in 2007 following the decision to revitalize the Fairgrounds through a public visioning process. The subject Plan provides the regulatory authority to implement the Vision and serves as a guide for land use and infrastructure improvements, public and private investments, and long-term phasing for the Fairgrounds and private development areas based on the Vision principles. The Plan contains the necessary elements of a specific plan as required by the VMC and outlines the implementation actions needed to address future development on the property. Staff believes the Plan is consistent with the Guiding Principles developed as part of an extensive public visioning process and the associated financial analyses conducted on the project conclude that the land use plan is financially feasible.

The proposed General Plan and Zoning Amendments ensure Plan consistency and compliance with the Vallejo General Plan and VMC. This is accomplished by establishing a new General Plan land use designation that addresses the desired use on the site, and incorporates policies and objectives consistent with the Plan. The proposal to rezone the property to MUPD allows the Plan to serve as the Master Plan for the area, per Section 16.116 of the VMC. Further, the incorporation of the Solano360 Implementation Committee as part of the VMC allows that Committee to continue in an oversight capacity.

The County has also filed a vesting tentative map to facilitate the land design and future leasing of the County-owned property. Given the proposed development phasing, multiple final maps will be filed. A number of conditions of approval to address the public improvements and future parcel development are recommended including the incorporation of the EIR mitigation measures.

Lastly, the DA between the County and City provides assurance to the County that the project may proceed in accordance with the zoning designations, densities, design guidelines, and review process set forth in the Specific Plan and establishes a revenue sharing agreement whereby the City agrees to share a portion of its revenue from the project, after covering its costs for service.

CONCLUSION

Staff recommends the adoption of the Solano360 Specific Plan, General Plan Amendment and Zoning Amendments, approval of the Development Agreement between the County and City regarding the Solano360 Specific Plan, and approval of the Solano360 Tentative Map based on the findings contained in the attached Resolutions.

The findings are based upon all evidence in the record including written correspondence and the staff report, all of which is incorporated by reference.

APPEAL

The applicant or any party adversely affected by the decision of the Planning Commission may, within ten days after the rendition of the decision of the Planning Commission appeal in writing to the City Council by filing a written appeal with the City Clerk. Such written appeal shall state the reason or reasons for the appeal and why the applicant believes he or she is adversely affected by the decision of the Planning Commission. Such appeal shall not be timely filed unless it is actually received by the City Clerk or designee no later than the close of business on the tenth calendar day after the rendition of the decision of the Planning Commission. If such date falls on a weekend or City holiday, then the deadline shall be extended until the next regular business day.

Notice of the appeal, including the date and time of the City Council's consideration of the appeal, shall be sent by the City Clerk to all property owners within two hundred or five hundred feet of the project boundary, whichever was the original notification boundary.

The Council may affirm, reverse, or modify any decision of the Planning Commission that is appealed.

ATTACHMENTS

1. Resolution to Approve and Recommend Adoption of the Solano360 Specific Plan, General Plan Amendment and Zoning Amendments with documents attached as follows:
 - Exhibit A – Ordinance to Adopt the Solano360 Specific Plan w/Specific Plan, Adopted Revisions to the Solano360 Specific Plan, and Mitigation Monitoring and Reporting Program (Final EIR is Referenced)
 - Exhibit B – General Plan Amendment
 - Exhibit C – Ordinance to Adopt the Zoning Amendments

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2. Resolution to Recommend Approval of the Development Agreement (DA) with DA as Exhibit A
 3. Resolution to Approve the Solano360 Tentative Map with documents attached as follows:
 - Exhibit A – Conditions of Approval
 - Exhibit B – Tentative Map dated April 3, 2013 prepared by MacKay and Soms

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. PC 13-06

A RESOLUTION OF THE PLANNING COMMISSION
RECOMMENDING THAT CITY COUNCIL ADOPT THE SOLANO360 SPECIFIC
PLAN/MASTER PLAN, GENERAL PLAN AMENDMENT AND ZONING
AMENDMENTS FOR THE SOLANO360 PROJECT

BE IT RESOLVED by the Planning Commission of the City of Vallejo as follows:

WHEREAS, the County of Solano (“County”), the City of Vallejo (“City”), and the Solano County Fair Association (“Fair Association”) have completed a comprehensive visioning process for redevelopment of the Solano County Fairgrounds, a 149.1 acre property owned by Solano County and located at the crossroads of Interstate 80 and State Route 37 in Vallejo, CA; and

WHEREAS, the County and City entered into a Memorandum of Understanding agreement to sponsor the creation of the Solano360 Committee for the purposes of guiding redevelopment of the existing Solano County Fairgrounds; and

WHEREAS, a Vision Report was accepted by the County Board of Supervisors and City Council in May 2009 providing the Guiding Principles for redeveloping the Solano County Fairgrounds; and

WHEREAS, Section 65450 et seq. of the California Government Code provides for preparation and adoption of Specific Plans for the systematic implementation of the General Plan; and

WHEREAS, the Solano360 Specific Plan (“Plan”) was developed as the Planned Development Master Plan for the area, in accordance with Section 16.116 of the Vallejo Municipal Code, and as the master plan for the County Fairgrounds to implement the Vision Report; and

WHEREAS, the Plan proposes a mixed use development that includes "Entertainment-Mixed Use" (EMU) venues and facilities, creation of a larger parcel for a future Entertainment Commercial" (EC) use, a total of 327,571 square feet of retail, commercial, entertainment and office space, up to 30 acres of theme park-type uses, three parking structures, and 35 acres for the revitalized Solano County Fairgrounds area known as the “Fair of the Future, including a new 149,500 square foot Exposition Hall, open space venues, a new water feature, demonstration farm, arrival plaza and midway/event lawn; and

WHEREAS, revisions to the Plan as documented in the “Adopted Revisions to the Specific Plan” are hereby incorporated as part of the Plan; and

WHEREAS, the City has complied with the requirements of the Local Planning Law (Government Code section 65300 et seq.), the current State of California General Plan Guidelines, and the City’s applicable ordinances and resolutions with respect to approval of the Plan; and

WHEREAS, the Plan requires a General Plan Amendment and Zoning Text and Map Amendments for approval from the Vallejo City Council for consistency with the Vallejo General Plan and Vallejo Municipal Code; and

WHEREAS, the potential environmental effects of the Zoning Amendments and the proposed Plan have been assessed in the Draft EIR and Final Environmental Impact Report prepared for the Solano360 Specific Plan (collectively the "FEIR"); and

WHEREAS, the FEIR was certified by the Solano County Board of Supervisors, the lead agency for the EIR on February 26, 2013; and

WHEREAS, the City of Vallejo Planning Commission conducted a duly noticed public hearing to consider the Plan and General Plan and Zoning Amendments on April 29, 2013 at which testimony and evidence, both written and oral, was presented to and considered by the Planning Commission; and

WHEREAS, based on evidence received at the public hearing, the Planning Commission makes the following factual findings:

II. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS

The Planning Commission finds that on the basis of the whole record before it, the City of Vallejo, as a local agency, acknowledges that an FEIR for the project has been prepared by the County and circulated for public review. The FEIR concluded that the project would result in impacts that can be mitigated to a level of insignificance, and impacts that are significant and unavoidable. The Solano County Board of Supervisors certified the FEIR and approved the Mitigation Monitoring and Reporting Program and Statement of Overriding Considerations on February 26, 2013.

III. FINDINGS RELEVANT TO SPECIFIC PLAN/MASTER PLAN APPROVAL

1. The Solano360 Specific Plan/Master Plan is consistent with the goals and policies of the Vallejo general plan and any applicable specific plan.

The Plan is consistent with the following existing goals and policies, as described in the staff report accompanying this Resolution:

Urban Design Goal 1: To establish a strong city identity.

Urban Design Goal 3: To have attractive, exciting shopping areas.

Land Use and Compatibility Policy 3: Promote the development of a pedestrian-oriented environment by – Reserving ground level floors for retail type uses and upper floors for retail shops, office and residential uses.

Circulation and Transportation, Non-Motorized Transportation Goal 1: To have facilities that encourage greater use of bicycle for recreation, commuting and shopping.

Circulation and Transportation, Non-Motorized Transportation Goal 2: To have safe and pleasant access for pedestrians throughout the community.

Public Facilities and Other Services Policy 1: Encourage infilling, that is, development within the urban area already sewer, drainage, and water lines, and streets.

Public Facilities and Other Services Policy 2: New development should bear the costs to extend or upgrade public facilities to serve the new development proportionately to the demand generated by the new development.

Public Facilities and Other Services Policy, Water Service: Landscaping of public facilities should feature drought tolerant species.

Economic Development Goal 5, Policy 4: Support expanded and new festivals and special events, particularly multiple day events, which draw upon the rich diversity of Vallejo's population and physical landscape.

Economic Development Goal 6, Policy 2: Enhance quality of-life amenities such as recreation, arts and culture, and entertainment.

The Plan is further consistent with the following goals and policies of the General Plan as proposed herein.

Urban Design Goal 1-Policy 7: Use a specific plan as the guide for re-use and development of the Solano County Fairgrounds. The Solano360 Specific Plan shall address and promote the development of mixed-use entertainment, mixed-use commercial and fair uses to create a thematic entertainment district that caters to specialty activities not found within the City of Vallejo or the greater region.

Urban Design Goal 3 - Policy 6: The Solano360 Specific Plan Area will serve as a specialty entertainment and retail district, compatible with Six Flags Discovery Kingdom and the Solano County Fair. Amenities located in the Plan Area will include unique and specialty options not found within the region.

Commercial Development Goal 3 - Policy 2: Consider the feasibility of developing complimentary commercial uses in the Northeast Quadrant with the Solano360 Specific Plan Area.

Commercial Development Goal 8: To promote the use of the Solano360 Specific Plan that encourages a mix of commercial entertainment related uses that will become unique commercial assets for the City of Vallejo.

Commercial Development Goal 8 – Policy 1: Use the Solano360 Specific Plan to guide new commercial development in the private parcels of the Solano360 Specific Plan Area in a manner that emphasizes specialty and thematic entertainment land uses.

Transit Goal, Policy 7: The City shall promote the use of the Solano360 Transit Center as a viable option for regional commuter transit use.

Non-Motorized Transportation Goal – Policy 7: The City shall integrate the existing Vallejo Bicycle Route Plan with the Solano360 bicycle route system and parking facilities as a viable option for Plan Area access.

Urban Design Goal: The Solano360 Specific Plan Area will create a unique place with an unmistakable identity that serves as a destination for visitors as well as a pedestrian-friendly, community gathering places. The Solano360 Specific Plan Area will combine a mix of complementary land uses, including retail, commercial, hospitality, recreational, residential, family and youth oriented, educational and civic uses that seamlessly integrate with the “Fair of the Future”. The Specific Plan Area will generate revenues for Solano County and the City of Vallejo, creating jobs and ensure long-term economic sustainability.

Urban Design Goal 3 - Policy 8: The Solano360 Plan Area is designed as a destination entertainment center for specialty retail, restaurant and thematic entertainment uses.

Commercial Development Goal 8: To promote the use of the Entertainment Mixed-Use and Entertainment Commercial Parcels in the Solano360 Specific Plan Area as unique commercial assets for the City of Vallejo.

Commercial Development Goal 8 – Policy 1: Use the Solano360 Specific Plan to guide new commercial development in the Solano360 Specific Plan Area in a manner that emphasizes specialty and thematic entertainment land uses.

Commercial Development Goal 8 – Policy 2: Utilize the unique entertainment and thematic land use patterns adjacent to and within the Solano360 Specific Plan Area in evaluating new commercial development.

Commercial Development Goal 8 – Policy 3: Develop development standards and flexible land use guidelines for commercial development in the Solano360 Specific Plan Area.

Transit Goal - Policy 7: The City shall promote the Solano360 Transit Center as a viable alternative for park and ride commuter transit and as alternative transit access for visiting the Solano360 Plan Area and Six Flags Discovery Kingdom.

Economic Development Goal 5 - Policy 9: The City shall promote the Solano360 Transit Center as a viable alternative for park and ride commuter transit and as alternative transit access for visiting the Solano360 Plan Area and Six Flags Discovery Kingdom.

Economic Development Goal 10: The Solano County Fairgrounds will be redeveloped as part of the Solano360 Specific Plan Area. The existing Fairgrounds will undergo a phased renovation and development of new facilities and structures to achieve economic self-sufficiency. The City, through its land use jurisdiction over the site, will collaborate with Solano County to assist in the phased redevelopment of private portions of the Fairgrounds site in efforts to maximize the economic return to Vallejo and its residents.

The Plan is further consistent with the General Plan land use designation “commercial recreation”, as proposed herein.

2. The Solano360 Specific Plan/Master Plan furthers the stated purpose of the planned development district.

The Plan creates and establishes regulations for a mixed use district in which commercial and residential uses, including office, retail, recreation, entertainment, and open space will be developed as an integral unit. All uses are complementary and will enhance each other and their diversity will be unified by an overall design concept, as provided in the Urban Design and Guidelines Chapter of the Plan. The Plan calls for flexibility of design and development of diverse land use involving a mix of uses involving regional entertainment that is appropriate for the benefit of the city as a whole.

3. The Solano360 Specific Plan/Maser Plan is in conformity with public convenience, the general welfare and good land use practice.

The Plan is consistent with Section 16.104 of the Vallejo Municipal Code Zoning Ordinance and is therefore in conformity with public convenience, the general welfare and good land use practice.

4. The Solano360 Specific Plan/Master Plan will not be detrimental to health, safety and general welfare.

The Plan includes a transportation system and infrastructure plan that meets the City’s requirements to ensure that improvements will not be detrimental to health, safety and general welfare.

5. The Solano360 Specific Plan/Master Plan will not adversely affect the orderly development or the preservation of property values.

The Plan promotes mixed use development within an underutilized area and incorporates urban design guidelines to ensure orderly development and preservation of property values.

IV. FINDINGS RELEVANT TO GENERAL PLAN AMENDMENT, ZONING MAP AMENDMENT AND CODE TEXT AMENDMENT APPROVALS

1. Notice of the public hearing was given for the time and in the manner as prescribed by law.
2. The potential impacts of the proposed amendments have been assessed and an FEIR has been certified for the Project.

V. RESOLUTION APPROVING THE SOLANO360 SPECIFIC PLAN/MASTER PLAN, GENERAL PLAN AMENDMENT AND ZONING AMENDMENTS

NOW, THEREFORE, THE PLANNING COMMISSION OF THE CITY OF VALLEJO HEREBY RESOLVES TO:

- (1) APPROVE AND RECOMMEND THAT CITY COUNCIL ADOPT an Ordinance provided herein as Exhibit A to Attachment 1, adopting the Solano360 Specific Plan/Master Plan (SPA #10-0001).
- (2) APPROVE AND RECOMMEND THAT CITY COUNCIL ADOPT the Solano360 General Plan Amendment provided herein as Exhibit B to Attachment 1 (GPA #10-0001).
- (3) APPROVE AND RECOMMEND THAT CITY COUNCIL ADOPT an Ordinance provided herein as Exhibit C to Attachment 1, adopting the Solano360 Zoning Map Amendment (ZMA #10-0001) and Code Text Amendment (CTA #10-0004).

VI. VOTE

PASSED AND ADOPTED at a regular meeting of the Planning Commission of the City of Vallejo, State of California, on the 29th day of April 2013, by the following vote to-wit:

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF VALLEJO ADOPTING
THE SOLANO360 SPECIFIC PLAN

THE COUNCIL OF THE CITY OF VALLEJO DOES ORDAIN AS FOLLOWS:

SECTION 1. General Findings.

The City Council hereby finds and determines that:

- A. The Solano360 Specific Plan, as defined and described in City Council Resolution No. _____ and attached to this Ordinance as Exhibit A-1, includes the “Adopted Revisions to the Specific Plan” attached to this Ordinance as Exhibit A-2, is the Planned Development Master Plan for the Solano360 Project, pursuant to Section 16.116 of the Vallejo Municipal Code.
- B. As required by Government Code Section 65358(a), the proposed Solano360 Specific Plan/Master Plan is in the public interest of the people of the City of Vallejo.
- C. The Solano360 Specific Plan/Master Plan is consistent with the objectives, goals, policies and general land uses specified in the City’s General Plan, as amended by the General Plan Amendment adopted by Resolution No. _____.
- D. The Solano360 Specific Plan/Master Plan was circulated for public review by Solano County and adopted as the master plan for the Solano County Fairgrounds site by the Solano County Board of Supervisors on February 26, 2013.

SECTION 2. Master Plan Adequacy.

The City Council hereby finds that the Solano360 Specific Plan/Master Plan meets the requirements of Vallejo Municipal Code Section 16.116.060 in that:

- a.) The Solano360 Specific Plan/Master Plan is consistent with the goals and policies of the Vallejo General Plan.
- b.) The Solano360 Specific Plan/Master Plan is consistent with the stated purpose of the Planned Development District.
- c.) The Solano360 Specific Plan/Master Plan is in conformity with public convenience, the general welfare and good land use practice.
- d.) The Solano360 Specific Plan/Master Plan will not be detrimental to the health, safety and general welfare.
- e.) The Solano360 Specific Plan/Master Plan will not adversely affect the orderly development or the preservation of property values.

SECTION 3. Compliance with the California Environmental Quality Act.

The potential environmental effects of the proposed Plan have been assessed in the Draft EIR and Final Environmental Impact Report prepared for the Solano360 Specific Plan Project (collectively the "FEIR"). The FEIR concluded that the project would result in impacts that can be mitigated to a level of insignificance, and impacts that are significant and unavoidable. The Solano County Board of Supervisors certified the FEIR and approved the Mitigation Monitoring and Reporting Program provided herein as Exhibit A-3 to Attachment 1, and Statement of Overriding Considerations on February 26, 2013.

SECTION 4 Adoption of the Solano360 Specific Plan/Master Plan

Based on the findings herein and in the Resolution approved concurrently with this action, the City Council hereby adopts the Solano360 Specific Plan/Master Plan (SPA#10-0001), as specified above, holding on first reading of this ordinance.

SECTION 5. Severability.

If any section, subsection, sentence, clause, phrase, or word of this Ordinance is for any reason held to be invalid by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance. The City Council hereby declares that it would have passed and adopted this Ordinance, and each and all provisions hereof, irrespective of the fact that one or more provisions may be declared invalid.

SECTION 6. Effective Date.

This Ordinance shall take effect and be in full force and effect from and after (30) days after its final passage.

FIRST READ at a regular meeting of the Council of the City of Vallejo held the _____ day of _____ and finally passed and adopted at a regular meeting of the Council held the _____ day of _____ by the following vote:

**ADOPTED REVISIONS TO THE
SOLANO360 SPECIFIC PLAN**

**FINAL ADOPTION OF THESE REVISIONS WILL BE
MADE BY THE VALLEJO CITY COUNCIL**

Proposed revisions to Solano360 Public Draft Specific Plan

Section	Existing text	Proposed revision	Proposed Text
2.3 MARKET FACTORS (p. 11)	As part of the Plan process, the County commissions a market study to evaluate and focus the Vision Report assumptions.	Added the "land use".	As part of the Plan process, the County commissions a market study to evaluate and focus the Vision Report <u>land use</u> assumptions.
New section 3.1.1		Add new section:	

Land use Changes from the Vision Report

As previously described in Section 2.3, Market Factors of the Plan, a market study was conducted to evaluate the land use assumptions described in the Vision Report. Based on the evaluation, the land use program for the Plan has been changed from the Vision Plan as shown below:

USE	VISION PLAN		SPECIFIC PLAN	
	Acres	Subtotals	Acres	Subtotals
PRIVATE DEVELOPMENT				
Entertainment Commercial	14.2		30	
Entertainment/Mixed Use	4.8		18.8	
Mixed Use Commercial/Hospitality	25.4			
Office/Flex Parking	16			
Hotel (250 room)	10.9			
Open Space/Drainage/Wetlands	19.7			
Subtotal		91.0		48.8
PUBLIC DEVELOPMENT				
Fairgrounds	44.9		35.2	
Transit/North Parking Center	2.5		2.2	
Shared Public Parking			24.7	
Creek Park & Water Features			6	
Fairgrounds Channel			17.9	
Major Roads	10.7		14.3	
		58.1		100.3
TOTALS		149.1		149.1

3.4.5 Entertainment-Mixed Use(EMU) (p. 23)	This land use is expected to include "Family Entertainment Centers" (FEC's) as well as associated restaurant and retail activities. Examples of FEC anchor uses within the EMU area include John's Incredible Pizza, Dave & Buster's, and other businesses that combine eating, entertainment, small amusement park, gaming, animatronic shows, and similar uses, either within buildings and/or as outdoor venues.	Clarification as to the meaning of "gaming".	This land use is expected to include "Family Entertainment Centers" (FEC's) as well as associated restaurant and retail activities. Examples of FEC anchor uses within the EMU area include John's Incredible Pizza, Dave & Buster's, and other businesses that combine eating, entertainment, small amusement park, <u>non-casino related gaming</u> , animatronic shows, and similar uses, either within buildings and/or as outdoor venues.
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3.5 PERMITTED USES (p.24)	While the list described below is intended to be inclusive, additional uses may be proposed provided they meet the general intention of the Plan and are approved by the City Development Services Director.	Changed title of City Development Services Director to City Economic Development Director.	While the list described below is intended to be inclusive, additional uses may be proposed provided they meet the general intention of the Plan and are approved by the City <u>Economic Development</u> Director.
3.5.1 Permitted Uses – Fair (p. 24)	· Recreation facilities, including parks, recreation areas and buildings for recreation use.	Include libraries as a permitted use.	· Recreation facilities, including parks, recreation areas, <u>libraries</u> , and buildings for recreation use.
3.5.1 Interim Uses for Fairgrounds (p. 25)	Prior to full buildout of the Plan Area, the Fair may operate interim uses on parcels not slated for development until later phases.	Add the word “any”.	Prior to full buildout of the Plan Area, the Fair may operate interim uses on <u>any</u> parcel not slated for development until later phases.
3.5.2 Permitted Uses - Entertainment-Mixed Use and Entertainment Commercial (p. 25-26)	<p>· Commercial Office including but not limited to establishments that provide financial, real estate, legal, medical services, marketing management, architectural and engineering design, and other comparable professional services and support services; also Business Services including administrative and professional services, business support services, research services, telecommunications facilities, gas and electric services, correspondence schools and vocational schools, educational services, public administrative services, and research and development. Business Services and Commercial offices are permitted up to a maximum of 220,000 square feet; these uses would substitute for other EMU uses.</p> <ul style="list-style-type: none"> • Recreational Vehicles (RV) parks and storage 	<p>Include libraries as a permitted use.</p> <p>Delete RVs as a permitted use on EMU or EC parcels.</p>	<p>· Commercial Office including but not limited to establishments that provide financial, real estate, legal, medical services, marketing management, architectural and engineering design, and other comparable professional services and support services; also Business Services including administrative and professional services, business support services, research services, telecommunications facilities, gas and electric services, correspondence schools and vocational schools, educational services, <u>libraries</u>, public administrative services, and research and development. Business Services and Commercial offices are permitted up to a maximum of 220,000 square feet; these uses would substitute for other EMU uses.</p> <p>Use deleted</p>
Chapter 4	Current chapter	Refine proposed design guidelines.	New chapter attached

CHAPTER FOUR: URBAN DESIGN AND GUIDELINES

4.1 INTRODUCTION

This chapter sets forth urban design concepts and guidelines to shape and facilitate redevelopment of the Plan Area, consistent with the Guiding Principles and land use provisions described above. The intent is to create an exciting, synergistic fusion of entertainment, fairgrounds, and mixed use destinations that builds on the regional visibility of the Plan Area and supports the ongoing success and long-term viability of the Solano County Fair, new Entertainment Mixed Use and Entertainment Commercial uses, and nearby major entertainment uses.

These design guidelines address both overall issues of site development and detailed issues of landscape, building form, walls and fences, and signage. Illustrative plans, photos and other materials are intended as guidelines and examples for review of future building approvals. Lastly, sustainability guidelines are included that both summarize sustainable project elements and provide suggestions for future development.

To assist future users of these design provisions, the following chapter contains separate sections for:

- The overall Plan Area,
- The Fair of the Future (Fairgrounds),
- Other Public Purpose Areas (Major Roads, Creek Park, Fairgrounds Channel, Transit/North Parking Center, and Shared Public Parking), and
- Private Purpose Areas (Entertainment Mixed Use and Entertainment Commercial parcels).

The information in this chapter is informed by the Plan's conceptual studies and may be subject to change as more detailed plans and specifications are developed as part of the design and development process. ~~More detailed design guidelines will be incorporated into a Development/Implementation Agreement between the County of Solano and the City of Vallejo review process.~~

4.2 PLAN AREA DESIGN

4.2.1 Urban Design Concepts

The Land Use Plan (Figure 3.1) establishes a framework for the Plan's proposed urban design features. The intent is to create a seamless integration of public and private areas, including Fairgrounds facilities and private mixed use development.

Guidelines are as follows:

- The Public Entertainment Core, the defining feature of Solano360, encompassing a lively, mixed use entertainment corridor connecting from the gateway at Fairgrounds Drive in the west to the demonstration farm at the Fair's eastern edge. The Public Entertainment Core includes:

- The Creek Park with its walkways, promenades, plazas and bridges,
 - The Creek's Park's central water feature that connects public and private area and provides multiple benefits including visual amenity, wateredge promenades, onsite stormwater hydromodification, capture and reuse of stormwater for irrigation, and water quality treatment,
 - The thematic "Main Street" or Entry Road aligned with Creek Park, terminating at the new Exposition Hall and offering wide urban sidewalks and a pedestrian-friendly frontage for a ~~"restaurant row"~~, restaurants, retail associated with entertainment uses, and gathering areas, and
 - Within the Fair, a major Arrival Plaza at the entrance to the Exposition Hall, a Midway/Event Lawn with terraced seating, the water feature and Creek Park with pedestrian bridge, and a demonstration farm oriented toward families and school groups.
- Indoor and outdoor venues for the Fair of the Future, fostering a year-round program of activities within a variety of active and passive spaces.
 - Transformative Phase 1 project that includes the Creek Park with its water feature and creates a new Exposition Hall located as a focal point for the Entry Road.
 - Strong relationship to nearby major entertainment uses via roadway and pedestrian connections, including integrated design elements and synergistic land use opportunities.
 - Pedestrian, bicycle and transit connections integrated into streets and open space systems.
 - Creation of a Rindler Creek drainage and adjacent buffer along the eastern, southern and western boundaries of the site to alleviate floodplain issues, establish riparian habitat and wetland benefits, and provide the opportunity for pedestrian trails.

These features are described further in this chapter and in Chapters Five and Six.



Building areas depicted here are conceptual only.

Figure 4.1: Illustrative Plan

Sections through Creek Park & Water Feature (at Fair and at Entry Road)

Figure 4.2: Illustrative Section

Figure 4.3: Urban Design Elements

Building areas depicted here are conceptual only.

Figure 4.4: Public Entertainment Core

Building areas depicted here are conceptual only.

Figure 4.5: Site Relationships

View from I-80

View from SR-37

Figure 4.6: Perspective Views

4.2.2 Access and Circulation

Connections to the Plan Area

Figure 4.7 illustrates key features relating to site access, parking, and entries.

The configuration of roads, entries and parking is intended to facilitate efficient access to parking facilities while focusing views on the Creek Park and other destinations, with attractive streets defined by buildings.

Because the Plan Area has a direct, physical connection to Six Flags Discovery Kingdom, the project has also been designed to establish a strong pedestrian character to encourage walking between the theme park and the Fair of the Future. Visitors to the Plan Area will be able to park, shop, dine, relax and visit Fair programs with the option of walking or taking a shuttle.

Connections within the Plan Area

The Plan proposes an integrated system of internal connections that encourages shared use, walking, bicycling and transit. Features include:

- Walkable [gridnetwork](#) of tree-shaded sidewalks, including special Entry Road streetscape (see Figures 4.24 to 26).
- Pedestrian trails within the Creek Park, connecting to continuous perimeter trail along the Fairgrounds Channel.
- Multi-use paths along the South Loop Road, connecting parking areas with the Public Entertainment Core.
- Continuous perimeter trail for the south area of the Plan Area as shown on Figure 5:10.
- New promenades and plazas within the Fair of the Future.
- Raised intersection and pedestrian crosswalks at the Entry Road/Loop Road to calm traffic and provide safe pedestrian crossings.
- A potential parking shuttle serving internal destinations and connecting to Six Flags Discovery Kingdom and the Transit/North Parking Center (see Figure 5.15: Transit and Shuttle Routes).

Accessibility

According to the Americans with Disabilities Act of 1990 "ADA" standards, new facilities constructed by, on behalf of, or for the use of a public entity must be designed and constructed in such manner that the facility or part of the facility is readily accessible to and usable by individuals with disabilities.

Public purpose areas within Solano360 will be designed to provide for ADA access according to applicable ADA Standards for Accessible Design.

4.2.3 Landscape Plan and Guidelines

Figure 4.8: Landscape Character illustrates the location and variety of landscape areas and public spaces envisioned for the Plan Area, including:

- Streetscape planting.
- Buffer/riparian planting along the Fairgrounds Channel, using species that are compatible with the flood control function of the channel.
- Planting along soft or earthen water edges.
- Park landscape.
- Turf, both regular and reinforced (such as with mesh reinforcement material).
- Rain gardens.
- Demonstration Farm.
- Hardscape and plaza areas (including the Fairgrounds Concourse).
- Terrace seating at grade changes along the Creek Park water feature and in the Fairgrounds amphitheater.
- Surface parking areas.

Specific guidelines for Fair property landscape features as well as for the Fairgrounds Channel and Creek Park are included in Section 4.3: Fair of the Future and Section 4.4: Other Public Areas, respectively. The following general guidelines apply to the Plan Area as a whole.

Street Character

- Hardscape and plazas should be paved attractively, with paving patterns and materials conducive to pedestrian circulation and gathering.
- Tree planting should be designed to create shaded areas, especially in public areas such as sidewalks, parking lots, roadways, courtyards, plazas and parks.
- Trees along the Entry Road and at the Arrival Plaza should be of a different character than the streetscape trees on the other roads, and should be planted in tree grates.
- Street trees should be placed in park strips between the curb and sidewalk as shown by Figures 4.24 to 4.26.
- Parkway strips and medians should be planted with a variety of drought-tolerant species.

- Contrasting tree species should be used for perimeter trees and trees along pedestrian corridors and hardscape areas to clearly identify paths of travel.
- Street trees should be spaced at approximately one tree per 25 feet, or less if smaller trees are used.
- Trees for major streets should be a minimum of 24-inch box container size. Fifteen-gallon container size may be used for minor streets and buffers.

Planting Criteria

- Plant materials should be selected from the plant palette in Appendix E: Solano360 Plant Palette. Substitutions or additions may be considered based on the suitability of the species in terms of similarity of form, adaptability, tolerance to site soils, climatic conditions or water quality, or other pertinent characteristics. The plant list is not intended to be exhaustive but to provide a clear guide for selection. Additional plants may be used that are compatible with this list and are consistent with the intent of these guidelines.
- In order to establish a unique and cohesive image for the Plan Area, a limit range of plant material should be used for public roads, park and common areas, commercial sites, and the Fairgrounds. For these areas, the intent is to employ a limited number of plant species for the majority of the planting in each identified area.
- Plant materials should be selected to be at an appropriate scale for the surrounding area when at mature size. Larger, more dramatic species should be utilized for important public areas such as the Public Entertainment Core, major entries, and Loop Road.
- Plant materials should be selected to meet the criteria listed below.
 - Emphasize the planting of drought-tolerant, long-lived plant species that are native and/or well adapted to the climatic and soils conditions of the Plan Area and require minimal maintenance.
 - Avoid planting tree species with invasive root systems near utility lines, concrete and other paving. Such species may be utilized in setback areas adjacent to roadways or in transition areas, provided there is adequate clearance.
 - Avoid the use of non-native, invasive species that may spread into areas of permanent, undeveloped open space.
- Landscaping is required where development is visible from major public roadways and public facilities including trails. Tree planting should consider the need to preserve solar access and views and maintain fire safety requirements.
- All plants should be carefully selected to avoid toxic species that could be harmful to children or cause allergic reactions.
- Planting design should consider year-round interest and seasonal character through the careful use of flower and leaf color.
- Landscape design should provide effective screening of parking areas, retaining walls, utility enclosures, utility cabinets, service areas, or service corridors to reduce

negative visual impacts. Screen landscaping should incorporate evergreen plant species in order to maintain year-round leaf cover.

- Plant materials along water edges at the water feature and in the fairgrounds channel should be native vegetation capable of filtering water, preventing erosion, and providing habitat and food to native species.
- Landscaping within the Plan Area will be subject to any special requirements identified by future soils or drainage investigations.
- Landscape plans should be prepared by a landscape architect registered to practice in the State of California.

Irrigation and Maintenance

- The use of potable water for landscape should be minimized. It is anticipated that non-potable water from the onsite water feature will serve as the irrigation source (refer to Chapter Six for additional details). If reclaimed water becomes available, it may be utilized as well. Any water-intensive planting should be concentrated in shaded areas, where natural runoff occurs, or at highly visible locations, such as within the Public Entertainment Core and at the Arrival Plaza.
- Groundcovers, grasses, or drought-tolerant turf should be used in place of standard lawn where possible.
- Existing vegetation is limited within the Plan Area; however, healthy existing vegetation along drainage ways or other areas should be retained to the extent feasible, with replacement provided where removal is unavoidable. In Phase 1, existing (and healthy) parking lot trees should be retained within parking areas if such trees do not interfere with site development.
- All public areas, rights-of-way and commercial project landscaping should have high efficiency, automatic irrigation systems. Low volume spray heads and drip irrigation systems should be utilized. Landscape improvements should be installed and maintained with a sustainable landscape maintenance plan that uses toxin-free organic or biological fertilizers and weed/pest control products.
- Landscape plans should be submitted to the City to ensure water-efficient irrigation systems according to City requirements.

Building areas depicted here are conceptual only.

Figure 4.7: Site Access & Parking

Building areas depicted here are conceptual only.

Figure 4.8: Landscape Character

Transition Areas and Buffers

Grade transition areas between development and site edges are subject to the following:

- Transition areas should be landscaped to create a visually pleasing transition between development and common areas, and provide filtered views both from and toward the Plan Area. Landscaping of transition areas is required where development is visible from major public freeways or roadways and from public facilities.
- Landscaping of transition areas should emphasize trees and shrub planting and grasses. Irrigation should be provided for plant establishment.

Site Drainage

- All site stormwater runoff must be treated consistent with the San Francisco Bay Region Municipal Regional Stormwater NPDES Permit (MRP) prior to discharging into an offsite drainage system. Treatment should utilize Best Management Practices (BMPs) and Low Impact Development (LID) principles as specified in MRP Provision C.3.
- Acceptable treatment measures within the Plan Area may include:
 - Infiltration
 - Evapotranspiration
 - Biotreatment (e.g., rain gardens, bioswales, biotreatment units, planter/tree boxes)
 - Minimizing impervious areas
 - Constructed riparian channel (see Section 4.4.3: Fairgrounds Channel)
- BMP's should be incorporated into parking lots, medians, and street/parcel edges.
- Sub-drains should be provided unless a percolation test shows such drains are unnecessary.

Erosion and Sedimentation

- Grading operations should be planned and implemented to efficiently control erosion and sedimentation.

Berms, Channels and Swales

Berms, channels, and swales should:

- Be shaped to appear as an integral part of the graded or paved surface.
- Have smooth transitions between changes in slopes.
- Be designed so as to appear a natural part of the site topography.

Slopes and Retaining Walls

- Landscapes should incorporate smooth transitions between changes in slope.

- The maximum slope for a landscaped area should be 2:1 if the area is planted with a ground cover and 3:1 if planted with lawn.
- Where space constraints exist, terracing with retaining walls will be allowed.
- Retaining walls should not exceed three feet in height. For grade changes that exceed three feet, walls should be stepped in equal increments with three foot-wide planted terraces between.
- Retaining walls should be constructed of a low-maintenance, durable material compatible with nearby architecture.

4.2.4 Parking Areas

This section addresses design of parking facilities, located per Figure 5:14: Land Use and Parking. Chapter Five provides additional information on phasing of parking facilities.

Overall Guidelines

- In general, parking should be located and designed to allow buildings to be located directly along street frontages, with parking areas to the rear, while providing adequate parking facilities to serve commercial and public uses.
- During peak use periods, such as Saturdays and Sundays during Fair Week, parking may be augmented by shuttles to offsite locations.
- Parking facilities (including surface lots and structured parking) with pedestrian or vehicle access from Entry Road should be screened at the street level by buildings or significant amenity features to maintain an active street character and well-defined street edge.
- Signs indicating routes to parking should be displayed clearly along the Entry Road, Loop Road and Connector Road in order to guide visitors.
- Shared parking between the Fairgrounds, nearby major entertainment uses, private development, and other parking users should be maximized and will be defined by a Parking Operations Management Plan to be prepared by the County and by parking agreements between the County and Six Flags Discovery Kingdom.
- Parking should not be located adjacent to the Creek Park or water feature in order to maintain the open space character of those areas (see Section 3.6.1).

Surface Lot Design and Landscaping

As described in Chapter Six, a majority of the Plan Area, including parking lots, will be designed to drain to the Creek Park water feature. The water feature will provide water quality treatment, but it is likely that bio-treatment will need to be integrated into the parking lot design as well.

- Surface parking lots should be planted with trees to minimize their visual impact, reduce heat gain, and create a more comfortable pedestrian setting.
- For private areas (EMU and EC development), trees should be planted at a rate of one tree per six parking stalls.

- Larger scale parking areas, such as Shared Public Parking, require more flexible landscape guidelines in order to serve multiple purposes such as temporary fairs and festivals; therefore, tree planting may be concentrated along perimeters, entries, and key pedestrian corridors.
- Parking lots may be developed with photovoltaic arrays (in place of trees) as described in Section 4.6.2 Next Step Sustainability Measures.
- Ample, well-lit and shaded (either by trees or solar collectors) pedestrian routes should be provided from parking areas to main destinations and building entries. Where possible, pedestrian circulation should be separated from vehicular areas.
- For interior parking lots, smaller trees should be selected to allow adequate visibility beneath mature tree canopies to building entries and storefronts.
- All surface lots should have landscape buffers at street or other public area edges. Landscape buffers should consist of trees and low plantings (to provide views into lot interiors) interrupted with regular pavers or other walkways for ease of pedestrian access.
- All major surface lots should incorporate bicycle parking facilities.
- Passenger loading areas for ridesharing vehicles and preferred parking for carpools and/or certified pure zero emission vehicles (100% battery electric and hydrogen fuel cell) and compressed natural gas (CNG) vehicles should be located near main building entrances.
- Two way parking lot drive aisles should be a minimum 24 feet wide.
- Parking lot landscape islands should be a minimum of eight feet wide at the aisle ends and a minimum of six feet wide elsewhere.
- Tree wells and planting strips should be a minimum of six feet diameter/ width and should be located between all doubled-loaded parking rows.
- Parking lots should incorporate handicapped spaces per ADA guidelines; such spaces should be located near entry points.

Design of Parking Structures

As parcels develop and land use intensifies, structured parking may replace surface lots in the southern end of the Plan Area (South Parking Structure), within the Transit/North Parking Center, and within the Entertainment Mixed Use area. These structures will support anticipated Phase 3 development including expansion of the Exposition Hall and expansion of the Entertainment Mixed Use and Entertainment Commercial development.

- Parking structures should be screened with planting of suitable scale and species.
- Parking structures located in the EMU area should be wrapped by ground floor retail or entertainment uses along the North Loop Road or other public roads, and retail/commercial uses are encouraged for the ground floor of parking structures to activate streets and pedestrian corridors.

- The upper floors of parking structures should utilize planters, trellises, vegetated walls or other decorative screens along vertical walls at street frontages or other public area and open space frontages.
- Parking structures should be designed to complement nearby architecture in terms of style, massing, color and detailing, and should be located to prevent shadowy, windy canyons.
- Interiors of parking structures shall be well lit and shall utilize light colors on interior walls to create a safe and comfortable environment.

4.2.5 Signage and Lighting Guidelines

See Section 4.3.6 for Fair of the Future signage, lighting and site furnishing guideline; see Section 4.4.6 for guidelines addressing electronic reader board signage on the Fairgrounds adjacent to I-80 and SR-37.

Figures 4.22 and 4.23 provide examples of site furnishings and lighting.

Signage

Signs will aid in establishing the sense of quality and character for the Plan Area, in addition to conveying critical wayfinding information for visitors.

- Comprehensive signage programs should be developed for both the Private and Public Purpose Areas. These programs should be prepared together or, if prepared separately, should be coordinated to convey a unified identity for Solano360 including the Fair of the Future, Creek Park, and the entertainment and retail development.
- Permanent signs prepared as part of comprehensive signage programs should include entry signs, area signs, directional signs for vehicles, bicyclists/pedestrians, street signs, interpretive and educational signage within the Creek Park and Fair, and signs identifying businesses in the EMU and EC areas.
- Temporary signs may include special event signs, temporary signage during construction or at the opening of a new venue or business, real estate information signs, and parking controls for major events.
- In general, signs should be utilized only where necessary, emphasizing an image of permanence and quality; however, signs should offer adequate visibility and reflectivity, where appropriate, to provide for safety and orientation at night. The purpose of permanent signage is to convey information, to aid in identifying visitor destinations and to add an element of consistency.
- Entry signs may be integrated into entry pylons, arches, or other features.
- All permanent signs and monuments should be constructed of durable, high quality materials.
- Freestanding signs should be limited to directory-type signs with information limited to the name of the project for multi-parcel developments and building or address numbers.
- Access to parking should be adequately signed to guide visitors to parking facilities.

- All free-standing parcel or project signs along streets and common access drives should be designed as a 'family' of signs, consistent with the architectural style of related buildings.
- Small, free standing signs for individual buildings may be allowed near building entries; such signs should be consistent with the architectural style of the building. Other signs for individual buildings or tenants should be located on the building in a manner consistent with the architectural style.
- A digital kiosk or marquis sign at the Entry Road entry or other appropriate location may be allowed for use by the Fair Association for Fair and other Solano360 events.
- With the exceptions noted above, all signs within Private Purpose Areas should conform to the City Zoning Ordinance Chapter 16.64.
- For signs within the Private Purpose Areas, sign area and dimensions shall be based on the approved sign program for a specific building or project.

Lighting

Street-level and pedestrian lighting are important for safety and will also contribute to site identity and character within the Plan Area. Lighting elements should adhere to the following.

- Lighting should be designed to differentiate use areas, emphasize amenities and landscape features, provide continuity along street corridors and promote safety.
- Lighting may be combined with banners or incorporated into other pageantry and wayfinding features to create a festive setting.
- In general, lighting should provide sufficient levels of ambient light to create a safe and pleasant environment without causing light pollution or glare into adjacent properties.
- Low-level, cut-off, pedestrian-scale fixtures should be utilized to the degree possible.
- Street lighting should be directionally shaded to reduce off-site fugitive light and glare.
- Exterior building lighting should be shielded to minimize direct glare and reflections.
- Lighting should utilize LED or other energy-efficient fixtures with pleasing light color.
- Materials for lighting fixtures should be durable and low maintenance. Natural finishes like bronze, and nickel steel are recommended.
- Spacing and illumination levels should be calibrated to achieve IESNA standards (e.g., a 0.5 foot candle level for sidewalks in medium pedestrian activity areas), and local requirements, based on photometric studies prepared as part of design submittals for each street.
- Intersection lights should be on 22-foot tall poles.
- Pedestrian lighting along sidewalks should not exceed 15 ft in height.
- Parking lot lights should be no higher than necessary to provide efficient lighting of the area, but should not exceed 28 feet, including the base.

4.2.6 Walls and Fences

Walls and fences may be used to define public and private boundaries and spaces, as described below. See additional guidelines for Fairgrounds fencing and entries in Section 4.3.

- Where used, walls and fences should be open and/or low to maintain an inviting, attractive appearance and provide adequate sight distance for entries. Materials should be compatible with and complementary to principal buildings. Fence and wall panels may be divided into regular modules that reflect the module of the principal building.
- Thick and thin elements should be used, with thicker pieces for supports and panel divisions. Fence posts and support columns should be emphasized and/or built-up.
- Screen walls are intended to screen uses such as loading, service areas, and utilities, while maintaining a common architectural language with the buildings surrounding them. All screen walls connected to buildings should match the building style. Maximum height of a screen wall should be six inches higher than the object being screened.
- Masonry walls should have a base and coping.
- Fences visible from public areas should be wrought iron, cast iron, and welded steel ornamental fences or wood. Metal fences may be mounted on a low masonry wall, and/or spanning masonry piers. Wooden fences should be painted, preferably a light color.
- Security fences should not be visually prominent. Black, vinyl-clad chain link fencing (with matching posts) may be used for security fencing with a maximum height of ~~seven~~six feet; taller fences may be allowed along freeway edges. Evergreen hedges, flowering vines and/or trees should be planted along the base of all security fences.
- Black, vinyl-clad chain link fencing (with matching posts) may be utilized for storage or service areas that are not visible from public areas, including public roads.
- Plywood, un-clad chain link, barbed wire or razor wire fence are prohibited.

4.2.7 Loading and Service Areas

- Loading areas should be sited to the rear building or sides of buildings not visible from public areas, including streets.
- All service, loading, trash, storage areas, and utility equipment should be screened from public view utilizing a combination of planting and architectural elements that are compatible with the building architecture.
- Loading/garage doors are prohibited on building facades facing a public street.
- Service loading from public streets is prohibited except for parcels where other configurations are not feasible, such as adjacent to the Creek Park.
- No refuse or storage areas may be located between the front of a building and a primary road right-of-way except for parcels where other configurations are not feasible, such as adjacent to the Creek Park.

- Refuse collection and storage should be located to the rear and sides of buildings, covered with a roof, and sized to contain all refuse generated on site between collections.
- Common recycling bins should be provided for all commercial uses and must be readily accessible to all tenants/employees, and be screened in the same manner as refuse collection areas.
- Transformers and other utility equipment should not be placed in the public street setback area.
- All rooftop equipment should be fully screened with the same or similar materials of which the building is constructed.

4.3 FAIR OF THE FUTURE

4.3.1 Fairgrounds Programming

Throughout the planning process, Solano County Fair Association representatives provided input regarding near-term and mid-term plans to establish a new Fair of the Future that could offer a broad array of year-round activities while maintaining the traditions and community connections of the existing Fair.

Outdoor spaces, including lawn and hardscape plazas, are of critical importance to the Fair.

Following are the identified program uses for the Fair of the Future:

- Establishment of a new, flexible event hall of approximately 50,000 net square feet of exposition/event space, with potential for expansion to 100,000 net square feet in the future when demand warrants such an expansion.
- Ability to provide an array of event and entertainment venues to respond to market opportunities and region serving demand.
- Selective update, expansion and/or replacement of existing Fair facilities.
- Desire to have complementary program to Six Flags Discovery Kingdom and adjacent mixed-use development.
- Convenient and proximate transitions from indoor to outdoor venues.
- Branding and image to focus on local culture and heritage of the Fair, with consideration of the County Fair roots/heritage: Livestock, Agriculture, Food and Community.
- Reinforcement of important County Fair themes including (1) heritage of Solano County Fair; (2) sustainability; (3) agricultural demonstration.
- Expression of the diverse character of Solano County, (urban / rural, ethnic/cultural diversity, lifestyle diversity) and effective use of the site's key location at the crossroads of major roads.

In addition to current events and activities at the Fair, specific new attractions and programming could include:

- A Ferris wheel or similar feature visible from I-80.

- "Mini-midway", or small amusement park, with year-around operation.
- "Festival-on-the-green" program of activities within a new event open space; consideration of an outdoor inflatable movie screen.
- Demonstration Farm that could attract school groups and take advantage of interests in micro-sustainability and urban farming.
- Wedding events with location for wedding 'photo op.'
- Tractor pulls, livestock shows and similar agriculture-related events and activities.
- Running or walking races.
- Flea markets and farmer's markets.
- Complementary operational relationships with Six Flags Discovery Kingdom, local hotels, and other businesses, such as providing exhibit or meeting space to help hotels attract larger scale meetings or convention business.

Figure 4.9: Existing Fairgrounds Facilities

Figure 4.10: Proposed Fairgrounds Facilities

4.3.2 Fairgrounds Design Objectives

Figures 4.11 and 4.12 illustrate the conceptual plans for the Fair's outdoor and building venues for Phases 1 and 3. As envisioned, the Fair of the Future plan upgrades the Fairgrounds in its current location, with long-term flexibility to expand southward into parking areas as additional space for event venues is required beyond the scope of this Plan.

The overall objectives of this conceptual-level design are as follows:

- Provide new, multi-functional event facilities that expand the Fair's abilities to market to a wide variety of entertainment, educational, commercial, and civic programs on a year-round basis.
- Create new outdoor venues adjacent to and in association with the new Exposition Hall to support the Fair's program of outdoor events and create appealing and durable outdoor public spaces. For maximum usability, these venues should include both turf and paved spaces and should be designed as "outdoor rooms" with simple, outdoor areas framed by trees and/or buildings.
- Distribute parking areas and entry gates, with clear wayfinding signage to enable flexible event programming and allow the Fair facilities to serve multiple, concurrent events.

- Develop options for year-round uses and products at the Fair; require that events and attractions stay relevant and relate to contemporary preferences for food, entertainment and education.
- Consider the selective update, expansion, and/or replacement of existing Fair facilities in a phased program that allows each incremental stage to function effectively.
- For intermediate/interim enhancements to Fair facilities, consider “facelifts” to key buildings and enhancements to the grounds.

4.3.3 Fairgrounds Phasing

Flexibility is a critical objective for the Fair of the Future. The phased upgrade of structures and open spaces is intended to allow multiple and shared uses, allowing the Fair to operate and generate revenue throughout the year and providing for maximum synergy with non-public and public uses on the overall site.

- **Phase 1** (Phases 1a and 1b) includes the demolition of the existing Expo Hall and construction of the new Exposition Hall providing approximately 50,000 net square feet (approximately 72,000 to 77,000 gross square feet, depending on whether Administrative and Security Offices are included). Associated outdoor venues, including Arrival Plaza and Midway/Event Lawn and Creek Park with water feature, are scheduled for Phase 1. If funds are available, Phase 1 could include relocation of the existing Administrative and Security Offices into the building; alternatively, this may occur in Phase 3.
- In **Phase 2**, in order to provide for North Fair Parking expansion, the existing County Building will be demolished. The Fair’s Administrative and Security Offices will also be demolished and housed in portable buildings, if not already located within the Exposition Hall in Phase 1.
- In **Phase 3**, or if sufficient demand arises in Phase 2 and if supported by onsite and offsite infrastructure and mitigations, the Exposition Hall will be expanded to approximately double the Phase 1 footprint and program. The Phase 3 expansion will require demolition of the existing concert arena and construction of a new amphitheater for concerts and theater events as shown in Figure 4.12. If Administrative and Security Offices are still housed in portables, they would be relocated into permanent space within the expanded Exposition Hall.

Together with the existing facilities that will continue to function (including Gibson, McCormack, the livestock and sheep buildings), this phased approach provides essential facilities that will allow for the efficient operation and financial sustainability of the Fair of the Future.

Table 4.1: Fair Building Program & Phasing

Facilities to be demolished and/or replaced by buildout				
Facilities to Remain				
EXISTING BUILDINGS AT CONCOURSE (Note: does not include facilities for horse racing or golf course)	EXISTING QUANTITY (sq. ft.)	PHASE 1 (sq. ft.)	PHASE 2 (sq. ft.) ¹	PHASE 3 (sq. ft.) ¹
Admin/Directors Trailer/Security Office	5,110			
County Bldg	17,170	17,170		
Gibson Hall	13,325	13,325	13,325	13,325
Concourse Restroom	1,650			
McCormack Hall	22,000	22,000	22,000	22,000
Civic Bldg	12,325	12,325	12,325	
Trash Shed	2,000	2,000	2,000	2,000
Maintenance Shed	4,550	4,550	4,550	4,550
Livestock Bldg	32,400	32,400	32,400	32,400
Sheep Barn	13,285	13,285	13,285	13,285
Concert Arena/Grandstand Cover	5,200	5,200	5,200	
Twilight Patio Office/Concessions/Storage	1,800			
Existing Exposition Hall	23,730			
Guard Shack (adjacent to director's trailer)	1			
TOTAL Existing	154,545	122,255	105,085	87,560
NEW BUILDINGS (based on project description)		PHASE 1	PHASE 2 ¹	PHASE 3 ¹
New Exposition Hall ²		72,000	72,000	144,000
Temporary Administrative Offices (Phase 2)			5,000	
New Concert Arena/Grandstand Cover				5,500
TOTAL New		72,000	77,000	149,500
TOTAL Existing and New	154,545	194,255	182,085	237,060
Notes				
1. Totals are cumulative and include prior phases				
2. The Exposition Hall replaces existing Expo Hall and concourse restrooms; also adds lobby, circulation, kitchen, and meeting rooms. In Phase 2, existing Admin offices would be demolished to provide North Fair parking; if not provided in Phase 1 Expo Hall, Admin office would be housed in portables until Expo Hall expansion in Phase 3 provides permanent admin space.				

Building areas depicted here are conceptual only.

Figure 4.11: Fair Illustrative Plan - Phase 1

Building areas depicted here are conceptual only.

Figure 4.12: Fair Illustrative Plan – Phase 3/Buildout

Figure 4.13: Aerial View– Phase 1

Building areas depicted here are conceptual only.

Figure 4.14: Aerial View – Phase 3/Buildout

4.3.4 Exposition Hall

As part of Phase 1a, the Plan proposes to replace the existing Expo Hall with a new Exposition Hall that offers 48,600 net square feet of exhibition space in a flexible, highly marketable venue integrated with the existing fair concourse and other facilities. This flexible space can be subdivided in logical increments, as described below, in order to accommodate a wide range of events including conventions, consumer shows, festivals, large parties, and other special events.

In addition to exhibition space, the Exposition Hall provides support space for lobbies, circulation, meeting rooms, kitchen, storage of movable wall panels, and restrooms for a total of 72,000 square feet.

Figure 4.15 to 4.19 illustrate the layout and architectural concepts for this important event building, which is envisioned as follows.

The following descriptions refer to the initial building proposed for construction in Phase 1a and anticipated to serve the Fair through Phase 2. Possible expansion in Phase 3 will approximately double this space and also provide for office space for Fair Administration and Security services.

Building Concept

Conceptual design for the Exposition Hall represents a functional, economical and flexible building design that also provides an architecturally distinct and compelling landmark facility for the Plan Area. In addition to its style and massing, a range of contemporary building materials were selected to reflect a forward-looking vision for the “Fair of the Future”. The conceptual design for the Exposition Hall includes the following key elements:

- In addition to serving as interior circulation and gathering spaces, the entry lobby and lounge areas (located on the south side of the building) have been organized to open directly onto a covered exterior terrace and multi-purpose lawn/event space, with views and direct access to the water feature beyond.
- The simple, yet geometrically expressive roof shape of the main Exposition Hall provides an iconic and easily identified building element within the overall site. With its inclined roof surfaces—reminiscent of the hillsides that surround the site—and exposed wall surfaces at both the east and west ends, the building’s height and orientation provide a highly visible signage/graphic opportunity when viewed from both SR-37 and I-80.
- The conceptual design embodies a commitment to environmental responsibility, and sustainable goals and practices through proposals for a variety of material selections, features, and elements (see below).

Central Exposition Space

- Nominally, a 270' long by 180' wide (48,600 net square feet), column-free exposition space for each phase, with 30 feet clear to the underside of the structural grid above.
- The space will likely be constructed as a system of steel columns and roof trusses at 15 feet on center, which will clear span the entire (180 feet) width of the hall.
- The interior layout for each phase accommodates the following program functionalities.
 - Up to 235 vendor booths, (at 10' x 10' each)
 - Approximately 1,823 people for banquet-type events, (assuming 20 s.f./person)
 - Approximately 3,645 people for live concerts and shows, (assuming 10 s.f./person)
- Movable, full-height wall panels allow the main space to be subdivided into multiple configurations and a broad range of sizes, including: 48,600; 32,400; 16,200; 10,800; 8,100; and 5,400 square foot options.
- Windows provide natural daylight at upper levels of exterior walls, and along east elevation of building, which can be fully blacked out (with movable drapes).
- The floor finish will be natural concrete, with painted interior gypsum board walls, with painted roof trusses and metal deck ceiling/roof.
- Electrical power will be provided at: the perimeter of the main space; the upper level grid/catwalk; and distributed locations across the floor (via floor boxes).
- Provisions will be made to accommodate audio/visual presentations in any of the various room configurations. Room lighting controls will be integrated with the A/V presentation systems.
- A system of catwalks (accessed by an interior caged ladder) will be provided at the bottom chord of roof trusses, to accommodate special event lighting and rigging systems (by others).
- HVAC and lighting systems will be separately zoned and controlled to accommodate the various room configurations.
- Event load-in and load-out will be achieved through on-grade access doors (including standard and high-bay doors) distributed around the perimeter of the building.

Entry Lobby/Café/Lobbies

These areas serve as the primary arrival/entrance point to the facility. The Entry Lobby has been positioned to be easily viewed from the main Entry Road and Arrival Plaza, yet can be easily accessed from secondary entry points. Features include:

- Two exterior walls of the Entry Lobby will be fully glazed to bring natural light into the building interior.
- Interior finishes will include either a carpet tile or quarry tile floor; painted gypsum board or wood paneled accent walls; and a decorative wood slat ceiling below acoustically absorptive materials.

- Secondary Lobbies and Corridors will be finished in a similar manner, and will include glass doors and windows, and a system of movable glass walls to open Lobby spaces directly to the exterior.
- A small café has been located along one wall of the Entry Lobby, to provide snacks and beverages to visitors.

Meeting Rooms

Four break-out meeting rooms have been provided with movable wall partition systems, allowing a variety of room sizes and configurations to serve larger and smaller group needs. Features include:

- Each Meeting Room will be provided with separately controlled lighting and audio/visual presentation systems
- Interior finish materials will include: carpet tile floors; painted gypsum board walls; and suspended acoustical tile ceilings (+12' high), which accommodate fluorescent room and display/accent lighting.
- Natural daylight will be provided through a glazed exterior wall system, (including provisions for drapes to fully black-out the room during presentations), with doors to access a landscaped exterior patio/garden.

Kitchen

The plan provides space for an approximately 1,800 s.f. commercial grade kitchen in the northeast corner of the building, immediately adjacent to the main Exhibition Hall, (and future Phase III expansion). The Kitchen, as currently sized, will be able to prepare and serve sit down meals to approximately 350-500 diners, in one or more of the exhibition halls or meeting rooms.

To serve larger events, the Kitchen will be optimized to also function as a "catering kitchen" (with food preparation/cooking done off-site, and delivery in warming ovens). For such events, plating and set up will likely need to be provided in temporary exterior space, or utilize a portion of one of the sub-divided exhibition halls.

Features include:

- Interior finishes will be commercial grade, durable and washable and able to meet stringent public health codes and sanitation standards.
- All kitchen appliances will be standard commercial grade.

Administrative Offices

In Phase 3 (or in Phase 1 or 2, if funds are available), the Fair's administrative offices should be located within the Exposition Hall to optimize operational efficiencies and enhance the market appeal of the new facility. Approximately 5,000 square feet will provide for fair management, security, and parking management, with areas for small staff meetings. Larger groups, such as the Fair Association Board, could make use of the Exposition Hall meeting rooms during non-paid events.

- If incorporated into the building in Phase 1, the administrative offices may be situated as second floor uses over the meeting rooms and hallway; this approach may be the

most cost effective as it makes use of building elements (walls and roof) already in place and requires only the addition of stairs, a one-story elevator, and flooring.

- If incorporated into the expanded Phase 3 building, the administrative offices would occupy the portion of the building designated as "Meeting Rooms" in the Phase 1 structure.
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Figure 4.15: Exposition Hall Schematic Floor Plan (Phase 1)

North Elevation

South Elevation

East Elevation

West Elevation

Figure 4.16: Exposition Hall –Elevations (Phase 3/Buildout)

Restrooms

Restrooms have been provided in strategic locations around the Exposition Hall.

Positioned on the exterior of the building, restroom entrances have been organized to allow direct access from either interior or exterior events, (and administratively controlled). The new restrooms on the north side of the building will replace the existing restrooms currently located along the concourse.

Exterior Elevations, Materials and Features

- Based on a system of pre-manufactured, insulated metal panels, exterior walls will include a variety of additional finish options (alternate colors, textures, or metal finishes; cement plaster; or stone veneer at select locations).
- Similar to the exterior walls, the main Exposition Hall roof structure will be based on a system of pre-manufactured, insulated metal panels, with a pre-finished standing seam metal roof finish.
- Lower (single-story) roofs will be designed with open-web roof trusses, metal decking, and a built-up or single-ply roofing system over rigid insulation.
- Glazing at the main and secondary entrance locations will be designed around a pre-finished (either natural or painted), aluminum storefront system. Additionally, large sections of the exterior glazing system will be designed as operable walls, to increase the inter-connection between interior and exterior spaces.
- As conceived, portions of the main Exposition Hall roof will receive photovoltaic and/or solar hot water heating panels.

- Gutters and roof drains will be also be piped to a series of landscaped "rain garden" areas, where rainwater can be collected and filtered before draining to the central water feature.

Sustainable Building Features and Goals

- The south-facing half of the Exposition Hall is proposed for installation of photovoltaic arrays and/or solar water heaters. With a total roof surface of approximately 50,000 square feet, this south-facing portion would provide an area of approximately 25,000 square feet. Additional roof areas over the entry lobby, meeting rooms, and/or south-facing shade canopy could also be utilized, depending on the results of more detailed studies in conjunction with overall energy programs for the Plan Area.
- Pre-manufactured exterior wall and ceiling panels should be selected to provide high insulation values, with metal support framing and finish surface options containing up to 85% recycled material content.
- Concrete slabs and foundations should include reinforcing steel with recycled content (typically ranging between 45% and 70%) and fly-ash, as part of a recycled waste diversion program.
- High efficiency water fixtures should be utilized to conserve water and offset high peak loads within the facility.
- To minimize the use of artificial light, south-facing yet shaded lobby/lounge spaces (as well as small meeting rooms) should have access to natural daylight through operable windows and exterior doors that open directly onto landscape areas. Additionally, skylights or light tubes should be included wherever practical.
- Operable windows should be provided at the upper (clerestory) level of the main Exposition Hall to provide natural daylight, as well as naturally ventilate the space.
- Efficient interior lighting and control systems should be provided, and occupancy sensors utilized wherever practical.

Phase 3 Expansion

Phase 3 assumes a doubling in size of the Exposition Hall from approximately 50,000 net square feet (72,000 gross square feet) to approximately 100,000 net square feet (144,000 gross square feet). If the administrative offices are already accommodated within the Phase 1 building, these uses would be accommodated. At full build out, the Exposition Hall will be a contiguous, column-free space that is sub-dividable into multiple smaller halls, as in Phase 1.

A second Entry Lobby will be "mirrored" at the opposite end of the building, to provide another primary entry point into the expanded facility. Similar in layout to Phase 1, additional lobbies, meeting rooms, restrooms, and an expansion of the Kitchen are also proposed in Phase 3.

4.3.4 Outdoor Venues

Arrival Plaza

- At the eastern terminus of Entry Road, a new Arrival Plaza at the Exposition Hall entry is envisioned for Phase 1a as a location for congregation, ticketing and entry, and a paved outdoor venue for art exhibitions, car shows, or similar events.
- The Arrival Plaza would create a flexible space incorporating movable bollards, planters, or other barriers to accommodate primarily pedestrians, but also occasional vehicles, according to the scheduled event. The width of the plaza should allow for turnaround of passenger vehicles (approximately 80-foot diameter) and drive-through of safety and service vehicles that need to access the west or south sides of the Exposition Hall, with exits to the landscape concourse.
- Portable ticket booths may be integrated into a dramatic entry element. The plaza design and ticket booth location should create spaces for pedestrian gathering and orientation both outside and inside a secured perimeter. Ticket booths may be integrated with signage, banners, and other elements celebrating the Fair of the Future.
- The Arrival Plaza would also be a suitable area for Farmer's Markets or other similar and temporary events.

Exposition Hall Gardens

- Rain gardens constructed as part of the Phase 1a and Phase 3 Exposition Hall should surround the building in order to capture, filter, and retain stormwater draining from the large roof surface. The rain gardens should be installed with suitable soil and drainage measures, and planted with species that tolerate rain garden conditions and provide visual appeal.

Midway/Event Lawn and South Concourse

- South of the Exposition Hall, a new Midway/Event Lawn of approximately four acres is proposed for Phase 1a to accommodate the midway during Fair week(s) and other major events throughout the year such as dog shows, festivals, and other activities where a turf surface is desirable. Between events, this area could serve as an extension of the Creek Park, with public access for strolling, picnicking, painting, and other passive recreation.
- The Midway/Event Lawn is intended as a simple grassy area sloping gently toward the water feature, with walks and ramps that provide accessibility. The slope should be approximately two percent in order to provide positive drainage and allow a wide range of activities.
- Mesh-reinforced turf should be used for the Midway in order to accommodate vehicles and temporary structures. A recommended surface material is reinforced turf (such as Grasspave or Advanced Pave Tech Turf) incorporating a root zone mesh or other system that provides a free draining natural grass surface with high load-bearing capability.
- The south-facing edge of the Exposition Hall is intended to include a South Concourse; this pedestrian promenade should be a minimum of 10 feet in width to

accommodate service vehicles. The promenade could include terraced steps that lead to the Event Lawn, providing a location of seating and viewing the Midway and water feature.

East Plaza

- In Phase 1, the East Plaza would provide a paved venue for outdoor events adjacent to the expanded portion of the Exposition Hall. It could also serve as a staging area and meeting place near the amphitheater.
- This area would also be suitable for art installations, either permanent or temporary.

Amphitheater

- In Phase 3, with expansion of the Exposition Hall, a new amphitheater is proposed to replace the Fair's existing 6,000-person concert venue. The new amphitheater is intended as a series of grassy terraces with concrete seat walls and steps for flexibility and visually appeal. A portion of the terraces may be designed to accommodate tables and chairs, so that the amphitheater can accommodate dinner concerts, weddings, and similar events.
- To protect the amphitheater from freeway noise, the upper areas should include berms and/or walls as suggested by Figure 4.20: Amphitheater Section.
- Mesh turf should be considered for amphitheater terraces.

Figure 4.17: Arrival Plaza Illustrative (Phase 3/Buildout Condition)

Figure 4.18: Arrival Plaza Perspective

Figure 4.19: South Lobby Perspective

Figure 4.20: Amphitheater Section

Demonstration Farm

The Demonstration Farm is envisioned for Phase 1a or 1b. Modeled after the popular Centennial Gardens in Orange County, the Demonstration Farm pays homage to Solano County's rich agricultural heritage and provides an outdoor living classroom for children and families to learn about new techniques in urban agriculture, horticulture,

composting, food preparation, healthy living and solar energy or other alternative energy technologies (for example, biofuel production).

Located at the eastern terminus of the Creek Park, the Demonstration Farm celebrates and carry forward the traditions of the Solano County Fair while allowing for exploration and year-round visits from families and school groups.

- The farm should be located close to parking areas to allow easy access for school groups, visitors and service vehicles. The farm should be secured by permanent fencing as needed for security and operations.
- The Demonstration Farm should be planted with rotating crops in all seasons to provide year-round visual interest.

4.3.5 Fairgrounds Fencing, Walls and Gates

Figure 4.21 illustrates the locations of proposed fencing and gates for the Fair of the Future.

Entries are planned for:

- North Gate at the existing concourse to serve the Exposition Hall and buildings including the satellite wagering facility and McCormack Hall.
- Main Gate at the Arrival Plaza to serve the Exposition Hall, overall Fairgrounds, Creek Park, and pedestrian traffic along the Entry Road.
- South Gate at the Creek Park to link from Shared Public Parking into the Midway and central areas.
- Farm Gate to also link from Shared Public Parking and serve school groups coming to visit the Demonstration Farm.
- Service gates at the north and south ends of the perimeter service road.
- In general, the Fairgrounds should appear open and welcoming to visitors throughout the year. A fortified, "closed for business" appearance should be avoided.

While providing an open, park-like appearance, the Fair's edges and entry points should be designed to provide flexible solutions for safety, security and controlled access to a variety of ticketed venues, with separate gates for concurrent events.

- Attractive, permanent frontage fencing of six to eight feet in height should be used along the more public and visible edges of the Fair, as defined by Figure 4.21. Such fences should be combined with landscape planting and constructed of wrought iron or similar high quality materials. Metal fences may be mounted on a low masonry wall, and/or spanning masonry piers.
- Movable barriers used at the Arrival Plaza for Fair Week and other special events should be designed to create an attractive, festive appearance. Portable ticket booths and other gateways elements should likewise be designed to be compatible with the Exposition Hall architecture and convey an image of quality befitting the Fair of the Future.
- Black, vinyl-clad chain link fencing (with matching posts) may be used to provide security and safety along the north and eastern edges of the Fair and for less visible

storage or service areas within the Fair. Evergreen hedges, flowering vines and/or trees should be planted along the base of all security fences. Security fences should be approximately ~~seven~~six feet in height or as needed for security.

- Walls may be used to accommodate grade transitions and provide informal seating areas along the water feature, amphitheater, or other areas. Walls should provide an image of permanence and quality, and may be used as locations for signage and permanent graphics.
- Plywood, un-clad chain link, barbed wire or razor wire fence are prohibited.

Building areas depicted here are conceptual only.

Figure 4.21: Fairgrounds Fencing and Gates

4.3.6 Fairgrounds Signage, Lighting and Site Furnishings

- Signage for the Fair of the Future should be designed as a comprehensive "family" of elements to:
 - announce arrival at entry gates,
 - provide schedule of current and upcoming events,
 - direct service vehicles and pedestrians to their destinations, and
 - supply information on the Fair's history and current features.
- Signage may be incorporated into gateway features such as the Arrival Plaza's turnstile/security check point.
- Signage should be considered in conjunction with other site furnishings including lighting and seating.
- All site furnishings should be selected to be low-maintenance, durable and attractive elements that harmonize with and complement the Exposition Hall architecture.
- Fairgrounds lighting fixtures should provide attractive, low-level lighting that promotes a safe environment for all users, but remains pedestrian-oriented.
- Lighting should utilize LED or other energy-efficient fixtures that provide pleasing light color.
- Materials for lighting fixtures should be durable and low maintenance. Natural finishes like bronze and nickel steel are recommended.

Figure 4.22: Site Furnishing Images

Figure 4.23: Signage Images

4.4 GUIDELINES FOR RIGHT-OF-WAY AND OTHER PUBLIC AREAS

4.4.1 Streetscape and Entries

Streetscape

- Streetscape should conform to the street sections provided in Figures 4.24 to 4.26 and the provisions of Chapter Five.
- Regularly-spaced street trees should be installed as part of roadway construction to along all new roadways to visually unify street edges, establish an identity with the Plan Area, provide a sense of visual enclosure along corridors and perimeters, and generate shade for pedestrian comfort.
- Special street sections include the following:
 - The North Loop Road includes a passenger drop-off lane along Parcel 6 in [Figure 1.2](#), northwest of the Exposition Hall Arrival Plaza. This drop-off serves visitors to the Exposition Hall and also helps to activate a small entry plaza within the Parcel 6 EMU development in [Figure 1.2](#).
 - The South Loop Road segment between the Entry Road and the bridge includes the same travel lane dimensions as the North Loop Road, with 10-foot wide monolithic sidewalks and no landscape area. Tree wells may be included in the sidewalk, but any additional landscaping would be located within the adjacent Fair or EMU parcels.
 - At the bridge itself, the South Loop Road sidewalks are 12 feet wide to serve bicycles and pedestrians. This segment does not include any street side landscape.
- Streetscapes should reflect the hierarchy and identity of the roadway system. Taller trees should define the Entry Road and Loop Road, with the most impressive tree type marking the Entry Road. Medium-sized trees may articulate the Connector Road and secondary onsite roads.
- Major streets should be planted with single species of trees to establish gracious and distinctive corridors. Trees should be used to enclose the street, create a comfortable pedestrian scale, and contribute to the identity of the street. Plant selection should consider City of Vallejo guidelines and be limited to hardy species that are drought-tolerant and will thrive in local climate and soil conditions.
- In general, street trees should at maturity be medium or large canopy trees, equal to or greater than the height of adjacent buildings. The planting pattern and species may vary at intersections to provide a flowering or contrasting tree.
- Trees should be planted between the curb and the sidewalk to protect pedestrians and reduce the scale of the street. Large street trees should be regularly spaced, typically 25 feet on center, but spacing may vary to accommodate street lights, driveways and utility boxes, or other conditions. Smaller scale trees may be spaced more closely.
- For street promenades along the Entry Road and at the pedestrian drop-off near the Arrival Plaza, trees should be provided within minimum five-foot wide tree grates.

- Parkway strips between sidewalks and the curb should be a minimum of seven feet in width, measured from sidewalk to face of curb. Parkways should be planted in low maintenance **trees**, shrubs, groundcovers or lawn, grasses or wild flowers. Plant material should be selected to be well-suited to location; for example, lawn is preferred to shrubs in areas where foot traffic is expected.
- Parkway strips should not be compacted as part of road bed preparation, or if compacted should be properly amended to support healthy root development and plant growth.
- Non-fruiting street trees species are preferred. If fruiting trees or vines are utilized, they should be located so as not to overhang sidewalks or otherwise create maintenance problems.
- Where bump-outs are provided, trees may be shifted into the enlarged planter area provided sight safety distances are maintained.
- Design of the Solano 360 public open space and street areas should create a consistent character and environment conducive to entertainment and urban activities, with a festive and colorful atmosphere.
- Site furnishings (including lighting, seating, wayfinding and waste/recycling receptacles) throughout the Plan Area should be designed and selected to establish a unified vocabulary of related forms and materials to reflect a sense of unity and identity.
- Bike lanes and pedestrian multi-use spaces will characterize the street environment in the Plan Area. As such, lighting, signalization and signage should be pedestrian-scale and should facilitate easy pedestrian and bicycle movement.
- **Seating** **Durable seating** should be provided at frequent areas throughout the Plan Area in the form of benches, movable tables and chairs and seat walls to encourage walking while providing rest opportunities.
- Low road speeds throughout the Plan Area should be defined to foster pedestrian and bicycle-friendly streets (see Section 5.2.1 for traffic calming features).

Entries and Intersections

- Roadway entries into the Solano360 Plan Area should provide a sense of arrival and celebration. The primary pedestrian and "ceremonial" entry at the Entry Road should be designed to welcome pedestrians and orient views toward the water feature. The Loop Road entries should likewise provide a strong sense of place, with clear signage indicated vehicular routes to parking areas.
- The Sage Street entry should emphasize clear signage for service vehicles, buses, and Transit/North Parking Center access.
- Entry plans should be prepared for each project entry prior to development of adjacent improvements. These plans should address landscape, pedestrian access, grading, drainage, monuments, signage, lighting and other public amenities.
- The design of the intersection of the Entry Road and Loop Road should include special features for traffic calming and pedestrian comfort. As envisioned, this stop

sign-controlled intersection will be raised six inches to alert vehicles and provide continuous, level crossings for pedestrians from the Entry Road promenade through to the Arrival Plaza.

- Other intersections along the Entry Road and Loop Road should also include traffic calming, bulb-outs to narrow the crossing distances for pedestrians, high-visibility striping, and special paving or textured crosswalks to enhance pedestrian safety. Up lighting may be considered to enhance safety at night and provide a festive atmosphere.

Figure 4.24: Entry Road Sections

Figure 4.25: North Loop Road Sections

Figure 4.26: South Loop Road Sections

4.4.2 Creek Park and Water Feature

The Creek Park is a critical project component, not only because of its ecologic and hydrologic function, but also because it will provide an important public open space and recreational amenity for visitors and future residents.

The Creek Park forms a new open space corridor through the site with waterfront promenades, picnic areas, lawn terraces, water view plazas, wetlands, and bridges. This example of sustainable design addresses drainage, flooding and water quality issues while providing an iconic feature that visually enhances the project's entries and activities within the central area.

Appendix F provides additional design criteria addressing water balance, water quality management, creation of wetlands, shoreline conditions, and shoreline safety.

Landscape and Amenity Features

- Creek Park should be a comfortable and beautiful multi-use space.
- The Creek Park should be planted with native and low-water vegetation to minimize irrigation needs.
- Plantings on flat, upland areas should vary from garden-like and decorative to more hardy species conducive to play, but requiring little maintenance.
- Pedestrian amenities within the park, including lighting, seating, wayfinding and waste/recycling receptacles should be designed and selected to establish a unified character for the park.
- The South Loop Road crossing over the water feature should be designed economically, while creating the appearance of a continuous waterway.

- A variety of edge conditions along the waterfront should be established to provide a safe and visually intriguing waterfront with opportunities for enjoyment of the water.
- Figure 4.27: Water Feature Section describes how the water feature could incorporate a wall or bulkhead in some areas, with riparian vegetation in other areas (see Appendix F for further details).

Recreation Opportunities

- The park should accommodate a wide-range of passive and active recreational uses including strolling, jogging, people watching, enjoying views, picnicking, meeting with friends, kite-flying and similar activities.
- ~~Pedal boat rental~~ **Small non-motorized watercraft rentals** could be considered as a concession in the Fairgrounds portion of the Creek Park so that visitors can interact with the park via the water feature.

Hydrological Function

Onsite stormwater will be routed through the Creek Park water feature which will discharge into an existing storm drain system and then into Lake Chabot. Offsite stormwater flows from Rindler Creek and/or Blue Rock springs will not be diverted through the onsite water feature but will continue to flow through the Fairgrounds Channel (Chapter Six provides additional detailed information).

- The water feature will capture, treat and store onsite stormwater runoff for water quality improvements and re-use (see Chapter Six).
- The minimum surface area and depth should be based on flood control and water quality requirements. The surface area is planned to be approximately 5.4 acres and the depth will be eight feet with a shallow shelf for wetland planting and safety (see Chapter Six and Appendix F for additional details).
- Sufficient freeboard should be provided between the normal water surface elevation and adjacent development, taking into account the varying types of land uses. Freeboard should be designed to accommodate fluctuations in the water elevation for water quality and flood control purposes.
- The minimum distance between shorelines should provide sufficient space for sides slopes taking into account the varying types of edge conditions. The maximum distance between shorelines should take the bridge designs into consideration. The maximum bridge span is currently planned to be no greater than 100 feet.
- Side slopes may vary depending on the edge conditions, safety considerations and liner requirements. In general, slopes should not exceed 4:1 in most locations. The bottom surface should be sloped at 2% minimum toward the middle of the water feature.

Access

- Plaza and hardscape areas along the west side of the park are associated with retail, shopping and dining uses along Entry Road and should engage pedestrian activity as follows:
 - A main plaza should be established along the north waterfront, visible from Entry

Road.

- Plaza and hardscape areas along the waterfront should provide ample room for dining and viewing.
- West Creek Park and all plaza and hardscape areas should be publically accessible, year round.
- The east portion of Creek Park is associated with the Fair of the Future programming. With the exception of facilities operated by private companies, for example a Ferris wheel, these portions of the park should be publically accessible except during major ticketed Fair events and as needed for maintenance and security of Fair facilities.

Figure 4.27: Water Feature Section

4.4.3 Fairgrounds Channel

- To the extent possible within the designated Fairgrounds Channel area as shown by Figure 3.1: Land Use Plan, the channel should be defined in a natural-appearing manner, with a meandering horizontal alignment and banks that vary in slope. If meandering or varied side slope angles are not possible within the Fairgrounds Channel area, the channel bottom should be constructed to undulate as much as is feasible, without creating undesirable ponding.
- The final design of the drainage corridor must meet the hydrological requirements for flood control and conform to the space limitations of the designated Fairgrounds Channel area.
- To increase the biotic value of the drainage channel, planting benches should be incorporated into the channel design. The banks of the creeks should be stabilized with native vegetation such as willow, and other native riparian plants adapted to the climate of Vallejo.
- Where feasible, the native tule at the bottom of the current channel may be left and will recruit naturally, as will sedges and rushes that could be planted on the channel benches. Side slopes should be planted with a variety of riparian plants adapted to the local climate; these include willows, coyote bush, wild rose, and native grasses. The overstory may be planted with larger, native trees such as sycamore and oak to provide shade and provide a visual buffer from adjacent freeways.
- Invasive species, such as arundo, tamarisk, or star thistle, should be eradicated if present along the drainage corridor.
- Preconstruction surveys should be carried out for special-status species, nesting raptors, nesting song birds and for roosting bats if mature trees will be removed along riparian area. To prevent direct take of a special-status species, under provisions of a Section 7 permit, any special-status species should be moved to a safe location or appropriately mitigated for, according to the requirements of the permitting process.

- Best Management Practices should be used to avoid siltation of the drainage channels from any onsite stormwater runoff.
- A SWPPP should be prepared specifically for the conditions of the site in compliance with the NPDES permit. Examples of BMPs include:
 - Conduct all in-channel construction activities during the regional “dry” period as approved by the RWQCB. All efforts should be made to perform all channel work potentially impacting surface waters during periods when surface water flows are at their lowest point.
 - No diversion of surface waters should occur during migration periods for special-status species.
 - The re-vegetation of banks should follow guidelines and specifications as outlined by environmental review for the Solano 360 project.
 - If creek flow is from Rindler Creek and/or Blue Rock Springs Creek is determined to be perennial, work should be conducted during the lowest flow portion of the year. Stream flow should be diverted around the work area using temporary bypass pipes, flumes, or excavated channels that temporarily re-route water around construction area(s). A qualified biologist should be present documenting the conditions and the impact of the construction activity, and assist in relocating stranded wildlife, where necessary.
 - Erosion control blankets and/or mats should be used to control erosion of banks and offer bank stabilization.
- Project construction should comply with all terms and conditions of a Streambed Alteration Agreement. Depending on the results of the Phase 1 ESA, and in coordination with the RWCQB, borrow materials should be examined for potential contaminants (e.g., mercury).
- The channel design should incorporate a walking/jogging trail as indicated in Figure 5.10: Pedestrian Circulation. To avoid adding extra width to the channel, this trail should make use of maintenance driveways if possible.

4.4.4 Transit / North Parking Center

The Plan proposes 2.2 acres for a transit/parking facility in the northwest area of site. The Transit/North Parking Center will provide bus access and parking through all phases of the project. In Phase 1, this consists of a bus stop and surface parking. Starting in Phase 2, a three-level parking garage will replace surface parking to serve commuters during the weekdays and parking for the Fair on weekends and at night.

Guidelines are as follows:

- The Transit/North Parking Center access should be from Sage Street and the North Loop Road.
- Buses, shuttles (to/from local hotels, nearby major entertainment uses and the Vallejo Ferry Terminal), taxis, Paratransit (and similar services for disabled individuals), personal electric vehicles and bicycles should be encouraged to use the Transit/North Parking Center.

- Secure bicycle parking should be provided and a bicycle repair and rental facility should also be included.
- Priority parking should be available for disabled persons and car-share services.
- Priority parking should be available for certified pure zero emission vehicles (100% battery electric and hydrogen fuel cell) and compressed natural gas (CNG) vehicles.

4.4.5 Public Parking

Public parking will be provided in parking lots and garages as shown in Figure 5.14: Land Use and Parking, and on the Entry Road.

- Parking facilities should adhere to the guidelines in Section 4.2.4: Parking Areas.
- Parking structures in Public Purpose Areas ~~are not required to~~ may incorporate retail uses or other non-parking uses at street level.
- To provide screening from public view, landscape plans for parking structures should include planting, trellises, vegetated walls or other decorative screens, both at the ground level and along vertical walls at street frontages or other public area and open space frontages.

4.4.6 Electronic Reader Boards

Electronic reader boards are planned along the freeway edges, in the locations shown on Figure 4.5: Site Relationships. These signs are intended to provide a revenue source for the Fair and include a new electronic reader board along SR-37, an upgraded electronic reader board along I-80, and two static electronic signs along I-80.

- Design and siting of electronic reader boards should not impede Fair programming or detract from the overall visual and aesthetic character of the Plan Area.
- Electronic reader boards should be oriented away from the Plan Area and toward freeways.
- Electronic reader boards should not contribute to light pollution that would affect nearby residences and should not adversely impact highway travel safety.
- Electronic reader boards must comply with any applicable federal and/or state requirements for highway-oriented signage.
-

4.5 GUIDELINES FOR PRIVATE PURPOSE AREAS

Private Purpose Areas consist of the Entertainment Mixed Use (EMU) parcels, totaling 18.8 acres, and the Entertainment Commercial (EC) parcel of 30 acres. These uses are distinct, as follows:

- EMU development is envisioned to create a connected, walkable area of family entertainment commercial (FEC) businesses and associated restaurants and retail, with buildings oriented to Entry Road, Creek Park, and North Loop Road. As the intensity of this area increases through Phases 2 and 3, development will include vertically mixed uses that contribute to a vibrant, pedestrian-oriented Public Entertainment Core.

- EC development is envisioned to be a single destination theme park or amusement park with outdoor rides and venues visible from adjacent freeways and public roads, contributing to the visibility and identity of Solano360 as an entertainment district. Should the EC area be developed as a multi-parcel, mixed-use commercial center, the land use and design provisions for EMU areas will apply.

~~Section 3.6 provides land use policies for these areas.~~

~~Section 4.2 establishes guidelines applicable to all portions~~ **4.5.1 Use of the Plan Area, including the Guidelines in Private Purpose Areas.** ~~The following guidelines address additional site and architectural~~

The Solano 360 Design Guidelines are intended to provide clarity in expectations for future design of projects in the Solano 360 Specific Plan Area. Projects found consistent under the Guidelines and other standards for EMU contained in the Specific Plan will receive expedited review and EC development approvals. The Guidelines will be utilized by the City of Vallejo as part of its review of development proposals in private purpose areas and by the County in working with properties in private purpose areas and fair related property in public purpose areas.

~~4.5.1~~

The Guidelines are written with enough specificity to facilitate and ensure the project vision is achieved, while retaining enough flexibility to account for the range of uses that may be allowed, and the anticipated multi-year build out of the project area. The Guidelines have been approved by both the County and the City as part of the overall approvals for the Specific/Master Plan. These are intended to be integrated into the development review processes set forth in the approvals and for development agreement between the City and County.

Guidelines by nature require some interpretation in implementation. Not all guidelines will be applicable in all situations. They are intended to provide guidance for facilitating compliance with the Solano360 vision as a whole. Individual guideline provisions are not intended to be standards that must be met in each and every circumstance. Substantial compliance with the overall design vision of the Solano360 project is the objective. To achieve this, compliance with Individual guidelines must be evaluated in the context of the project vision and overall design guideline package.

4.5.2 Design Review Process – Private Purpose Areas

Project proposals on private purpose areas are subject to application review processes set forth in the Vallejo Municipal Code as amended by the Specific Plan and Development. Project proposals found consistent with the Design Guidelines contained in this Chapter will receive expedited review.

4.5.3 Entertainment Mixed Use (EMU) Guidelines

Design Concept and Objectives

The intent of the Design Guidelines is to encourage new private purpose developments that will contribute to the vibrancy and success of the Solano 360 vision. The Design Guidelines do not dictate specific design themes or architectural styles, but instead outline design concepts that support the vision articulated in this Solano360 Specific Plan.

SOLANO360 CHARACTER AND DESIGN PRINCIPLES

Solano360 will have a highly unique ~~Urban Design~~

- ~~• Primary intersections, particularly those along Entry Road and Creek Park, should be reinforced with high quality landmark buildings or gateway elements to support the identity of the Plan Area. Such buildings should exhibit thoughtful, imaginative architectural design to welcome visitors and promote a pedestrian-oriented character.~~
- ~~• The Entry Road should provide an urban, pedestrian-oriented corridor of specialty shops and services, restaurants, tree-shaded sidewalks, and art illustrating the history of Vallejo and Solano County, all developed at an appealing pedestrian scale.~~

character shaped by the existing Discovery Kingdom Park, the proposed Fair of the Future, the “Main Street” type of mixed use and entertainment area, the unifying Creek Park and a future theme park. The following design principles reflect that unique character and form the basis for the Design Guidelines.

Create a Unique Place

Solano360 will be and iconic public entertainment destination. The Entertainment – Mixed Use component has the physical structure of a traditional mixed-use urban neighborhood, with a variety of uses and activities, including shops, offices, arts and entertainment venues, and residences. The Entertainment – Mixed Use area’s urban form is defined by buildings that maintain a relatively consistent framework of building facades lining a traditional pedestrian oriented street and opening onto the Creek Park plazas.

The rich visual architecture expected in Solano360 will help create an inviting environment. Individual buildings can contribute greatly to a positive experience for pedestrians with small scale, intimately-designed facades and storefronts that emphasize interaction with passersby. This interactive architecture creates opportunities for a lively streetscape environment, with public amenities, places to stroll, shop and dine.

The design of new buildings should be distinctive, while still part of the visual composition of the streetscape. Designs at the sidewalk level should highlight interaction with pedestrians. The architecture should be carefully composed, with variety in massing, changes in materials and unique details that stay in the memory of visitors and residents.

Create Connectivity and Synergy

The core of Solano360 is a unique combination of major public entertainment venues each interconnected with the other. The Entertainment – Mixed use area gains synergy through the connectivity provided for in the Solano360 Specific Plan. New buildings and developments should emphasize a pedestrian orientation to the unifying elements and linkages surrounding the Entertainment – Mixed Use component of Solano360 plan area.

Urban Design

- New developments should substantially conform to the urban form and footprint for the Entertainment – Mixed Use area as illustrated in the Solano360 Specific Plan.
- Design of buildings and outdoor spaces along Entry Road should utilize complementary color, special materials, signage, furnishings and landscaping to promote a unique identity and active commercial heart for the Plan Area.
- ~~Buildings and entries should be located primarily at the back of road rights-of-way. Where building entries are set back in courtyards, paseos, or arcades, landscape features such as vertical planting treatments, trellises, or decorative walls should define and clearly mark such openings at the street edge.~~
- To create a "restaurant row" and an attractive environment for restaurants and an active pedestrian promenade along the Entry Road, blocks that include FEC's or large retail stores are envisioned to include smaller footprint storefronts along the primary road right-of-way (see Figure 4.28: Entertainment-Mixed Use Building Prototype).
- Entries to large footprint buildings, such as FEC's or large retail stores, may be recessed, emphasized with architectural elements, or otherwise articulated to identify entry points to primary FEC uses.
- Development along North Loop Road in Phase 3 may also include large footprint buildings, but should also incorporate smaller, street-oriented retail shops with recessed entries or entries off of an interior courtyard or arcade.
- ~~All buildings should provide a clearly articulated pedestrian entrance, either via storefront, recessed storefront, arcade or courtyard, with direct pedestrian access to either North Loop Road or Entry Road.~~
- ~~Parking should be located to the rear of parcels. By Phase 3, no surface parking lots should front on either Entry Road or North Loop Road.~~
- Open spaces for recreation, gathering and visual relief should be designed to appear deliberate and not as "left over" space between buildings.
- Outdoor dining should be encouraged along sidewalks and promenades to promote street activity.

Buildings depicted here are conceptual only.

Figure 4.28: Entertainment-Mixed Use Building Prototype

- Use ground-level open space to complement retail shops, live/work units, cafes and restaurants, or other ground floor uses. Provide benches, sitting areas and other elements that allow people to linger. Use decorative railings, special paving or other design techniques to demarcate outdoor dining areas.
- Provide physical and visual connections to the public way, while using distinct pavement, landscaping, art, signage, screening or decorative fences to identify the ownership and acceptable uses of the space.
- Open space can be provided through ground-level courtyards. Office or residential courtyards at upper levels, as applicable, or rooftop decks and gardens.

Architectural Design

Buildings should reflect the vibrant, urban mixed-use nature of the Solano360 Plan Area, supporting the pedestrian character of streets and contributing to an overall identity for the project.

Site Design and Building Orientation

- Parking should be located to the rear of parcels. By Phase 3, no surface parking lots should front on either Entry Road or North Loop Road.
- New buildings and development in the Entertainment – Mixed Use core should orient primary facades toward the street edge, parallel to the sidewalk to create activity along sidewalks. Intersections should be activated by orienting uses toward corners.
- Organize sidewalks, pedestrian circulation, open spaces and entries to connect and align with surrounding pedestrian circulation patterns, paseos, plazas and pathways. Orient pedestrian pathways to connect with links to public transportation, such as bus stops and transit terminals.
- Incorporate retail entries at corners facing intersections and provide pedestrian amenities. Corners should emphasize pedestrian interaction at the sidewalk level with entries, canopies, small plazas, arcades or other architectural elements. Pedestrian entries should be accessed from the street with the greatest pedestrian intensity.
- Locations designated in the Specific Plan or these Design Guidelines as Gateways should address both streets with primary facades, and should provide space at the corner for special streetscape enhancements.

Building Design

- All buildings shall be designed to be attractive on all sides utilizing similar architectural detailing and building form concepts.
- All buildings shall be well modulated both horizontally and vertically to avoid monotonous and unattractive facades and overall form.
- Architectural interest shall be derived primarily through use of design elements that are integral to overall building form. Tack on elements should generally be avoided except if there is a specific purpose such as an overhang to shade a west facing window. A mansard roof would be an example of an architectural detail that would be a tack on and inappropriate.
- Building function should be integral to overall form.
- Exaggerated or oversized architectural detailing should be discouraged as such features are often utilized to compensate for poor overall design.

Entries and Access

- The Entry Road should provide an urban, pedestrian-oriented corridor of specialty shops and services, restaurants, tree-shaded sidewalks, and art illustrating the history of Vallejo and Solano County, all developed at an appealing pedestrian scale.

- All buildings should provide a clearly articulated pedestrian entrances, either via storefront, recessed storefront, arcade or courtyard, with direct pedestrian access to either North Loop Road or Entry Road.
- Buildings and entries should be located primarily at the back of road rights-of-way. Where building entries are set back in courtyards, paseos, or arcades, landscape features such as vertical planting treatments, trellises, or decorative walls should define and clearly mark such openings at the street edge.
- Pedestrian entries and retail shops should open directly to a public sidewalk or major pedestrian corridor. Mixed-use buildings with residential uses should be accessed through a clearly identifiable primary entryway directly from an adjacent sidewalk.
- Entries to buildings and retail shops should generally be located directly at the sidewalk level. Ramps for barrier free access should generally be located inside the building envelope and integrated into the overall design.
- ~~Alleys may provide entries to small retail shops, where conflicts between pedestrian and vehicles will be minimized.~~
- Buildings that front on both the Entry Drive and Creek Park should have appropriate design elements to take advantage of both frontages. Portions of buildings facing the Creek Park should feature elements that enable for outdoor dining and seating opportunities while the Loop Road frontage should contain elements of a downtown shop, including recessed entries and shop windows.
- Building entries at ground level shall be accentuated through use of human scale design elements in building architecture.
- Entries to shops and restaurants at ground floor level should directly access and be at sidewalk level to facilitate ADA access.
- Entries shall be clearly identifiable and highlighted through the use of sheltering elements such as canopies, awnings or inserts tucked under the second floor.
- Addresses shall be clearly identified at building entries and shall be sized and designed in accordance with a detailed sign plan for the overall project.
- Building design should carefully consider how service entries are addressed to ensure they do not detract from overall building appearance and design concept. These must be identified with preliminary design and floor plan concepts to avoid becoming an afterthought.
- In no case will these Design Guidelines supersede or negate any applicable regulations for Barrier-Free Design required by the US Government, the State of California, the City of Vallejo or other responsible authorities.

Massing, Scale and Articulation

- New buildings and developments should promote distinctive and visually interesting streetscapes through the thoughtful expression of building massing and façade design.

- The massing of buildings and the arrangement of volumes at the lower floors should visually reinforce the grid pattern of surrounding streets in the Entertainment – Mixed Use area by maintaining a street wall at the edge of the adjacent street or sidewalk area.
- Building facades should generally be of similar height and scale to facades on buildings directly across the street.
- The perceived heights of buildings are as important as the actual heights, and incorporating varying heights at the street edge will create visual interest in the streetscape. Vary the heights of the building volumes, incorporate changes of materials and rooflines, or step back upper floors.
- Consider the visual relationship with neighboring buildings. Some facade elements that may relate to adjoining buildings and should be considered include:
 - building modulation patterns
 - ground floor arcades or upper floor setbacks
 - signage bands above the storefront level
 - patterns of change in materials, colors, or finishes
 - architectural elements such as belt courses, cornices, awnings and canopies, window types and patterns
 - the alignment of storefront windows
 - transom and clerestory windows
 - window sills on upper floors
 - windows opening patterns and styles
 - roof lines and horizontal changes
- Buildings should establish continuous storefronts and courtyard openings along Entry Road and, in Phase 3, North Loop Road. Buildings should maintain a distinctive urban character with storefronts oriented to streets.
- Building frontages should contribute to an active street life by providing ample seating, gathering places, and exterior protection from sun and rain in the form of recessed walkways, awnings, canopies, or trellises along primary pedestrian traffic areas.
- Building Longer building façades longer than 200 feet should be designed to appear as more than one building, aggregated on the block with variation in massing, eave/parapet, color, material and balcony depth.
- Buildings should incorporate vertical height variety/variation to break the monotony of long un-interrupted building facades of matching height.
- Building floor plans should be designed with flexibility to accommodate changes in commercial tenants over time.
- Sun angles should be considered in the design and placement of structures to allow sunlight into deep spaces and provide for both shaded and sunlit public spaces.

Mechanical equipment Gateways and Corners

- Buildings on corner lots should orient windows and openings toward the intersection and to both public street frontages.
- Primary intersections, particularly those along Entry Road and Creek Park, should be reinforced with high quality landmark buildings or gateway elements to support the identity of the Plan Area. Such buildings should exhibit thoughtful, imaginative architectural design to welcome visitors and promote a pedestrian-oriented character.
- be hidden or screened by architectural Corner lots present special opportunities for incorporating distinctive architectural forms and details in the project. Special design treatment for Gateway locations should serve as a visual marker announcing an arrival into the Entertainment – Mixed Use area.
- The corners of buildings located at Gateway intersections designated in the Specific Plan should incorporate special architectural forms with significant visual emphasis, such as vertical towers, spires or other roof forms, with distinctive fenestration, architectural detailing and other elements that match visually emphasize the architecture of the rest massing of the building.
- Corner edges of buildings should be maintained on upper floors. Locate windows, balconies and other architectural elements near corners, and avoid blank walls or large decks that erode the corner's edge on upper floors. Incorporate distinctive canopies, roof forms and other architectural elements to emphasize the corner.

Rooflines

- Rooflines should be varied to reflect the articulation and modulation of the overall building. Unbroken horizontal rooflines should be avoided.
- Utilize roof design elements and roof shapes as part of the overall building composition and architectural expression.
- Use distinctive roof forms, profiles and cornices to provide a termination to the top of the building.
- Consider that rooflines not visible from the street level may be highly visible from a distance and have a different visual impact. Explore designs from multiple viewpoints.
- Rooflines should be integral to overall building form and design concept. False rooflines should be avoided in most situations.
- If flat rooflines are utilized, they should be articulated through use of architectural features such as articulated parapets or cornices.
- Rooflines should reflect and be integral to overall building form and function, reflecting and accentuating entries, floor plans and overall building form.

- Quality roof material shall be utilized that are attractive and durable. Tile is one example; other similar materials may also be suitable.

Architectural Details

- Utilize a variety of architectural elements to add dimensional detail to the architectural expression of the facade. Primary facades should include human-scaled details, unique material finishes and architectural elements such as:
 - Decorative masonry patterns and courses
 - Unique windows and doors
 - Cornice, trim and roofline line details
 - Detailing on the underside of projecting bay windows and other overhead projections
 - Decorative metal balconies and railings
 - Windows with special detailing
 - Decorative spandrel panels
 - Unique or custom lighting fixtures
 - Unique, artist-made building parts that are integrated into the design of the building
 - Pavers and other surface treatments that create custom patterns
 - Grates, grilles and other screening materials that incorporate artwork or decorative patterns
 - Other unique or custom features that add to the character of the overall streetscape.

Weather Protection

- Provide shade and cover for inclement weather, canopies, awnings and other weather protection to help create a sense of safety and comfort for pedestrians.
- When designed as part of the overall facade and streetscape composition, the design and detailing of weather protection will add visual interest of the streetscape.
- Arcades, awnings, canopies, recessed entries and other methods of weather protection should be designed as integral parts of the building when adjacent to sidewalk and public walkways. At a minimum, weather protection elements should be provided at retail and building entry locations.
- Single continuous canopies or other overhead weather protection that emphasizes horizontality are discouraged.
- Awnings and canopies should fit within framed openings relating to storefronts, should be consistent with the architectural style and character of the building, and should be constructed with materials, finishes and profiles that exceed the minimum physical and structural requirements.
- Awnings should fit into the openings of the building on which they attach without overlapping the opening or multiple openings. They should generally add color and serve as a transition between the storefront and the upper facade.
- Avoid a uniform awning design for multiple retailers.
- Awnings and canopies should identify a business's street frontage, and be identified as part of the tenant's image.
- Awning material should be of a woven fabric or other material that projects the natural appearance of canvas. Traditional canvas awnings are recommended. Retractable or open side awnings are preferred and vinyl awnings are prohibited. Canopies should be fabricated of durable materials such as steel, and glass.

Windows and Doors

- Wall openings should show depth of the wall, without use of flat or tacked-on window trims.
- Windows and doors should be simple in both design and placement. Use of mullions that divide window into panes of glass is encouraged.
- Building doors and windows facing street frontages should be fully functional.

Porches and Patios

- Upper level patios (either recessed or extended) or French balconies are encouraged, but should be usable and not merely decorative.

Colors and Materials

- Rich materials such as stone, brick, and wood are encouraged. Material mixture must be in accord with the simplicity of building massing.
- Brick and stone should be detailed in proper corner-turning and load-bearing proportions.

- Local materials and vendors are preferred.
- Exterior materials on primary facades should incorporate materials common to the buildings in found throughout Vallejo and convey a sense of permanence.
- At the ground floor, incorporate materials such as bronze, steel, brick or other masonry, and architectural-grade concrete that have a heavy, permanent appearance.
- Preferred facade materials include:
 - brick and stone masonry
 - pre-cast concrete lintels, sills and panels
 - stucco with a quality finish
 - wood profiles and details
 - stone (marble, granite) lintels, sills, cladding and detailing
 - ceramic and clay tiles or masonry
- Other materials that are acceptable include:
 - metal panels that are pre-finished or painted
 - metal and glass curtain wall systems when used for less than 30 percent of the facade area
 - synthetic detail profiles when covered with a stucco finish
 - concrete masonry units, except gray, and when used in limited quantities at the ground floor and designed with patterns of multiple colors and/or finishes
 - other innovative materials and new technologies that convey high-quality design and durability
- Thin materials generally do not convey high-quality and durability. At the pedestrian level avoid thin materials such as "stick-a-brick", clear-anodized aluminum windows and storefronts, and other light-weight materials and finishes.
- The following materials and finishes are generally inappropriate:
 - Coarsely finished, "rustic" materials, such as wood shakes, shingles, barn board or fir plywood
 - Indoor-outdoor carpeting ("astro-turf")
 - Corrugated or expanded metal, except as part of a design feature or detail
 - Corrugated fiberglass panels
 - Imitation masonry and stone materials or panels
 - Rough coat stucco
 - Silver or clear anodized aluminum sheets
 - Silver or clear anodized aluminum extrusions for windows, doorways and storefronts

- Plastic molded imitations of a conventional building material
- Mirrored or metallic reflective glass
- Glass block, except as a limited part of a design feature or detail
- To avoid the appearance of a false facade, materials and finishes should return around comers and terminate with an architectural detail or relief.
- Avoid colors that contrast dramatically with the colors of neighboring buildings. Neon and other bright colors should be avoided, except when used in a very limited amount as part of an architectural detail or feature.
- The grade of finishes should be highest at the pedestrian level of buildings. Textures should generally be more fine-grained and smooth in ground floor areas. In areas of building facades with little or no human activity, materials may be less highly-finished.

Lighting and Signage for Buildings

- Materials for lighting and signage fixtures should be durable and weather well.
- Natural finishes like bronze, nickel steel and sustainably-treated wood are recommended.
- Lighting and signage should be integrated into building design.
- Lighting, where appropriate for convenience and safety, should not cause light pollution or glare into adjacent properties.
- Energy-efficient LED lighting is highly encouraged.
- In addition to wall signs, pedestrian scale signage such as blade signs, awning signs, and window decal signs are encouraged throughout the project to contribute to an active, vibrant pedestrian experience. Signage that clutters pedestrian environments is discouraged.

Utilities and Mechanical Equipment

- Mechanical equipment should be hidden or screened by architectural elements that match the architecture of the rest of the building.
- Where possible, alleys or secondary streets should be utilized for access to utilities and building services access, including, but not limited to, trash/recycling storage and collection mechanical equipment servicing and fire department connections.
- Service facilities should generally be located in less visible locations. Where possible, facilities and equipment should be located within the building envelope.
- Fire Department connections, water sprinkler risers and other emergency and public works equipment should be located internally to the development. Backflow preventer devices should be located away from public streets, in a recessed location or located underground.
- Utilize landscape design, art elements or other architectural details to integrate the design of service access, utility connections or other mechanical equipment into the

overall design of the development. Consider artist-made building parts for screening if appropriate for the equipment.

- Any mechanical equipment, including when located on rooftops, should be visually screened in a manner that is integrated into the design of the building. Materials used should be finished and incorporate colors that blend with the overall building and reduce their visual impact. Plastic screens, chain link fences, and other utilitarian screens are insufficient for screening mechanical equipment.

Retail Storefronts

- The predominant length of sidewalk-level retail frontages should be storefronts, entry ways, doors, windows, and other openings that allow for a visual connection between the interior and the street environment, and for access directly from the sidewalk. Multiple entries should be incorporated where possible. Entry doors may be recessed, but storefront windows and displays should not be set back from the sidewalk. Storefront designs should be coordinated with adjacent designs to create a cohesive streetscape facade.
- Utilize traditional storefront designs. Storefronts should be individual expressions of a tenant's identity, but should create an expression that is complimentary to the downtown architectural vocabulary. National and regional tenants who have a standard, recognizable storefront design and color palette will be required to tailor their designs and colors to complement Vallejo's community identity and the Solano 360 vision.
- Storefronts should consist predominantly of transparent glass to provide views into the store, but glass should not be the exclusive material. Opaque, smoked and reflective glass should be used for accents only.
- The degree of construction detailing and finish in storefronts should generally exceed that of other parts of a building.
- Subject to approval by the applicable authority having jurisdiction, retailers may use sidewalks as a part of their presence on the street. A storefront expansion zone of approximately 2' wide may be identified along the building facades that will be available for tenants to extend their merchandising past the building facade plane.

Retail Signage and Lighting

- Retail signage should be incorporated into storefront designs, communicating a retailer's identity.
- Creative signage design is encouraged. Appropriate signage can take the form of wall-mounted signs, projecting blade signs, awning or canopies. Blade/projecting signs are appropriate for storefronts on the Entry Road and Loop Road. Signage may be incorporated into the design of canopies, marquees and awnings, where the latter are incorporated into the design of storefronts or entries. Emphasis should be placed on durable materials and quality manufacturing.
- The following types of signs should be avoided:
 - generic box signs
 - back-lit plastic and neon sign

o pole-mounted and freestanding signs for individual businesses

- Storefront facades, recessed doorways, outdoor spaces and passageways should be lighted. Lighting fixtures should generally complement the architectural expression and detailing of the building and storefront.
- Creative use of lighting may be incorporated into the architectural design of buildings to highlight feature elements, particularly at corners.
- Fixtures should be located and angled to ensure that they spotlight a retailer's merchandise and do not point toward the window or cause distracting reflections.
- Awnings and canopies may incorporate lighting with fixtures that light the sidewalk and storefront. Back-lit awnings are not allowed. Signage lighting, including flat-mounted signs, blade and banner signs, must be lit with concealed lighting or from above with down-lighting.
- Storefronts should provide for "after hour" lighting within the front floor area of stores so as to highlight goods and to contribute to pedestrian lighting. Night lighting will help animate the Solano360 public purpose area and increase pedestrian safety.
- Use fixtures and a comprehensive lighting plan that maximizes the efficiency of light sources and limits light intrusion into residential units. Pedestrian lights placed on buildings along streets and sidewalks should complement and supplement the pedestrian lighting plan of the street lighting without creating excess light or glare.

On-Site Amenities

- On primary pedestrian frontages and in open space setbacks adjacent to public pedestrian ways consider providing amenities for use by the public such as benches, fountains, planters with seating walls, art, bicycle racks, kiosks and notice boards.
- Public amenities should be designed and located to complement public streetscape improvements, and should exceed the normal levels of craftsmanship, reflecting the typical materials, finishes and colors of the building.
- Kiosks and Boards for wayfinding, public notices and information can be provided along pedestrian ways. They should be accessible and well-lighted. The design and construction should complement the design and composition of the building facade and/or other streetscape elements.
- Amenity features should be fixed in one place or attached to the building so as to be permanent.

Safety and Accessibility

- Amenity features should not create physical hazards or other issues of safety for pedestrians or drivers. They should allow for easy pedestrian access and required barrier free accessibility. Materials should be permanently fixed, durable, easily cleaned and maintained, and without sharp edges or points.

4.5.2 Entertainment Commercial (EC) Guidelines

In addition to the general guidelines provided in Section 4.2, the following guidelines are included to address the Entertainment Commercial (EC) area.

- Design of the northern portion of the EC parcel should address the Creek Park by incorporating a pedestrian gateway connected to trails and promenades along Entry Road and Creek Park. Design of venues and structures along this northern edge should create appealing, festive views for visitors traveling southbound on Fairgrounds Drive.
- Because development of the Entertainment Commercial Area may not occur until Phase 2, landscape treatment of the northern edge adjacent to the Creek Walk should occur with initial phases of the project.
- EC entries should be reinforced with high quality, highly visible landmark structures or gateway elements to support the identity of the Plan Area as an entertainment hub for Vallejo and the greater Solano County. Such elements should exhibit thoughtful, imaginative architectural design to welcome visitors.
- Any security barriers along Creek Park should consist of high quality, ornamental fencing with low vegetation that allows filtered views. Visually impermeable barriers along the Creek Park should be avoided.
- Taller rides and venues, up to 250 feet in height, should be concentrated within the central and eastern portions of the EC parcel in order to maximize visibility from I-80 and provide transitions to Fairgrounds Drive and the Creek Park. Along the EC parcel's northern, western, and southern boundaries, maximum heights should be limited to approximately 150 feet.
- Parking areas should be concentrated in the southern portion of the EC parcel, with active venues concentrated to the north along the Creek Park and the west along Fairgrounds Drive (see Section 3.6: Land Use Policies). Design of venues should consider creation of exciting views from freeways.
- EC development should incorporate locations for shuttle stops along the Loop Road.

4.6 SUSTAINABILITY AND RESOURCE MANAGEMENT

4.6.1 Solano360 Sustainable Design Attributes

The Plan incorporates sustainable design and development within the land use, transportation, infrastructure, and design provisions described in this document. The following section summarizes those measures and provides cross-references to relevant sections. In addition, this section provides "next step" measures for sustainability that can be incorporated into subsequent design proposals and project implementation.

The following measures incorporate aspects of national guidelines and standards for sustainability, including the United States Green Building Council (USGBC) Leadership in Energy & Environmental Design – Neighborhood Development (LEED-ND) rating system and the Guidelines and Performance Benchmarks identified under the Sustainable Sites Initiative (SSI).

Sustainable Site and Building Design

- *Location and Facility Reuse:* The Plan makes use of areas that have been previously developed, including significant portions of the existing Fairgrounds facilities. Approximately 87,000 square feet of existing Fair building area will be retained as well as the concourse itself (approximately 83,300 square feet.) and associated outdoor (paved and lawn) venue areas totaling over 30,000 square feet. This approach recycles previously disturbed land and reduces the need for construction of buildings and infrastructure. Reusing buildings, materials and existing paved surfaces also reduces waste, debris, and air quality impacts that would be generated during demolition.
- *Compact Development:* The Plan land use mix emphasizes the phased development of themed entertainment park and family entertainment uses, with flexibility to accommodate office and residential uses. Higher density development helps to conserve land and preserve open space and, when provided alongside a mix of uses, promotes livability, transportation efficiency and walkability.
- *Diversity of Uses:* The housing allowed in the Private Purpose Areas would be located within a quarter-mile (five minute) walk of onsite uses including shops, restaurant, entertainment and offices. As mentioned in Section 3.6.2, establishing a small grocery store onsite would deter some vehicle trips for residents and workers.
- *Open Space:* Open space areas can provide habitat, reduce urban heat island effects and allow for enhanced stormwater management. The Plan establishes a variety of open spaces that encourage walking, physical activity and time spent outdoors. New open space uses include six acres of Creek Park within Private Development Area and three acres within the Fair, two acres of Demonstration Farm, four acres of Midway/Event Lawn, one and a half acres of concert amphitheater, three acres of paved plazas and promenades, and one acre of other gardens and courtyards around the new Exposition Hall (acreages are approximate).
- *Sustainable Building Design:* The proposed conceptual design for the Exposition Hall incorporates sustainable features, such as natural ventilation and photovoltaic roof panels, that will partially enable the building to obtain LEED Silver certification or meet equivalent performance standards, as required by County General Plan policy. The Plan will comply with the Solano County General Plan requirement [and Vallejo Climate Action Plan](#) relative to energy efficiency and green construction policies, [as applicable](#).

Health and Well-Being

- *Bicycle and Pedestrian System:* In addition to the open space described above, the Plan proposes pedestrian and bicycle routes as illustrated by Figures 5.10 and 5.11. In addition, a jogging circuit is proposed along the Fairgrounds Channel. These public trails, promenades, bike lanes and paths encourage residents and visitors to get out of their cars and walk, bike or jog from destinations within and near the Plan Area.
- *Walkable Streets:* Walking is key to providing healthy and sustainable communities. The major roads (Entry Road and Loop Road) provide a minimum of 10-foot wide, tree-shaded sidewalks or multi-purpose paths on each side. Controlled intersections,

bulb-outs, and high-visibility crosswalks are provided at onsite intersections to enhance pedestrian safety; this includes the raised intersection at the Fairgrounds Arrival Plaza (see Figure 4.17).

- *Bicycle Facilities:* The Plan proposes bicycle facilities along the Entry Road and Loop Road, connecting to proposed bike lanes on Fairgrounds Drive between SR 37 and Redwood Parkway and allowing easy bike connections to onsite destinations. These facilities consist of bike lanes on Entry Road and North Loop Road, multi-purpose paths along South Loop Road, and secure bicycle parking at key activity nodes including the Fairgrounds and private purpose development (EMU and EC) parcels. The Transit/North Parking Center will also provide a secure bicycle parking area and may include other bicycle amenities such as a bicycle repair facility (see Figure 5.11: Bicycle Circulation).
- *Noise:* To the extent possible, the Plan provides buffers and provisions for onsite uses that may be particularly sensitive to noise impacts. The amphitheater, located in the eastern portion of the Fairgrounds near the I-80 freeway, is buffered by an earthen berm as shown by Figure 4.20: Amphitheater Section. Within the Fairgrounds, the amphitheater is separated from the future midway to avoid noise impacts during multiple events or Fair Week. Possible housing is restricted to the western portions of the Plan Area in order to avoid impacts from noise and air quality. Impacts by the project on offsite uses are mitigated by the distance between noise-generating uses, such as the amphitheater or midway, and sensitive offsite areas such as residential neighborhoods.
- *Equitable Site Use:* Site uses will provide economic or social benefits to the local community, with public access to recreational and civic facilities such as the Creek Park, renovated Fair of the Future and outdoor spaces, and Demonstration Farm.
- *Sustainability Awareness and Education:* The proposed Demonstration Farm provides opportunities to celebrate the historic agricultural character of the area and provide educational programming. Other environmental education programs may be provided through the Fair. Educational and interpretive signs describing restored habitat and water conveyance systems will be located throughout the Creek Park.

Water Quality and Management

- *Flood Control:* The Plan proposes removing the western and southern portions of the Plan Area from the floodplain, alleviating flooding in the offsite mobile home park to the extent possible, and improving the quality of onsite storm runoff. As described in Chapter Six, these improvements involve enlarging the Fairgrounds Channel and adding improving the existing crossing under Fairgrounds Drive.
- *Stormwater Collection and Re-use:* The new multi-purpose water feature within Creek Park will retain and improve runoff from the Plan Area, which can then be re-used onsite for irrigation. It also functions as a recreational amenity and water quality BMP (see Chapter Six). Capture and reuse is consistent with Low Impact Development practices and the San Francisco Bay Area NPDES stormwater quality permit. As described in Chapter Six, a majority of the Plan Area will be designed to drain to the Creek Park water feature for water quality treatment. Portions of the southern Plan

Area may drain to the Fairgrounds Channel depending on the storm drain system hydraulic limitations.

- *Potable Water Demand:* Capture and reuse of stormwater for irrigation within the water feature will reduce potable water demand. Use of drought-tolerant and local plant species will further reduce potable water demand (see Section 4.2.3: Landscape Plan and Guidelines). In addition, a "purple-pipe" (recycled water) system is planned within each backbone roadway (see Figure 6.3: Non-Potable Water Exhibit). The "purple-pipe" system will be installed in accordance with Title 22 standards for recycled water use in the event recycled water becomes available on a municipal scale.
- *Low Impact Design (LID):* Structural LIDs proposed by the Plan include the water feature bioswales and rain gardens to collect water from the Exposition Hall roof. Non-structure LID's include minimization of paved parking areas through creation of shared parking strategies and multi-purpose turf areas, such as the midway, that can accommodate overflow parking.
- *Wastewater:* The Plan's water reduction and conservation measures also result in reduced generation of wastewater due to recycling and reduced flows.

Chapter Six provides additional measures (see Sections 6.2.4, 6.3.4, and 6.4.4).

Transportation

- *Transit:* The Plan provides a multi-modal Transit/North Parking Center where commuters can park their vehicles and board buses bound for job centers or other destinations such as the Vallejo Ferry Terminal. Frequent local bus service will provide a better option for bringing people to the project, reducing the overall traffic impact. The Transit/North Parking Center can also be used for parking during weekend events.
- *Linked Trips:* The project is designed to include a variety of complementary venues and attractions within easy walking distance of each other, resulting in a 33% rate of linked vehicular trips and a corresponding reduction of transportation impacts.
- *Parking:* The Plan designates paved parking areas to serve development uses as the project builds out, but minimizes the extent of parking through phased and shared parking strategies and multi-purpose turf areas, such as the midway, that can accommodate overflow parking when it is not in use for outdoor events. Within the Entertainment Mixed Use areas, parking is allocated to the side and/or rear of blocks, creating more pedestrian-oriented streets. Larger surface lots will have landscape buffers at the street and channels edges and will incorporate shade trees or, as described below, solar arrays for an onsite source of renewable energy.

Energy

- *Solar Arrays at Exposition Hall:* As described in Section 4.3.3, the main Exposition Hall roof is proposed for a photovoltaic array and/or solar hot water heating panel installation of approximately 24,300 or more square feet. Other buildings and parking facilities are also available for installation of photovoltaics.

- *Natural Cooling:* The Exposition Hall incorporates a shade canopy to mitigate the effects of solar glare along the south-facing facade.

4.6.2 Next Step Sustainability Measures

In addition to the sustainable provisions embodied in the Plan as described above, additional "next step" measures are proposed for consideration during implementation of projects within the Plan Area.

Green Building

- Other green building and low impact design (LID) measures should be considered for more detailed stages of building and site design. These may include:
 - cisterns to capture rain water,
 - recycled water facilities for flushing toilets and other uses where potable water is not required,
 - high efficiency fixtures and appliances within buildings,
 - vegetated roofs and photovoltaic arrays on roofs,
 - use of recycled and locally available materials,
 - maximizing opportunities for natural shading and ventilation,
 - orientation of buildings to maximize energy efficiency and provide natural cooling and ventilation,
 - deciduous trees next to buildings and along streets to reduce ambient temperature, reduce heat gain, allow for cooler natural ventilation, and provide a more pleasant pedestrian environment,
 - deciduous trees and vines in front of south-facing walls and windows to further cool buildings by intercepting sunlight during summer months, yet allow direct sunlight during the winter,
 - green screens (metal lattices planted with vines and/or climbing flowers) to shade south- and west-facing walls to reduce interior heat gain and beautify buildings,
 - trees of appropriate heights and spreads to provide ample shade in the summer months for outdoor spaces such as patios and plazas, pedestrian walkways, roadways, and parking lots,
 - structures such as trellises and porticoes incorporated into the building/landscape edge, especially on south- and west-facing exposures, to provide shade in the summer and allow solar penetration when the sun is at a low angle in the winter,
 - landscape buffers, screens, and windrows to permit facilitate cooling by prevailing breezes in summer months and to reduce interior heat gain, and
 - site lighting minimized to reduce light pollution and minimize energy usage, using full cutoff luminaries, low-reflectance surfaces, and low-angle spotlights.
- Non-structural LID measures should be established where practical. These may include, but are not limited to, programs to monitor pavement cleaning (street sweeping), illicit discharge elimination, and parking lot design and management.

- Developer of projects within the Plan Area should be encouraged to pursue LEED certification and other green building credits and awards, as such recognition will physically and symbolically represent the sustainability values of Solano360.

Energy

The following measures are in addition to the photovoltaic arrays / solar hot water heating panels planned for the Exposition Hall roof, as described previously. All proposals should be developed in coordination with the County Operations Manager.

- A Public Private Partnership (PPP) with a solar partner may be pursued to provide some of the infrastructure costs associated with the site development. The Plan allocates extensive areas for parking, including approximately 24.7 acres for Shared Public Parking. These large-scale facilities could include photovoltaic arrays to provide onsite energy, shade for cars, cost savings and a possible revenue source (as excess energy could be sold).
- A district energy system, or cogeneration, could be evaluated to provide on-site energy and reduce building water heating and cooling requirements. The water feature in the Creek Park could be utilized to provide cooling via a heat transfer/cooling tower device for adjacent buildings.
- Photovoltaic arrays should be considered for all new and retrofitted buildings, including structures within the EMU and EC areas.
- Wind turbine and other alternative energy technologies could be incorporated into the Demonstration Farm to test and provide educational examples for families and visiting school groups.

Waste Management

- A construction waste management plan could be developed that would identify salvage, recycling or donation of construction materials.

Materials, Operations and Maintenance

- No wood from threatened tree species should be used in construction or finishing. Certified wood should be used wherever practical.
- Building and landscape materials should contain recycled content wherever practical.
- Materials that are produced and sold locally, including soils, should be used wherever practical.
- Any adhesives, sealants, paints and coatings used should be those with reduced VOC emissions.

**MITIGATION MONITORING AND REPORTING PROGRAM FOR THE
ENVIRONMENTAL IMPACT REPORT**

SOLANO360 SPECIFIC PLAN

**Mitigation Monitoring and Reporting Program
for the
Environmental Impact Report
Solano360 Specific Plan
County of Solano, California**

State Clearinghouse No. 2011092067

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Table 1: Solano360 Specific Plan Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>2. Air Quality</p> <p><i>Entertainment Area and Fairgrounds</i> MM AIR-1: The project shall exceed Title 24 energy efficiency standards by at least 15 percent.</p>	<p>Private Purpose Areas: Building specifications for each building, including the Title 24 report, shall be submitted the Project Engineer of the City of Vallejo's Building Division. Additionally a courtesy copy of the Title 24 report will be forwarded to the County's Building and Safety Services Division for comment.</p> <p>Public Purpose Areas: Building specifications for each building, including the Title 24 report, shall be submitted the Project Engineer of the County's Building and Safety Services Division. Additionally a courtesy copy of the Title 24 report will be forwarded to the City of Vallejo Building Division for comment.</p>	<p>Prior to construction activities</p>	<p>Private Purpose Areas: City of Vallejo Economic Development Building Division Official</p> <p>Public Purpose Areas: County of Solano Department of Resource Management Building Official</p>		
<p><i>Entertainment Area and Fairgrounds</i> MIM AIR-2: All construction activity: During construction</p>	<p>Private Purpose Areas: Construction</p>	<p>Prior to construction</p>	<p>Private Purpose Areas: City of</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>activities, the following air pollution control measures shall be implemented:</p> <ul style="list-style-type: none"> Exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day, or more as needed. All haul trucks transporting soil, sand, or other loose material offsite shall be covered All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited. All vehicle speeds on unpaved roads and surfaces shall be limited to 15 mph. All roadways, driveways, and sidewalks shall be paved as soon as possible. A publicly visible sign shall be posted with the telephone number and person to contact at the City of Vallejo regarding dust complaints. This person shall respond and take corrective action within 48 hours of a complaint or issue notification. The Bay Area Air Quality Management District's phone number shall also be visible to ensure compliance with applicable regulations. 	<p>specifications, including the dust control measures shall be submitted to the Project Engineer of the City of Vallejo's Public Works Department. Additionally a courtesy copy of the proposed dust control measures will be forwarded to the County's Division of Public Works for comment.</p> <p>Public Purpose Areas: Construction specifications, including the dust control measures shall be submitted to the Project Engineer of the County's Division of Public Works. Additionally a courtesy copy of the proposed dust control measures will be forwarded to the City of Vallejo's Public Works Department for comment.</p>	<p>activities</p>	<p>Vallejo Project Engineer</p> <p>Public Purpose Areas: County of Solano Project Engineer.</p>		
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM AIR-3a: During construction, the following measures shall be implemented:</p> <p>a) Use paints with a volatile organic compound (VOC) content of 100 grams per liter or lower for both interior and exterior</p>	<p>Private Purpose Areas: Prior to construction, a solvents and paints management plan will be prepared by the City of Vallejo</p>	<p>Prior to Construction Activities</p>	<p>Private Purpose Areas: City of Vallejo Economic Development Building Division Official</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>surfaces, if painted.</p> <p>b) Recycle leftover paint. Take any leftover paint to a household hazardous waste center; do not mix leftover water-based and oil-based paints.</p> <p>c) Keep lids closed on all paint containers when not in use to prevent VOC emissions and excessive odors.</p> <p>d) For water-based paints, clean up with water only. Whenever possible, do not rinse the clean-up water down the drain or pour it directly into the ground or the storm drain. Set aside the can of cleanup water and take it to the hazardous waste center (refer to www.cleanup.org).</p> <p>e) Use compliant, low-VOC cleaning solvents to clean paint application equipment.</p> <p>f) Keep all paint and solvent laden rags in sealed containers to prevent VOC emissions.</p>	<p>and forwarded to the County of Solano for comment.</p> <p>Public Purpose Areas: Prior to construction, a solvents and paints management plan will be prepared by the County of Solano and forwarded to the City of Vallejo for comment.</p>		<p>Public Purpose Areas: County of Solano Department of Resource Management Building Official.</p>		
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MIM AIR-3b: When more than five pieces of off-road diesel equipment with a horsepower greater than 70 per piece of horsepower are operating on one day, equipment greater than 70 horsepower shall meet or exceed United States Environmental Protection Agency Tier 3 off-road emissions standards.</p>	<p>Private Purpose Areas: A construction equipment management plan shall be submitted to the Project Engineer of the City of Vallejo's Building Division. Additionally a courtesy copy of the construction equipment management plan will be forwarded to the County's Building and Safety Services Division for comment.</p> <p>Public Purpose Areas: A construction equipment management plan shall be submitted</p>	<p>Prior to construction activities.</p>	<p>Private Purpose Areas: City of Vallejo Economic Development Building Division Official.</p> <p>Public Purpose Areas: County of Solano Department of Resource Management</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p><i>Entertainment Area and Fairgrounds</i> MM AIR-3c: Paving of the onsite roads shall occur prior to building construction.</p>	<p>the Project Engineer of the County's Building and Safety Services Division. Additionally a courtesy copy of the construction equipment management plan will be forwarded to the City of Vallejo Building Division for comment.</p>		Building Official.		
	<p>Private Purpose Areas: A construction schedule shall be submitted to the Project Engineer of the City of Vallejo's Building Division. Additionally a courtesy copy of the construction schedule will be forwarded to the County's Building and Safety Services Division for comment.</p>	<p>Prior to construction activities.</p>	<p>Private Purpose Areas: City of Vallejo Economic Development Building Division Official.</p>		
	<p>Public Purpose Areas: A construction schedule shall be submitted the Project Engineer of the County's Building and Safety Services Division. Additionally a courtesy copy of the construction schedule</p>		<p>Public Purpose Areas: County of Solano Department of Resource Management Building Official.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<p><i>Entertainment Area</i> MM AIR-3d: Any residential units on the project site shall not include wood-burning appliances. Natural gas fireplaces are allowed.</p>	<p>will be forwarded to the City of Vallejo Building Division for comment. Prior to construction, the final building plans shall be submitted to the Project Engineer of the City of Vallejo's Building Division. Additionally a courtesy copy of the construction schedule will be forwarded to the County's Building and Safety Services Division.</p>	<p>Prior to construction activities</p>	<p>City of Vallejo Economic Development Building Division Official</p>		
<p><i>Entertainment Area</i> MM AIR-4a: Any proposed residences shall be located at least 700 feet from the freeways. The residential units shall install high-efficiency Minimum Efficiency Reporting Value (MERV) filters of MERV 13 or better in the intake of residential ventilation systems. Heating, air conditioning and ventilation systems shall be installed with a fan unit power designed to force air through the filter. The owner/property manager shall maintain and replace filters in accordance with the manufacturer's recommendations.</p>	<p>A site plan and operational statement shall be submitted to the City of Vallejo's Planning Division. Additionally a courtesy copy of the construction schedule will be forwarded to the County's Building and Safety Services Division for comment.</p>	<p>Prior to construction activities</p>	<p>City of Vallejo Economic Development Planning Division Official</p>		
<p><i>Fairgrounds</i> MM AIR-4b: There shall no idling allowed on the site. Emergency generators are allowed on the site. Electrical hookups shall be available for vendors to avoid the use of onsite</p>	<p>An operational plan shall be submitted to the County's Building and Safety Services Division</p>	<p>Prior to construction activities</p>	<p>County of Solano Department of Resource Management Building Official</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<p>diesel-powered generators.</p>					
<p>3. Biological Resources</p> <p><i>Entertainment Area and Fairgrounds</i></p> <p>MM BIO-1a: Species-specific surveys, following established protocol, shall be conducted during the appropriate season(s) to identify whether California red-legged frogs (CRLF) or Pacific pond turtles (PPT) are present within the reaches of the creeks associated with the site. Typically, the appropriate season for California red-legged frog surveys is from May 1 to November 1, which allow surveys to be conducted with minimal disturbance of breeding frogs, eggs, or tadpoles during a period when frogs can be reliably detected. The appropriate season for Pacific pond turtle surveys is from May to August. Surveys shall include CRLF surveys conducted between January and September; and surveys for PPT adults can be performed concurrently with CRLF surveys, and shall include nesting surveys for PPT starting in June. These surveys must be completed the year prior to work occurring within the bed or banks of the creeks.</p> <p>Avoidance. To avoid impacts to CRLF and PPT, any construction conducted in or adjacent to the waterways shall be conducted after the breeding season for the species or construction may occur during the time when the creek contains its lowest flows (generally creek flows are lowest between August and October). To ensure no animals are present in the impact area, within 48-hours of construction beginning (e.g., trenching, water diversion, etc.), a qualified biologist shall conduct a preconstruction survey, and a biological monitor shall be present during construction within a water feature or within 50 feet of its banks if either species is determined to be present onsite.</p> <p>Conduct Dewatering Surveys. The biological monitor will walk the creeks after dewatering looking for CRLF and PPT. If species are encountered, they will be moved upstream to a safe</p>	<p>All Areas: The species-specific surveys for California red-legged frogs (CRLF) and Pacific pond turtles (PPT) shall be submitted to the Department of Fish and Game for review.</p> <p>A qualified biologist shall provide preconstruction survey prior to the start of ground-disturbing activities. During construction within a water feature or within 50 feet of its banks if either species is determined to be present onsite, the qualified biologist shall provide regular monitoring reports.</p> <p>The qualified biologist shall report findings from the dewatering survey and, if species are encountered, notify USFWS within 3 working days.</p>	<p>Species Surveys: The year prior to work occurring within the bed or banks of the creeks.</p> <p>Preconstruction Surveys: prior to ground-disturbing activity.</p> <p>Monitoring Reports: during ground-disturbing construction activity.</p> <p>Dewatering Survey: after dewatering and prior to earthmoving activity.</p> <p>Mimimization</p>	<p>Private Purpose Areas: City of Vallejo and/or California Department of Fish and Wildlife.</p> <p>Public Purpose Areas: County of Solano and/or California Department of Fish and Wildlife.</p>		

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<p>location. If CRLF are encountered, the USFWS will be notified within 3 working days.</p> <p>Minimization. Fine mesh fencing shall be placed between construction areas and the creek to direct CRLF, and PPT (should any be present onsite) away from the construction zone.</p> <p>All construction crews shall be trained (e.g., during a tailgate session) to ensure they are aware of any protective measures they must employ and to understand the purpose of such measures.</p> <p>Prior to disturbing any habitat occupied by CRLF, the applicant shall enter into consultation with the USFWS and obtain an incidental take permit.</p>	<p>In addition, prior to initiation of any ground disturbing activities, fine mesh fencing shall be placed between construction areas and the creek to direct CRLF, and PPT (should any be present onsite) away from the construction zone construction activities into this zone.</p> <p>The construction manager shall provide verification that all active construction crews will be trained for protective measures.</p> <p>Prior to disturbing any habitat identified as occupied by CRLF, the applicant shall enter consultation with the USFWS and obtain an incidental take permit.</p> <p>Private Purpose Areas: Additionally a courtesy copy of the reports, permits, and construction specifications will be forwarded to the</p>	<p>activities: Prior to initiation of any ground disturbing activities</p> <p>Construction crew training: prior to initiation of any ground disturbing activities.</p> <p>Consultation with USFWS: prior to initiation of any ground disturbing activities in habitat identified as occupied by CRLF.</p>			

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<p>County.</p> <p>Public Purpose Areas: Additionally a courtesy copy of the reports, permits, and construction specifications will be forwarded to the City of Vallejo.</p>					
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM BIO-1b: Migratory Birds and Raptors: A qualified biologist shall conduct a pre-construction survey for nesting migratory birds and tree-nesting raptors in all trees occurring within 500 feet of construction areas. Pre-disturbance surveys shall also be conducted prior to tree trimming or tree removal. These surveys should be conducted within 30 days of initial ground disturbance activities within the project site, if such disturbance occurs during the breeding season (February 1 to August 31).</p> <p>Avoidance. Conduct construction, tree trimming, and/or tree removal within areas supporting avian nesting habitat during the non-breeding season (September 1 to January 31).</p> <p>Minimization. If protected birds (including raptors) are detected, a construction-free buffer (appropriately sized based on species) shall be established around each active nest and monitored by a qualified biologist for the duration of the breeding season or until it is determined the young are have fledged and independent of their parents. Pre-construction avian surveys are not required during the non-breeding season, as birds are expected to abandon their roosts if disturbed by construction, tree trimming, or tree removal.</p> <p>Burrowing Owls: Surveys will be conducted in suitable</p>	<p>Private Purpose Areas: The nesting bird survey shall be submitted to the Department of Fish and Game for review if avian nesting habitat is to be removed between February 1 and August 31.</p> <p>The burrowing owl survey report shall be provided to the City of Vallejo. If burrowing owl are observed on site, the California Department of Fish and Wildlife shall be notified, and an assessment of impacts shall be prepared.</p> <p>Construction specifications,</p>	<p>Nesting bird survey: prior to the removal of avian nesting habitat between February 1 and August 31.</p> <p>Burrowing owl report: within 14 days prior to any construction for any new construction phase.</p> <p>Construction specifications: prior to the start of</p>	<p>Private Purpose Areas: City of Vallejo.</p> <p>Public Purpose Areas: County of Solano.</p>		

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<p>burrowing owl habitat, including a 500-foot buffer of the proposed work area. Because the Planning Area will be developed over an extended length of time and because of the low-quality burrowing owl habitat onsite, pre-construction burrowing owl surveys will be conducted within 14 days prior to the start of any new construction phase, regardless of the time of year. Since burrowing owls may overwinter at a site, surveys should be conducted prior to any construction planned during either the nesting season (typically February 1 until August 30) or the non-breeding season.</p> <p>The California Department of Fish and Wildlife will be notified in the event that owls occupy the Plan Area or adjacent lands. In the event that burrowing owl are observed onsite, an assessment of project related impacts and long-term conservation requirements will be conducted to determine the appropriate avoidance, minimization and/or mitigation measures based on the current site conditions. Measures may vary from passive relocation to offsite habitat compensation, depending on a number of environmental and biological factors.</p>	<p>including avoidance and minimization measures, shall be provided to the City of Vallejo.</p> <p>In addition, courtesy copies of surveys assessments and construction specifications shall be forwarded to the City of Vallejo.</p> <p>Public Purpose Areas: The nesting bird survey shall be submitted to the Department of Fish and Game for review if avian nesting habitat is to be removed between February 1 and August 31.</p> <p>The burrowing owl survey report shall be provided to the County of Solano. If burrowing owl are observed on site, the California Department of Fish and Wildlife shall be notified, and an assessment of impacts shall be prepared.</p> <p>Construction specifications,</p>	<p>any construction, tree trimming, and/or tree removal.</p>			

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<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM BIO-1c: Bat Species: Presence of bat species is not always easy to determine, as absence of evidence does not necessarily equate to evidence of absence. Nonetheless, to be prudent, the following conditions shall be implemented:</p> <ul style="list-style-type: none"> Do not remove snags or live trees without first having a qualified bat biologist (holder of Scientific Collection Permit and Memorandum of Understanding for bats with the CDFG) conduct nighttime emergence surveys for roosting bats and develop suitable strategies for tree removal. If any trees must be removed, they shall only be removed during seasons when bats are active and young are volant (March 1 to April 15; and August 1 to October 15). Daytime surveys shall be conducted for all buildings prior to being removed. It is best to conduct surveys at least 6 to 8 months prior to demolition to best understand what measures will be necessary to ensure demolition occurs when bats are active and young are volant (March 1 to April 15; and August 1 to October 15). Surveyor must have access to all parts of the structures. If bats are present, demolition of night roosts shall occur only during daylight hours. Demolition could occur between June 	<p>including avoidance and minimization measures, shall be provided to the County of Solano.</p> <p>In addition, courtesy copies of surveys assessments and construction specifications shall be forwarded to the City of Vallejo.</p>	<p>Prior to removal of snags, live trees, or buildings.</p>	<p>Private Purpose Areas: City of Vallejo.</p> <p>Public Purpose Areas: County of Solano.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	
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<p>1 and mid-late October 15. If demolition is scheduled to occur between late-October 15 and March 1, 4-foot by 8-foot sections (number of sections to be determined at time of surveys) of the roof must be removed by mid-October 15 (prior to start of hibernacula use).</p> <ul style="list-style-type: none"> If bats are present, demolition of maternity roosts shall only occur after young are volant (usually by August 15) and before start of hibernacula use (by mid-October). Demolition of known maternity roost habitat shall be conducted as follows: passive eviction of bats by a qualified biologist if possible, and if not possible, removal of windows and doors or other appropriate portions of the structure, as determined by a qualified biologist, 7 to 10 days prior to demolition. Demolition must occur during daylight hours. 	<p>Public Purpose Areas: Qualified biologist survey report shall be submitted to the County of Solano. Construction specifications, including demolition, passive eviction, and other minimization measures shall be provided to the County of Solano. Additionally a courtesy copy of the biologist survey report and construction specifications will be forwarded to the City of Vallejo.</p>			
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM BIO-2: Minimization. To minimize impacts to the riparian system associated with the Solano360 project, the following minimization measures shall be followed:</p> <ol style="list-style-type: none"> 1. Conduct all in-channel construction activities during the regional "dry" period as approved by the RWQCB, typically from April to October. All efforts will be made to perform all channel work, potentially impacting surface waters, during periods when surface water flows are at their lowest point in the channel. 2. No diversion of surface water will occur during the season when California red-legged frog (CRLF) or Pacific pond turtle (PPT) are most active (i.e., March through November), if present. 	<p>Private Purpose Areas: Construction specifications, including schedule biologist requirements, and other minimization measures, shall be provided to the City of Vallejo. Additionally a courtesy copy of the construction specifications will be forwarded to the County of Solano.</p> <p>Public Purpose Areas: Construction</p>	<p>Prior to the start of construction activity.</p>	<p>Private Purpose Areas: City of Vallejo.</p> <p>Public Purpose Areas: County of Solano.</p>	

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<p>3. In most years, portions of Rindler Creek may be perennial, and therefore it may not be possible to conduct work when no water is running in the creek. In this case, the following mitigation measures will be implemented during dewatering activities.</p> <p>A. All water diversion-related pumps will be screened with an appropriate sized mesh (no larger than 0.25 inches). Pump capacity must be sufficient for design flow.</p> <p>B. The removal of all temporary in-channel barriers will proceed in an upstream direction from a downstream location. Removal of temporary barriers should not cause flows to exceed more than two times the current flow in the construction area. Normal flows shall be restored to the affected stream immediately upon completion of work.</p> <p>C. Safely stockpile sediments outside the riparian zone to dry before disposal. Saturated sediments set aside for drying shall be inspected for sensitive species by the onsite biologist before offsite transport.</p> <p>D. Wet sediments shall be stockpiled away from the creek channel to the extent feasible. No runoff from wet sediments shall flow back into the channel.</p> <p>E. Properly size bypass pipes, if used, to prevent increases in temperature and decreases in dissolved oxygen. Bypass pipes may be avoided by creating a low-flow channel (such as sandbags or visqueen) or using other methods to isolate the work area. All bypass channels or flumes shall be sized to handle flows expected during the course of in-channel construction.</p> <p>F. When bypass flows are reintroduced to dewatered construction areas, they will be reintroduced in a non-erosive manner.</p>	<p>specifications, including schedule, biologist requirements, and other minimization measures, shall be provided to the County of Solano. Additionally a courtesy copy of the construction specifications will be forwarded to the City of Vallejo.</p>				

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<p>G. Diversion and reintroduction of water shall be done at appropriate distances upstream and downstream of the work site to minimize habitat disruption.</p> <p>H. A qualified biologist shall be present to mark sensitive areas, to monitor the impact of the construction activity, and to provide guidance on problem solving.</p> <p>I. All stranded fish and native aquatic vertebrates will be relocated under the direction of a qualified biologist.</p> <p>J. Implement surface water monitoring and reporting protocols identified in permits the USACE 404 permit and CDFG streambed alteration agreement will be required to confirm compliance with State and Federal water quality standards.</p> <p>4. Bank stabilization after channel work is complete shall be completed. Such methods may include:</p> <p>A. Erosion Control Blankets and Mats - Erosion control blankets (ECBs) and soil stabilization mats (turf reinforcement mats TRMs) can shall be applied to problem areas to supplement revegetation during its initial establishment. Blankets and matting surfaces temporarily stabilize and protect disturbed soil and enhance water infiltration, decrease compaction and soil crusting, and conserve soil moisture. These temporary surfaces also protect seeds from predators, and reduce desiccation and evaporation by insulating the soil and seed environment. ECBs and TRMs shall be used on drainage channels where water velocities between 3 and 6 feet per second (ft/sec) are likely to wash out new vegetation.</p> <p>Some types of ECBs and TRMs are specifically designed to stabilize channelized flow areas. These blankets and mats can shall aid in the establishment of vegetation in</p>					

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<p>waterways and increase the maximum permissible velocity of the given channel by reinforcing the soil and vegetation to resist the forces of erosion during runoff events. Stems, roots, and rhizomes of the associated vegetation become intertwined with the mat, thereby reinforcing the vegetation and anchoring the mat. Conditions where ECBs and TRMs are appropriate may include:</p> <ul style="list-style-type: none"> • Slopes and disturbed soils where mulch must be anchored. • Critical slopes adjacent to sensitive areas such as streams and wetlands. • Disturbed soil areas where planting is likely to be slow in providing adequate protective cover. • Channels with flow exceeding 2 to 4 ft/sec. • In channels intended to be vegetated and where the design flow exceeds the permissible velocity. Allowable velocity, with turf reinforcement mats after vegetative establishment, is up to 10 ft/sec (3 m/sec). <p>B. Hydraulic planting techniques – A method of applying erosion control materials to bare soil and establishing erosion-resistant vegetation on disturbed areas and critical slopes. By using hydraulic equipment (hydroseeders and hydromulchers) seed, soil amendments, wood fiber mulch and tackifying agents, bonded fiber matrix and liquid co-polymers shall be uniformly broadcast, as a hydraulic slurry, onto the soil. These erosion and dust control materials can shall often be applied in one operation.</p> <p>Hydraulic planting techniques are expensive, but provide the most dependable results on steep critical slopes, with limited accessibility and on which mulch must be anchored and on shallow soils which restrict the use of erosion control blankets. Hydraulic machines today are used to spray seed, tack down straw, bind the soil, seal the soil, or apply blanket-like coats of bonded fiber matrix (BFM).</p>					

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<p>C. Mulching – The most common use of mulch or plant debris is to provide temporary stabilization of soil, usually until permanent-stabilizing vegetation is established. Where mulches are used to complement vegetation establishment, they should be designed and installed to maximize contact with the ground and last as long as it takes to establish vegetation. On steep slopes, greater than 2.5:1, or where the mulch is susceptible to movement by wind or water, the material should be appropriately anchored. On small sites, where plant material is distributed by hand, it can shall be anchored by hand punching it into the soil every 1 to 2 feet (0.3 to 0.6 meters) with a dull, round nosed shovel. Mulching effectively complements surface roughening applications.</p>					
<p>D. Coconut Coir Matting or Tackified Hydroseeding Compounds – Fiber rolls that consist of straw that is wrapped in tubular black plastic netting is prohibited. These rolls are used extensively in the construction industry due to their cost-effectiveness. However, plastic monofilament or similar material containing netting are known to entangle or trap amphibians. If installed correctly, coconut coir matting or tackified hydroseeding compounds will capture and keep sediment and minimize sheet and rill erosion until permanent vegetation can be established. Installed, these materials shorten the slope length, thereby interrupting the erosion processes. Organic matter and native seeds are trapped behind the erosion control material, which provides a stable medium for germination.</p> <p>It is imperative, especially on steeper slopes, that a sufficient installation and monitoring be implemented so that the erosion control material will function properly, runoff will not scour underneath it, and trees or shrubs planted behind the roll will have a stable environment in which to become established. The effective control life of the chosen material is an important factor when planning</p>					

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<p>the optimum length of time the slope or construction site will need mechanical stabilization.</p> <p>E. Compost Blankets and Berms – Compost blankets are usually used on slopes of 2:1 or less, and shall be used on slopes up to 1:1, with consideration given to the length of slope and depth of application. Compost blankets should not be applied in areas of concentrated flow, and shall be used in conjunction with compost berms. Adding components such as a tackifier, or using compost blankets in conjunction with other techniques can increase the allowable steepness of the slope to be treated. Compost blankets should be extended 3 to 6 feet over the top shoulder of the slope to prevent water from getting underneath. Compost blankets can be more effective than ECBs, because they come in better contact with the underlying soil, reducing the chance of rill formation.</p> <p>5. Controlling Sedimentation. If treatment of the diverted flow or dewatered groundwater is determined necessary based on the flow present or other contributing factors, the installation and removal of temporary sediment control measures will be employed. The following is a range of measures that would be suitable for use to control sediments. These include temporary sediment basins, compost or continuous berms, and bioretention basins. The specific sediment control device shall be determined during the permit acquisition process with the appropriate regulatory agency (USACE, RWQCB, and/or CDFG). Also, sedimentation control devices may also be listed in the Storm Water Pollution Prevention Plan (SWPPP), which may also be required for this project prior to obtaining a grading permit from the County. Each of these sediment control measures are described more thoroughly below.</p> <p>A. Temporary Sediment Basins – A temporary sediment basin is a pond created by excavation in construction of an embankment and designed to retain or detain runoff</p>					

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<p>sufficiently to allow excess sediment to settle. The temporary sediment basin is intended to collect and store sediment from sites that are cleared and/or graded during construction or for extended periods of time before permanent vegetation is re-established or before permanent drainage structures are completed. It is intended to trap sediment before it leaves the construction site. The basin is temporary, with a design life of 12 to 18 months, and is to be maintained until the site area is permanently stabilized.</p> <p>Basins should be located at the stormwater outlet from the site, not in any natural or undisturbed stream. Use of temporary dikes, pipes, and/or channels may be necessary to divert runoff from disturbed areas into the basin and to divert runoff originating from undisturbed areas around the basin. Sediment basins can trap 70 to 80 percent of the sediment, which flows into them if designed and constructed appropriately. This design requires a runoff detention time of 24 to 40 hours and is only practically effective in removing sediment down to the medium silt size fraction. Sediment-laden runoff with smaller size fractions, fine silts and clay, will likely pass untreated through the basin. For this reason, basins modified with a “skimmer” device can increase efficiency and reduce turbidity by skimming relatively clear water from the top.</p> <p>There are inherent problems associated with constructing basins large enough to pond all the sediment-laden runoff long enough to allow all of the fine soil particles to settle out. Therefore, sediment basins must be used in conjunction with other erosion control practices in order to increase effectiveness and trap efficiently. These other concurrent practices include:</p> <ul style="list-style-type: none"> • Temporary seeding and/or mulching • Minimizing disturbance • Scheduling construction operations 					

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<ul style="list-style-type: none"> • Diversions to reduce runoff into the basin • Frequent use of other, smaller erosion control structures that will capture sediment upslope • Frequent inspection and maintenance of all practices <p>B. Compost/Continuous Berms – A compost filter berm is a trapezoidal berm that intercepts sheet flow and ponds runoff, allowing sediment to fall out of suspension, and often filtering sediment as well. Compost binds heavy metals and can break hydrocarbons down into carbon, salts, and other benign compounds. Compost is organic, biodegradable, renewable, and can be left onsite. This is particularly important near streams. Compost does not generally leach nutrients. Standard specifications for compost berms have been developed by the American Association of State Highway and Transportation Officials (AASHTO).</p> <p>Compost berms are more cost-effective than many other erosion/sediment control methods. The invention of the blower truck makes compost an easy to install and reliable method of sediment and erosion control. Most municipal programs are now generating compost as municipal greenwaste programs, thus making it readily available in most areas.</p> <p>C. Bioretention Basins – Bioretention basins direct sheet flow across a grass buffer strip to a ponding area for infiltration. They utilize soils and both woody and herbaceous plants to remove pollutants from stormwater runoff (EPA, 1999). The ponding area generally consists of a surface layer containing organics such as mulch, trees, grasses and shrubs, a subsurface layer of planting soil, and a sand bed.</p> <p>Bioretention areas are used to treat stormwater runoff from impervious surfaces in commercial, residential, and industrial developments, but can be just as effective in</p>					

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<p>treating runoff from intensively managed open spaces, such as parks, golf courses, or gardens. Bioretention ponds shall be used to filter stormwater prior to discharge to a storm drain or sewer system or as an infiltration device with no outflow. By virtue of the intended purpose (e.g. pollutant removal), the vegetative growth should be routinely maintained via mechanical treatments (e.g. mowed) to remove the various pollutants that have been assimilated by the plant mass. The plant debris should be properly disposed of at a local landfill.</p>					
<p><i>Entertainment Area and Fairgrounds</i> MM BIO-3a: Prior to the commencement of construction, the applicant shall obtain a Section 404 permit from the USACE for any areas under their jurisdiction. Loss of wetland habitat within the project boundaries shall be mitigated by the applicant's purchase of credits at an agency-approved mitigation bank within the region, or similar available mitigation purchase or habitat creation. The requirements of the 404 permit will be incorporated into the project design. A typical mitigation requirement for impacts to wetland features is a no-net loss of wetlands, which is associated with a minimum of a 1:1 mitigation ratio. This again is similar to the discussion of riparian habitat mitigation mentioned above, is directly related to the habitat function and value of the wetlands that will be impacted. For higher quality habitat, a 2:1 or 3:1 mitigation ratio may be required. Ultimately, it is the regulatory agencies that make the final decision during the permitting process. The proposed project will likely restore the existing drainage features on site to accommodate more flows, allowing for an increase in wetland creation following project construction. Therefore, it is anticipated that project related wetlands will increase based on restoration efforts associated with the realignment and restoration of Rindler Creek.</p>	<p>Private Purpose Areas: Obtain the Section 404 permit from the USACE for any areas under their jurisdiction. Additionally a courtesy copy of the Section 404 permit will be forwarded to the County.</p> <p>Public Purpose Areas: Obtain the Section 404 from the USACE for any areas under their jurisdiction. Additionally a courtesy copy of the Section 404 permit will be forwarded to the City of Vallejo.</p>	<p>Prior to the start of construction.</p>	<p>Private Purpose Areas: City of Vallejo.</p> <p>Public Purpose Areas: County of Solano.</p>		
<p><i>Entertainment Area and Fairgrounds</i></p>	<p>Private Purpose Areas: Obtain the</p>	<p>Prior to the start of</p>	<p>Private Purpose Areas: City of</p>		

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<p>MM BIO-3b: Proposed project activities that affect jurisdictional features will require a Section 401 Water Quality Certification from the RWQCB. Requirements of the permit will be incorporated into the project design. Potential mitigation measures associated with the 401 Water Quality Certification often includes Best Management Practices that specifically target water quality issues both before and after project construction. Many of these measures are previously described in MM BIO-2. In addition to erosion control measures, the 401 Water Quality Certification also requires BMPs such as silt fence, slope breakers, straw bales, and other energy dissipating devices to reduce erosion and sediment transport to downstream areas. Also included in the 401 permit will be construction specific requirements for refueling, spill prevention, and other precautionary measures to reduce off-site pollution.</p>	<p>Section 401 permit from the RWQCB for any areas under their jurisdiction. Additionally a courtesy copy of the Section 401 permit will be forwarded to the County.</p> <p>Public Purpose Areas: Obtain the Section 401 permit from the RWQCB for any areas under their jurisdiction. Additionally a courtesy copy of the Section 401 permit will be forwarded to the City of Vallejo.</p>	<p>construction.</p>	<p>Vallejo.</p> <p>Public Purpose Areas: County of Solano.</p>		
<p>4. Cultural Resources</p>					
<p><i>Entertainment Area</i></p>					
<p>MM CUL-1a: Because the buildings and structures appear to be historically significant, prior to demolition of any of the existing buildings or structures, they shall be evaluated for historic significance and eligibility for listing (under criteria A, B, C, and D) on the California Register of Historical Resources (CR) or local registers. Each of the buildings that are determined to be historically significant shall be recorded on appropriate Department of Parks and Recreation (DPR) 523 forms. As detailed in MM CUL-1b, inadvertent discovery measures for cultural resources shall be included in all construction contracts.</p>	<p>Historical significance report shall be provided to the City of Vallejo. Additionally a courtesy copy of the historical significance report will be forwarded to the County.</p>	<p>Prior to demolition of any existing building or structures.</p>	<p>City of Vallejo</p>		
<p><i>Fairgrounds</i></p>	<p>Historical significance evaluation shall be</p>	<p>Prior to demolition of any existing</p>	<p>County of Solano</p>		

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<p>MM CUL-1b: Because the structures appear to be historically significant, prior to demolition of any of the existing structures, they structures shall be evaluated for historic significance by a qualified archaeologist or an architectural historian to determine if they are eligible for listing on the CR (under criteria A, B, C, and D) or local registers. Each of the structures that are determined to be historically significant shall be recorded on appropriate Department of Parks and Recreation (DPR) 523 forms. A determination of eligibility may result in the need for additional archival research and/or further documentation.</p> <p>If potentially significant cultural resources are encountered during grading activities for the project, all construction activities within a 50-foot radius of the find shall cease until a qualified archaeologist determines whether the resource requires further study. A standard inadvertent discovery clause shall be included in every construction contract to inform contractors of this requirement. Any previously undiscovered resources found during construction shall be evaluated for significance by a qualified archaeologist and recorded on appropriate DPR forms. Potentially significant cultural resources consist of, but are not limited to stone, bone, glass, ceramics, fossils, wood, or shell artifacts, or features including hearths, structural remains, or historic dumpsites. If the resource is determined significant under CEQA, the qualified archaeologist shall prepare and implement a research design and archaeological data recovery plan that will capture those categories of data for which the resource is significant.</p> <p>The types of procedures that are typically included in a research design and data recovery plan include but are not limited to:</p> <ul style="list-style-type: none"> • A pre-construction sensitivity meeting with construction and management personnel. • Data recovery excavation units, as required, with the goal of addressing research issues from resources discovered during the fieldwork including local manifestations of regional chronology, subsistence, settlement, and exchange. Specific 	<p>provided to the County. Additionally a courtesy copy of the historical significance report will be forwarded to the City of Vallejo.</p>	<p>building or structures.</p>			

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<p>research questions to be addressed include temporal placement of the archaeological materials, site formation processes, subsistence, flaked stone technology, settlement patterns, and exchange and interaction systems.</p> <ul style="list-style-type: none"> Field and laboratory analysis methodology would include as appropriate: <ul style="list-style-type: none"> Initial processing, photography, faunal and lithic artifact analysis, and cataloging of artifacts. Construction monitoring, if required. Inadvertent discovery procedures for features/artifacts and human remains. Archaeological data recovery report would be prepared detailing the findings of the procedures listed above. <p>The archaeologist shall also conduct appropriate technical analyses, prepare a comprehensive report, file it with the appropriate Information Center, and provide for the permanent curation of the recovered materials.</p>					
<p><i>Entertainment Area</i></p> <p>MM CUL-2a: If potentially significant cultural resources are encountered during grading activities for the project, all construction activities within a 50-foot radius of the find shall cease until a qualified archaeologist determines whether the resource requires further study. A standard inadvertent discovery clause shall be included in every construction contract to inform contractors of this requirement. Any previously undiscovered resources found during construction shall be evaluated for significance by a qualified archaeologist and recorded on appropriate DPR forms. If the resource is determined significant under CEQA, the qualified archaeologist shall prepare and implement a research design and archaeological data recovery plan that will capture those categories of data for which the resource is significant. The archaeologist shall also conduct appropriate technical analyses, prepare a comprehensive report, file it with the appropriate Information Center, and provide for</p>	<p>Inspection of log sheets to verify cultural resource monitoring is being properly conducted, if required. Additionally a courtesy notification of cultural resources discovery will be forwarded to the County.</p>	<p>During subsurface earthwork and construction activities.</p>	<p>City of Vacaville</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>the permanent curation of the recovered materials.</p> <p>The types of procedures that are typically included in a research design and data recovery plan include but are not limited to:</p> <ul style="list-style-type: none"> • A pre-construction sensitivity meeting with construction and management personnel. • Data recovery excavation units, as required, with the goal of addressing research issues from resources discovered during the fieldwork including local manifestations of regional chronology, subsistence, settlement, and exchange. Specific research questions to be addressed include temporal placement of the archaeological materials, site formation processes, subsistence, flaked stone technology, settlement patterns, and exchange and interaction systems. • Field and laboratory analysis methodology would include as appropriate: <ul style="list-style-type: none"> - Initial processing, photography, faunal and lithic artifact analysis, and cataloging of artifacts. - Construction monitoring, if required. - Inadvertent discovery procedures for features/artifacts and human remains. • Archaeological data recovery report would be prepared detailing the findings of the procedures listed above. 					
<p><i>Fairgrounds</i></p> <p>MM CUL-2b: If potentially significant cultural resources are encountered during grading activities for the project, all construction activities within a 50-foot radius of the find shall cease until a qualified archaeologist determines whether the resource requires further study. A standard inadvertent discovery clause shall be included in every construction contract to inform contractors of this requirement. Any previously undiscovered cultural resources found during construction shall be evaluated for significance by a qualified archaeologist and recorded on appropriate DPR forms. Potentially significant cultural resources</p>	<p>Inspection of log sheets to verify cultural resource monitoring is being properly conducted, if required. Additionally a courtesy notification of cultural resources discovery will be forwarded to the City of Vallejo.</p>	<p>During subsurface earthwork and construction activities.</p>	<p>County of Solano</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>consist of, but are not limited to stone, bone, glass, ceramics, fossils, wood, or shell artifacts, or features including hearths, structural remains, or historic dumpsites. If the resource is determined significant under CEQA, the qualified archaeologist shall prepare and implement a research design and archaeological data recovery plan that will capture those categories of data for which the resource is significant. The archaeologist shall also conduct appropriate technical analyses, prepare a comprehensive report, file it with the appropriate Information Center, and provide for the permanent curation of the recovered materials.</p>					
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM CUL-3: In the event a fossil is discovered during construction for the proposed project, excavations within 50-feet of the find shall be stopped until the discovery is examined by a qualified paleontologist, in accordance with Society of Vertebrate Paleontology standards. A standard inadvertent discovery clause in every construction contract to inform contractors of this requirement. The paleontologist shall make recommendations of the procedures to be followed before construction is allowed to resume at the location of the find. If the find is determined to be significant and it is determined that avoidance is not feasible, the paleontologist shall develop a Paleontological Mitigation Plan and carry out a data recovery plan consistent with the Society of Vertebrate Paleontology standards.</p> <p>A paleontologic mitigation monitoring program would be developed by a qualified paleontologist that may include but is not limited to:</p> <ul style="list-style-type: none"> • Full-time monitoring of excavation activities below 10 feet. Paleontologic monitors would be equipped to salvage fossils, as they are unearthed, to avoid construction delays, and to remove samples of sediments likely to contain the remains of small fossil invertebrates and vertebrates. Monitors would be empowered to temporarily halt or divert equipment to allow 	<p>Private Purpose Areas: Inspection of log sheets to verify paleontological resource monitoring is being properly conducted, if required. Additionally a courtesy notification of fossil discovery and copy of the paleontologic mitigation monitoring program will be forwarded to the County.</p>	<p>During subsurface earthwork and construction activities.</p>	<p>Private Purpose Areas: City of Vallejo.</p>		
	<p>Public Purpose Areas: Inspection of log sheets to verify paleontological</p>		<p>Public Purpose Areas: County of Solano.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>removal of abundant or large specimens.</p> <ul style="list-style-type: none"> Preparation of recovered specimens to a point of identification and permanent preservation, including washing of sediments to recover small invertebrates and vertebrates. Preparation and stabilization of all recovered fossils are essential to fully mitigate adverse impacts to the resources. Identification and curation of specimens into an established, accredited museum repository with permanent retrievable paleontologic storage. These procedures are also essential steps in effective paleontologic mitigation and CEQA compliance. The paleontologist must have a written repository agreement in hand prior to the initiation of mitigation activities. Mitigation of adverse impacts to significant paleontologic resources is not complete until such curation into an established museum repository has been fully completed and documented. Preparation of a report of findings with an appended itemized inventory of specimens. The report and inventory, when submitted to the appropriate lead agency along with confirmation of the curation of recovered specimens into an established, accredited museum repository, will signify completion of the 	<p>resource monitoring is being properly conducted, if required. Additionally a courtesy notification of fossil discovery and copy of the paleontologic mitigation monitoring program will be forwarded to the City of Vallejo.</p>				
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM CUL-4: In the event of the accidental discovery or recognition of any human remains, CEQA Guidelines §Section 15064.5; Health and Safety Code §Section 7050.5; Public Resources Code §Section 5097.94 and §Section 5097.98 must be followed. In this instance, once project-related earthmoving begins and if there is accidental discovery or recognition of any human remains, the following steps shall be taken:</p> <ol style="list-style-type: none"> There shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent human remains until the County Coroner is contacted to determine if the remains are Native American and if an investigation of the cause of death is required. If the coroner determines the remains to be Native American, 	<p>Private Purpose Areas: The Solano County Coroner, the Most Likely Descendent, and a representative from the City of Vacaville shall consult and determine appropriate procedures to be followed. Additionally a courtesy notification of discovery will be forwarded to the</p>	<p>During subsurface earthwork and construction activities.</p>	<p>Private Purpose Areas: City of Vacaville and/or Solano County Coroner.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>the coroner shall contact the Native American Heritage Commission (NAHC) within 24 hours, and the NAHC shall identify the person or persons it believes to be the “most likely descendant” (MLD) of the deceased Native American. The MLD may make recommendations to the landowner or the person responsible for the excavation work within 48 hours, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods as provided in PRC Section 5097.98.</p> <p>2. Where the following conditions occur, the landowner or his authorized representative shall rebury the Native American human remains and associated grave goods with appropriate dignity either in accordance with the recommendations of the most likely descendant or on the project site in a location not subject to further subsurface disturbance:</p> <ul style="list-style-type: none"> • The NAHC is unable to identify a most likely descendant or the most likely descendant failed to make a recommendation within 48 hours after being notified by the commission. • The descendant identified fails to make a recommendation. • The landowner or his authorized representative rejects the recommendation of the descendant, and mediation by the NAHC fails to provide measures acceptable to the landowner. 	<p>County.</p> <p>Public Purpose Areas: The Solano County Coroner, the Most Likely Descendent, and a representative from the Solano County Administrators Office shall consult and determine appropriate procedures to be followed. Additionally a courtesy notification of discovery will be forwarded to the City of Vallejo.</p>		<p>Public Purpose Areas: Solano County Administrator and/or Solano County Coroner.</p>		
<p>5. Geology and Soils</p> <p><i>Entertainment Area</i></p> <p>MM GEO-1a: Prior to issuance of building permits, the project applicant shall submit a design-level geotechnical study and building plans to the City of Vallejo for review and approval. The building plans shall demonstrate that they incorporate all</p>	<p>A geotechnical study and building plans to the City of Vallejo Building Division.</p>	<p>Prior to issuance of building permits.</p>	<p>City of Vallejo Building Division.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Initial
<p>applicable recommendations of the design-level geotechnical study and comply with all applicable requirements of the most recent version of the California Building Standards Code. Recommendations from the design-level geotechnical study may include standard grading techniques such as removal and replacement and/or ground improvement methods to densify these soils in place to reduce the risk for future development. Alternatively, foundation design practices can reduce the impacts of potentially liquefiable soils. A licensed professional engineer shall prepare the plans, including those that pertain to soil engineering, structural foundations, pipeline excavation, and installation. The approved plans shall be incorporated into the proposed project. All onsite soil engineering activities shall be conducted under the supervision of a licensed Geotechnical Engineer or Certified Engineering Geologist.</p>					
<p><i>Fairgrounds</i> MM GEO-1b: Prior to commencement of site grading, the project applicant shall complete a design-level geotechnical study and building plans. The building plans shall demonstrate that they incorporate all applicable recommendations of the design-level geotechnical study and comply with all applicable requirements of the most recent version of the California Building Standards Code. Recommendations from the design-level geotechnical study may include standard grading techniques such as removal and replacement and/or ground improvement methods to densify these soils in place to reduce the risk for future development. Alternatively, foundation design practices can reduce the impacts of potentially liquefiable soils. A licensed professional engineer shall prepare the plans, including those that pertain to soil engineering, structural foundations, pipeline excavation, and installation. The approved plans shall be incorporated into the proposed project. All onsite soil engineering activities shall be conducted under the supervision of a licensed Geotechnical Engineer or Certified Engineering Geologist.</p>	<p>Geotechnical studies and building plans shall be submitted to the County of Solano.</p>	<p>Prior to commencement of site grading.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<i>Entertainment Area</i> MM GEO-2a: Implement Mitigation Measure HYD-1a.	Verification of implementation of Mitigation Measure HYD-1a	Prior to the issuance of grading permits or building permits (whichever occurs first).	City of Vallejo Economic Development Building Division Official		
<i>Fairgrounds</i> MM GEO-2b: Implement Mitigation Measure HYD-1b.	Verification of implementation of Mitigation Measure HYD-1b	Prior to the commencement of project grading or construction (whichever occurs first).	County of Solano Department of Resource Management Building Official.		
<i>Entertainment Area</i> MM GEO-3a: Implement Mitigation Measure GEO-1a.	Verification of implementation of Mitigation Measure GEO-1a	Prior to issuance of building permits.	City of Vallejo Building Division.		
<i>Fairgrounds</i> MM GEO-3b: Implement Mitigation Measure GEO-1b.	Verification of implementation of Mitigation Measure GEO-1b	Prior to commencement of site grading.	County of Solano Department of Resource Management Building Official.		
<i>Entertainment Area</i> MM GEO-4a: Implement Mitigation Measure GEO-1a.	Verification of implementation of Mitigation Measure GEO-1a	Prior to issuance of building permits.	City of Vallejo Building Division.		
<i>Fairgrounds</i> MM GEO-4b: Implement Mitigation Measure GEO-1b.	Verification of implementation of Mitigation Measure GEO-1b	Prior to commencement of site grading.	County of Solano Department of Resource Management Building Official.		
6. Greenhouse Gas Emissions					

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Initial
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM GHG-2a: To be consistent with the City of Vallejo Climate Action Plan, the project shall incorporate the following measures:</p> <ul style="list-style-type: none"> • Install indoor real-time energy monitors in each unit or tenant space. • Provide information to prospective buyers or tenants on available rebates for appliances with smart grid technology. (See PG&E's SmartMeter Program) • Comply with minimum Title 24 requirements for cool roofs to have a minimum solar reflectance index (SRI) of 10 for steep slope and 64 for low slope roofs. • Reduce exterior heat gain by planting vegetation, installing solar panel shade structures, or utilizing paving materials with a minimum SRI of 29 for at least 50 percent of non-roof impervious site surfaces. • Install and maintain street trees in compliance with current development standards. • Utilize high albedo paving material when required to install or renovate sidewalks, roads, crosswalks, parking lots, and driveways. • Pre-wire and pre-plumb new residential and commercial buildings for solar and solar thermal installations. • Provide bicycle support facilities at a rate of 1 changing room and shower per 200 occupants within non-residential developments. • Provide bike racks for 5 percent of the projected building occupants within 200 feet of the building entrance and one long-term bicycle storage space per two-multi-family units. • Design parking lots to include clearly marked and shaded pedestrian pathways between existing and planned transit facilities and building entrances. • Encourage employers and employees to utilize the Solano Transit Authority's rideshare matching system and support services. • Include designated stalls for low-emitting, fuel efficient vehicles and carpool/vanpool vehicles for a minimum of 8 	<p>Public Purpose Areas: Building and facility specifications shall be submitted to the City of Vallejo.</p> <p>Private Purpose Areas: Building and facility specifications shall be submitted to the County of Solano.</p>	<p>Prior to the commencement of project grading or construction (whichever occurs first).</p>	<p>Public Purpose Areas: City of Vallejo Economic Development Building Division Official.</p> <p>Private Purpose Areas: County of Solano Department of Resource Management Building Official.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>percent of total non-residential parking capacity and pre-wire stalls for future electric vehicle charging stations for 2 percent of total parking capacity. The stalls shall be in preferred parking locations and shall be marked with signs.</p> <ul style="list-style-type: none"> • Install individual water meters for each residential unit and high water use commercial uses such as restaurants or laundromats. • Provide an additional water meter or sub-meter for landscaping uses for all new non-residential facilities with 1,000 to 5,000 square feet of irrigated landscaped space. • Comply with the City of Vallejo's Construction/Demolition Waste Reuse and Recycling Ordinance. • Incorporate recycled content materials for a minimum of 10 percent of total materials. • Install outdoor electrical outlets on the exterior of each building in an accessible location. • Require construction contractors to shut construction equipment off when not in use or reduce the maximum idling time to 5 minutes or less. • Construction contractors shall maintain construction equipment per manufacturer's specifications. • Substitute electrified equipment for diesel- and gasoline-powered equipment where practical. 					
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM GHG-2b: To be consistent with the County of Solano Climate Action Plan, the project shall incorporate the following measures:</p> <ul style="list-style-type: none"> • The residential units shall be LEED certified units or meet equivalent performance standards. For new affordable housing projects, performance standards shall be established pursuant to the requirements of the funding source(s). • Buildings over 10,000 square feet in size shall incorporate renewable energy generation to provide the maximum feasible amount of the project's energy needs. Commercial buildings shall incorporate renewable energy generation to provide at least 20 percent of the project's needs. 	<p>Public Purpose Areas: Building and facility specifications shall be submitted to the City of Vallejo.</p> <p>Private Purpose Areas: Building and facility specifications shall be submitted to the County of Solano.</p>	<p>Prior to the commencement of project grading or construction (whichever occurs first).</p>	<p>Public Purpose Areas: City of Vallejo Economic Development Building Division Official.</p> <p>Private Purpose Areas: County of Solano Department of Resource Management</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<ul style="list-style-type: none"> Energy Star rated appliances and the most energy-efficient Energy Star rated water heaters and air conditioning systems that are feasible shall be installed in the new residential units. New buildings over 10,000 square feet in size shall achieve LEED certification, or meet equivalent performance standards. Require the design and orientation of all buildings to maximize passive solar heating during cool seasons, avoid solar heat gain during hot periods, enhance natural ventilation, and promote effective use of daylight. Any new shuttles that are used in the project or to shuttle people to the adjacent theme park shall use electricity, natural gas, or hybrid-electric technology. All buildings shall have space in the design for adequate recycling, composting, and yard waste collection. During demolition of the existing structures prior to construction of the project, at the time of permit application, the project shall submit a plan to the County that outlines methods to maximize reuse of building materials. The project shall recycle or reuse a minimum of 50 percent of unused or leftover building materials. 			Building Official.		
<p>7. Hazards and Hazardous Materials</p>					
<p><i>Entertainment Area</i></p>					
<p>MM HAZ-1a: Prior to issuance of a grading permit, a soil investigation shall be completed and submitted to the City of Vallejo to determine if the aboveground and underground gasoline storage tanks have leaked and if there are remnant soil impacts. The soil investigation shall also assess whether heavy metal or hydrocarbon-impacted soils were placed as fills within the property. In the event significant soil impacts are noted, exceeding applicable Cal-EPA and/or USEPA risk criteria, a soil mitigation plan shall be developed and implemented for the property.</p>	<p>A soil investigation shall be completed and submitted to the City of Vallejo.</p>	<p>Prior to issuance of a grading permit.</p>	<p>City of Vallejo Building Division.</p>		
<p><i>Entertainment Area</i></p>					
<p>MM HAZ-1b: Prior to renovation or demolition of the existing</p>	<p>A lead and asbestos survey shall be</p>	<p>Prior to renovation or demolition of</p>	<p>City of Vallejo Building Division.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>fair buildings, a lead and asbestos survey shall be completed and submitted to the City of Vallejo. Based on the findings of the survey, a mitigation plan shall be developed for the removal of asbestos containing material or lead-based paint, as necessary in accordance with BAAQMD and CAL-OSHA requirements.</p>	<p>completed and submitted to the City of Vallejo.</p>	<p>the existing fair buildings.</p>			
<p><i>Entertainment Area</i> MM HAZ-1c: Prior to the commencement of project construction, all transformers that are no longer in use shall be collected for appropriate disposal to the satisfaction of the City of Vallejo Fairgrounds.</p>	<p>A transformer removal and disposal report shall be provided to the City of Vallejo.</p>	<p>Prior to the commencement of project construction.</p>	<p>City of Vallejo Building Division.</p>		
<p><i>Fairgrounds</i> MM HAZ-1d: Prior to commencement of site grading, a soil investigation shall be completed to determine if the aboveground and underground gasoline storage tanks have leaked and if there are residual soil impacts. The soil investigation shall also assess whether heavy metal or hydrocarbon-impacted soils were placed as fills within the property. In the event significant soil impacts are noted, exceeding applicable Cal-EPA and/or USEPA risk criteria, a soil mitigation plan shall be developed and implemented for the property.</p>	<p>A soil investigation shall be completed and submitted to the County.</p>	<p>Prior to issuance of a grading permit.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p><i>Fairgrounds</i> MM HAZ-1e: Prior to renovation or demolition of the existing fair buildings, a lead and asbestos survey shall be completed. Based on the findings of the survey, a mitigation plan shall be developed for the removal of asbestos containing material or lead-based paint, as necessary in accordance with BAAQMD and CAL-OSHA requirements.</p>	<p>A lead and asbestos survey shall be completed and submitted to the County.</p>	<p>Prior to renovation or demolition of the existing fair buildings.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p><i>Fairgrounds</i> MM HAZ-1f: Prior to the commencement of project construction, all transformers that are no longer in use shall be collected for appropriate disposal.</p>	<p>A transformer removal and disposal report shall be provided to the County.</p>	<p>Prior to the commencement of project construction.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p><i>Fairgrounds</i></p> <p>MM HAZ-1g: Prior to commencement of site grading, a groundwater investigation shall be completed to assess potential impacts to shallow groundwater from animal waste washing into the eight crushed rock-filled pits along the western side of the eastern row of horse stables as well as potential petroleum hydrocarbons impacts. In the event significant groundwater impacts are noted, exceeding applicable Cal-EPA and/or USEPA risk criteria, a groundwater management plan shall be developed and implemented for the property.</p>	<p>A groundwater investigation report shall be provided to the County.</p>	<p>Prior to commencement of site grading.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p><i>Fairgrounds</i></p> <p>MM HAZ-1h: Prior to commencement of site grading, limited soil sampling and laboratory testing shall be completed to determine if PCB-containing cooling oil was discharged to surface soils from the pole mounted transformer within the southwestern area of the property. In the event significant soil impacts are noted, exceeding applicable Cal-EPA and/or USEPA risk criteria, a soil mitigation plan shall be developed and implemented to address PCB-impacted soil.</p>	<p>Soil analysis results shall be provided to the County.</p>	<p>Prior to commencement of site grading.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p><i>Fairgrounds</i></p> <p>MM HAZ-1i: Prior to renovation or demolition of the existing shop building located within the corporation yard in the northeastern area of the property, all chemicals that are no longer in use shall be collected for appropriate disposal. Additionally, the area of surface staining shall be scarified for appropriate disposal, and confirmation soil sampling and laboratory analysis shall be conducted to determine that an appropriate amount of impacted soil has been excavated. Prior to grading, testing for persistent pesticides shall also be conducted. In the event significant pesticide impacts are noted, exceeding applicable Cal-EPA and/or USEPA risk criteria, a soil mitigation plan shall be developed and implemented for the shop building area.</p>	<p>A chemical removal and disposal report shall be provided to the County.</p>	<p>Prior to renovation or demolition of the existing shop building located within the corporation yard in the northeastern area of the property.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p><i>Entertainment Area</i></p>	<p>Verification of</p>	<p>Timing same as</p>	<p>City of Vallejo</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<p>MM HAZ-2a: Implement Mitigation Measures HAZ-1a through HAZ-1c.</p>	<p>Implementation of Mitigation Measures HAZ-1a through HAZ-1c.</p>	<p>verification of Mitigation Measures HAZ-1a through HAZ-1c.</p>	<p>Building Division.</p>		
<p><i>Fairgrounds</i> MM HAZ-2b: Implement Mitigation Measures HAZ-1d through HAZ-1i.</p>	<p>Verification of implementation of Mitigation Measures HAZ-1d through HAZ-1i.</p>	<p>Timing same as verification of Mitigation Measures HAZ-1d through HAZ-1i.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p>8. Hydrology and Water Quality</p>					
<p><i>Entertainment Area</i></p>					
<p>MM HYD-1a: Prior to the issuance of grading permits or building permits (whichever occurs first), the project applicant shall prepare and submit a Stormwater Pollution Prevention Plan (SWPPP) to the City of Vallejo that identifies specific actions and Best Management Practices (BMPs) to prevent stormwater pollution during construction activities in accordance with the revised NPDES General Permit for Stormwater Discharges Associated with Construction Activity (Order 2009-0009 DWQ). The City of Vallejo shall confirm that the RWQCB has approved the SWPPP prior to issuance of grading or building permits. The SWPPP shall identify a practical sequence for BMP implementation and maintenance, site restoration, contingency measures, responsible parties, and agency contacts. The SWPPP shall include but not be limited to the following elements:</p>					
<ul style="list-style-type: none"> • Temporary erosion control measures shall be employed for disturbed areas. • No disturbed surfaces shall be left without erosion control measures in place during the winter and spring months. • Sediment shall be retained onsite by a system of sediment basins, traps, or other appropriate measures. • The construction contractor shall prepare Standard Operating Procedures for the handling of hazardous materials on the construction site to eliminate or reduce discharge of materials 					
<p>A Stormwater Pollution Prevention Plan (SWPPP) shall be submitted to the City of Vallejo.</p>					
<p>Prior to the issuance of grading permits or building permits (whichever occurs first).</p>					
<p>City of Vallejo Economic Development Building Division Official</p>					

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<p>to storm drains.</p> <ul style="list-style-type: none"> BMP performance and effectiveness shall be determined either by visual means where applicable (e.g., observation of above-normal sediment release), or by actual water sampling in cases where verification of contaminant reduction or elimination (such as inadvertent petroleum release) is required by the RWQCB to determine adequacy of the measure. In the event of significant construction delays or delays in final landscape installation, native grasses or other appropriate vegetative cover shall be established on the construction site as soon as possible after disturbance, as an interim erosion control measure throughout the wet season. 					
<p><i>Fairgrounds</i></p> <p>MIM HYD-1b: Prior to the commencement of project grading or construction (whichever occurs first), the project applicant shall prepare and submit a Stormwater Pollution Prevention Plan (SWPPP) to the City of Vallejo that identifies specific actions and Best Management Practices (BMPs) to prevent stormwater pollution during construction activities in accordance with the revised NPDES General Permit for Stormwater Discharges Associated with Construction Activity (Order 2009-0009 DWQ). The SWPPP shall identify a practical sequence for BMP implementation and maintenance, site restoration, contingency measures, responsible parties, and agency contacts. The SWPPP shall include but not be limited to the following elements:</p> <ul style="list-style-type: none"> Temporary erosion control measures shall be employed for disturbed areas. No disturbed surfaces shall be left without erosion control measures in place during the winter and spring months. Sediment shall be retained onsite by a system of sediment basins, traps, or other appropriate measures. The construction contractor shall prepare Standard Operating Procedures for the handling of hazardous materials on the construction site to eliminate or reduce discharge of materials to storm drains. 	<p>A Stormwater Pollution Prevention Plan (SWPPP) shall be submitted to the County of Solano.</p>	<p>Prior to the commencement of project grading or construction (whichever occurs first).</p>	<p>County of Solano Department of Resource Management Building Official.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<ul style="list-style-type: none"> BMP performance and effectiveness shall be determined either by visual means where applicable (e.g., observation of above-normal sediment release), or by actual water sampling in cases where verification of contaminant reduction or elimination (such as inadvertent petroleum release) is required by the RWQCB to determine adequacy of the measure. In the event of significant construction delays or delays in final landscape installation, native grasses or other appropriate vegetative cover shall be established on the construction site as soon as possible after disturbance, as an interim erosion control measure throughout the wet season. 					
<p><i>Entertainment Area</i></p> <p>MM HYD-2a: Prior to the issuance of grading permits for the proposed project, the project applicant shall submit a stormwater quality management plan to the authority having jurisdiction for review and approval.</p>	<p>The stormwater quality management plan shall be submitted to the City of Vallejo for review and approval</p>	<p>Prior to the issuance of grading permits.</p>	<p>City of Vallejo Building Division.</p>		
<p><i>Fairgrounds</i></p> <p>MM HYD-2b: Prior to the commencement of grading for the proposed project, the project applicant shall submit a stormwater quality management plan to the authority having jurisdiction for review and approval.</p>	<p>The stormwater quality management plan shall be submitted to the County for review and approval</p>	<p>Prior to the issuance of grading permits.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p>9. Noise</p> <p><i>Entertainment Area and Fairgrounds</i></p> <p>MM NOI-1a: Stationary noise-generating construction equipment shall be placed a minimum of 275 feet from the property line of the Marriott Hotel property and a minimum of 1,550 feet from the property line of the closest existing residential property line (south of the project boundary), when and where feasible.</p>	<p>Private Use Areas: Construction specifications, including equipment staging and use, shall be provided to the City of Vallejo.</p> <p>Public Use Areas: Construction</p>	<p>Prior to issuance of grading permit or construction activity (whichever occurs first).</p>	<p>Private Use Areas: City of Vallejo Building Division.</p> <p>Public Use Areas: County of Solano Department of Resource Management</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p><i>Entertainment Area</i></p> <p>MM NOI-1b: Once precise grading and architectural plans are made available, and prior to building permit issuance, a final acoustical impact analysis shall be performed for all residential planning areas in order to confirm that exterior noise standards of 60 dBA are achieved and interior noise levels are reduced to 45 dBA or less. If the final acoustical analysis determines that noise levels are in excess of these standards, then mitigation in the form of noise barriers to reduce exteriors noise levels and/or higher STC-rated windows and doors to reduce interior noise levels may be required.</p>	<p>specifications, including equipment staging and use, shall be provided to the County.</p> <p>Final acoustical impact analysis shall be provided to the City of Vallejo.</p>	<p>Prior to building permit issuance.</p>	<p>Building Official.</p> <p>City of Vallejo Building Division.</p>		
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM NOI-4: The project applicant shall require construction contractors to adhere to the following noise attenuation requirements:</p> <ul style="list-style-type: none"> • Construction activities shall be limited to between the hours of 7:00 a.m. and 9:00 p.m. • All construction equipment shall use noise-reduction features (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer. 	<p>Private Use Areas: Construction specifications, including construction hours and noise reduction features, shall be provided to the City of Vallejo.</p> <p>Public Use Areas: Construction specifications, including construction hours and noise reduction features, shall be provided to the County.</p>	<p>Prior to issuance of grading permit or construction activity (whichever occurs first).</p>	<p>Private Use Areas: City of Vallejo Building Division.</p> <p>Public Use Areas: County of Solano Department of Resource Management Building Official.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
10. Public Services					
<i>Entertainment Area</i> MM PS-2a: Prior to issuance of the certificate of occupancy for each building, the project applicant shall prepare a security plan after consultation with the Vallejo Police Department.	A security plan shall be provided to the City of Vallejo.	Prior to issuance of the certificate of occupancy for each building.	City of Vallejo Building Division.		
<i>Fairgrounds</i> MM PS-2b: Prior to completion of construction of Phase 1 fair facilities, the project applicant shall prepare a security plan in consultation with the Vallejo Police Department and/or the Solano County Sheriff's Office.	A security plan shall be provided to the County.	Prior to completion of construction of Phase 1 fair facilities.	County of Solano Department of Resource Management Building Official.		
<i>Entertainment Area</i> MM PS-3: Prior to building permit issuance, the applicant shall pay the applicable City development impact fee consistent with the Vallejo City Unified School District planning requirements and defray the cost of increased demand for schools attributable to project implementation.	Confirmation of impact fee payment shall be provided to the City of Vallejo.	Prior to building permit issuance.	City of Vallejo Building Division.		
11. Transportation/Traffic					
<i>Entertainment Area and Fairgrounds</i> MM TRANS-1: The project will contribute funding toward the I-80 Express Lanes project for the segment south of Redwood Parkway in Vallejo, if and when the project is programmed for funding by the MTC and the STA, through traffic impact fees administered by Solano County or the City of Vallejo. Because the funding and construction of the express lanes cannot be assured, this impact remains significant and unavoidable after mitigation.	Private Use Areas: Confirmation of impact fee payment shall be provided to the City of Vallejo. Public Use Areas: Confirmation of impact fee payment shall be provided to the County.	When the I-80 Express Lanes project for the segment south of Redwood Parkway in Vallejo is programmed for funding by the MTC and STA.	Private Use Areas: City of Vallejo. Public Use Areas: County of Solano.		
<i>Entertainment Area and Fairgrounds</i> MM TRANS-2: Implement Mitigation Measure TRANS-9.	Verification of implementation of Mitigation Measure	Timing same as verification of Mitigation	Private Use Areas: City of Vallejo.		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Initial
<p><i>Entertainment Area and Fairgrounds</i></p> <p>MM TRANS-8: Refer to Mitigation Measure TRANS-1.</p>	<p>TRANS-9</p>	<p>Measure TRANS-9.</p>	<p>Public Use Areas: County of Solano.</p>		
<p><i>Entertainment Area and Fairgrounds</i></p> <p>The project will mitigate the Phase 1, 2, and 3 impacts identified above as follows:</p> <p>The project will mitigate the Phase 1, 2, and 3 impacts identified above as follows:</p> <p>Phase 1 (Option a): Contribute a proportional share toward the widening of the westbound leg of Redwood Street at Fairgrounds Drive to provide space for a dedicated right-turn lane onto Fairgrounds Drive, and re-time signal accordingly. Widening would take place west of the I-80 bridge structure. The project's proportional share of the need for this improvement is 11 percent.</p> <p>Phase 1 (Option b): Allocate mitigation funds equivalent to that described in Option (a) toward the ultimate improvements at the Fairgrounds Drive/Redwood Parkway interchange, to be held in a dedicated fund until those improvements are constructed.</p> <p>Event Management Plan to ensure that the summer weekend late morning peak hour trips do not exceed the current trip generation:</p> <ul style="list-style-type: none"> For summer weekends, May - October (when Six Flags 	<p>Verification of implementation of Mitigation Measure TRANS-1</p>	<p>Timing same as verification of Mitigation Measure TRANS-1.</p>	<p>Private Use Areas: City of Vallejo.</p> <p>Public Use Areas: County of Solano.</p> <p>County of Solano County Administrator.</p>		

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<p>Discovery Kingdom is open), the following Exposition Hall and general Fairgrounds event management plan should be followed:</p> <ol style="list-style-type: none"> 1. When Banquet Seating, Assembly Seating, or Trade Show events with estimated attendance at 75 percent or higher occupancy are scheduled on weekend days starting by 1 p.m., all other events on-site should have start times staggered by a minimum of two (2) hours (later than the Exposition Hall event start time). End times for those events should also be staggered by at least two (2) hours. 2. When Banquet, Assembly or Trade Show events with estimated attendance from 50 percent to 75 percent occupancy are scheduled on weekend days starting by 1 p.m., all other events on-site should have start times staggered by at least one (1) hour (later than the Exposition Hall event start time). End times should also be staggered by at least one (1) hour. 3. Non-seated concert events with estimated attendance at 50 percent or higher occupancy should not be scheduled to start before 1 p.m. on weekend days. 4. When non-seated concert events with estimated attendance below 50 percent are scheduled for weekend days starting by 1 p.m., all other events should have start times staggered by at least two (2) hours (later than the concert). End times should also be staggered by two (2) hours. 5. In addition to the above guidelines, when multiple venues including the Exposition Hall are scheduled on summer Saturdays and Sundays, all events should be staggered by a minimum of one (1) hour. <p>Phase 2: Contribute funds toward the construction of the Redwood Parkway/Fairgrounds Drive improvement project at the two interchanges, at a level proportional to the full project's share of total future traffic at 2035, and considering other sources of potential traffic growth not modeled in this analysis, in</p>					

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Initial
<p>particular that of Six Flags Discovery Kingdom. The project's share of total 2035 traffic, as modeled in this analysis – without any Six Flags Discovery Kingdom traffic growth—is as follows:</p> <ul style="list-style-type: none"> • At Fairgrounds Drive/SR-37 Ramps: 23 percent • At Redwood Street/I-80 Ramps: 10 percent <p>The above proportions may be subject to reduction if growth plans for Six Flags Discovery Kingdom are proposed and approved.</p> <p>The mitigation is tied to the Project's proportional share of total future traffic because the Redwood Parkway/Fairgrounds Drive Improvement Project's purpose, as defined by Caltrans and the STA, is to:</p> <ul style="list-style-type: none"> • Relieve existing congestion and improve traffic flow on the local roadway network for approved redevelopment and planned land uses in the area; • Improve the existing interchanges and intersection operations; • Improve the safety of the local roadway network by reducing congestion. <p>Thus, the project is not designed solely to serve traffic growth, but also to address existing deficiencies.</p> <p>In addition to the above Phase 2 mitigation, the retiming of intersection #8, Columbus Parkway/Admiral Callaghan Lane, is required.</p> <p>Phase 3: Adjust signal timing of intersection #1, Fairgrounds Drive/Whitney Lane.</p> <p>Because the full funding and construction of the Fairgrounds Drive/Redwood Parkway Interchange improvements cannot be assured, the impacts at intersections #2, #3, and #15 remain significant and unavoidable.</p>					

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion Date	Verification of Completion Initial
<p>12. Utilities and Service Systems</p> <p><i>Entertainment Area</i> MM USS-4a: Prior to issuance of building permits for the proposed project, the project applicant shall retain a qualified contractor to perform construction debris recycling. The applicant shall establish an objective of diverting a minimum of 50 percent of construction debris from the waste stream, as required by the 2010 California Green Building Standards Code. The project applicant shall provide documentation to the satisfaction of the City of Vallejo demonstrating that construction and demolition debris was recycled.</p>	<p>Confirmation of qualified contracting services.</p>	<p>Prior to issuance of building permits.</p>	<p>City of Vallejo Building Division.</p>		
<p><i>Entertainment Area</i> MM USS-4b: Prior to issuance of the final certificates of occupancy for the proposed project, the project applicant shall install onsite facilities necessary to collect and store recyclable materials. Recyclable collection facilities shall be located in public spaces and clearly identify accepted materials.</p>	<p>Inspection of recyclable facilities.</p>	<p>Prior to issuance of the final certificates of occupancy.</p>	<p>City of Vallejo Building Division.</p>		
<p><i>Fairgrounds</i> MM USS-4c: Prior to the commencement of construction for the proposed project, the project applicant shall retain a qualified contractor to perform construction debris recycling. The applicant shall establish an objective of diverting a minimum of 50 percent of construction debris from the waste stream, as required by the 2010 California Green Building Standards Code.</p>	<p>Confirmation of qualified contracting services.</p>	<p>Prior to the commencement of construction.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		
<p><i>Fairgrounds</i> MM USS-4d: Prior to final occupancy for the proposed project, the project applicant shall install onsite facilities necessary to collect and store recyclable materials. Recyclable collection facilities shall be located in public spaces and clearly identify accepted materials.</p>	<p>Inspection of recyclable facilities.</p>	<p>Prior to final occupancy for the proposed project.</p>	<p>County of Solano Department of Resource Management Building Official.</p>		

**PROPOSED GENERAL PLAN AMENDMENT FOR
THE SOLANO360 SPECIFIC PLAN PROJECT**

The following amendments are proposed for the Vallejo General Plan:

- (1) The land use designation for Assessor's Parcel Numbers 0052-240-550 and 0052-240-560 shall be changed from Open Space-Community Park to Commercial Recreation, and as illustrated in Exhibit B-1 to Attachment 1.
- (2) Various text changes shall be made as described below: (New text is shown in *bold italic*, deleted text shown in ~~strike-through~~.)

I. Scope and Use of the Plan

Page I-1, amend fourth paragraph to recognize Solano360 Specific Plan Area:

The existing Solano County Fairgrounds will be redeveloped as the Solano360 Specific Plan Area. References to the Solano360 Specific Plan Area in this document include both the public land uses specific to the Solano Fair of the Future and private land uses targeted for market development. Fair parcels are subject to the City of Vallejo General Plan but are owned and operated by Solano County and Solano County Fair Association.

Part II – Summary of Goals and Objectives

Page II-1, amend Urban Design Goal 1, by adding Urban Design Policy 7 to recognize the Solano360 Specific Plan:

- 7. Use a specific plan as the guide for re-use and development of the Solano County Fairgrounds. The Solano360 Specific Plan shall address and promote the development of mixed-use entertainment, mixed-use commercial and fair uses to create a thematic entertainment district that caters to specialty activities not found within the City of Vallejo or the greater region.*

Page II-2, amend Urban Design Goal 3, by adding Urban Design Policy 6:

- 6. The Solano360 Specific Plan Area will serve as a specialty entertainment and retail district, compatible with Six Flags Discovery Kingdom and the Solano County Fair. Amenities located in the Plan Area will include unique and specialty options not found within the region.*

Page II-7: Commercial Development Goals; delete Commercial Development Goal 3, Policy 2 and add new Policy 2:

- ~~*3. Consider the feasibility of tying in the office center with a convention center on the Fairgrounds property.*~~
- 2. Consider the feasibility of developing complimentary commercial uses in the Northeast Quadrant with the Solano360 Specific Plan Area.*

Page II-8, Commercial Development Goals; add new Commercial Development Goal 8 and associated policy:

Commercial Development Goal 8: To promote the use of the Solano360 Specific Plan that encourages a mix of commercial entertainment related uses that will become unique commercial assets for the City of Vallejo.

Policy

- 1. Use the Solano360 Specific Plan to guide new commercial development in the private parcels of the Solano360 Specific Plan Area in a manner that emphasizes specialty and thematic entertainment land uses.*

Page II-11, Transit Goal: To have a transit system that results in a significant increase in transit usage especially Page II-11, Transit Goal: To have a transit system that results in a significant increase in transit usage especially among commuters and better service for transit dependent residents; add new Policy 7

- 7. The City shall promote the use of the Solano360 Transit Center as a viable option for regional commuter transit use.*

Page II-12, Non-Motorized Transportation Goal: To have facilities that encourages greater use of bicycles for recreation, commuting and shopping; add new Policy 7:

- 7. The City shall integrate the existing Vallejo Bicycle Route Plan with the Solano360 bicycle route system and parking facilities as a viable option for Plan Area access.*

Part III – Land Use Element

Page III-5: B. Urban Design, add fourth bullet: 4. Solano360 Specific Plan Area and associated language.

The Solano360 Specific Plan Area will create a unique place with an unmistakable identity that serves as a destination for visitors as well as a pedestrian-friendly, community gathering places. The Solano360 Specific Plan Area will combine a mix of complementary land uses, including retail, commercial, hospitality, recreational, residential, family and youth oriented, educational and civic uses that seamlessly integrate with the “Fair of the Future”. The Specific Plan Area will generate revenues for Solano County and the City of Vallejo creating jobs and ensure long-term economic sustainability.

Page III-6: Urban Design Goal 1: To establish a strong city identity; add new Policy 6

- 6. Use the Solano360 Specific Plan to evaluate projects proposed within this area.*

Page III-6: Urban Design Goal 3: To have attractive, exciting shopping areas; add new Policy 8

- 8. The Solano360 Plan Area is designed as a destination entertainment center for specialty retail, restaurant and thematic entertainment uses.*

Page III-15, F. Commercial Development: 1. Major Commercial Areas; edit introductory paragraph; add Solano360 Specific Plan Area definition.

There are ~~six~~ **seven** major commercial sites on the Plan Map: 1) Downtown; 2) Northeast Quadrant along I-80 between Columbus and Redwood Parkways; 3) Vallejo Plaza area; 4) Tennessee Street between Mare Island Way and Tuolumne Street intersection; 5) Springs Road between I-80 and Columbus Parkway; 6) Mare Island and 7) *Solano360 Specific Plan Area.*

It is proposed that the ~~six~~ seven major commercial areas described above be delineated as follows:

SOLANO360 SPECIFIC PLAN AREA: The existing Solano County Fairgrounds will be redeveloped to include uses for the “Fair of the Future” and parcels intended for Entertainment Mixed-Use and Entertainment Commercial uses providing specialty retail and shopping experiences. These commercial uses will also be compatible with the existing Fair and Six Flags Discovery Kingdom.

Page III-19, Commercial Development Goals; edit existing Commercial Development Goal 3 Policy 2:

- ~~2. Consider the feasibility of tying in the office center with a convention center on the Fairgrounds property.~~
2. Consider the feasibility of developing complimentary commercial uses in the Northeast Quadrant with the Solano360 Specific Plan Area.

Page III-20, Commercial Development Goals; add new Commercial Development Goal 8 and associated policies:

Commercial Development Goal 8: To promote the use of the Entertainment Mixed-Use and Entertainment Commercial Parcels in the Solano360 Specific Plan Area as unique commercial assets for the City of Vallejo.

Policies

1. Use the Solano360 Specific Plan to guide new commercial development in the Solano360 Specific Plan Area in a manner that emphasizes specialty and thematic entertainment land uses.
2. Utilize the unique entertainment and thematic land use patterns adjacent to and within the Solano360 Specific Plan Area in evaluating new commercial development.
3. Develop development standards and flexible land use guidelines for commercial development in the Solano360 Specific Plan Area.

Page III-29: add a Commercial Recreation designation for the Solano360 Specific Plan Area including the land use designations and standards:

Amend the General Plan Land Use Map replacing the Open Space – Community Park designation with Commercial Recreation.

This designation is for the Solano360 Specific Plan Area:

Commercial Recreation

The purpose of the Commercial Recreation designation for the Solano360 Specific Plan Area is to create and establish regulations for a mixed use district in which recreational land uses such as the fair, as well as entertainment, commercial and/or office uses are developed as an integral unit. All uses shall complement and enhance each other and their diversity shall be unified by an overall design concept. The intent of this district is to implement the policies of the Vallejo General Plan that call for the establishment of specific areas where flexibility of design and development of diverse land use is appropriate for the benefit of the city as a whole.

Clearly compatible zoning classifications:
Mixed Use Planned Development

IV. Circulation and Transportation Element

Page IV-9, Transit Goal: To have a transit system that results in a significant increase in transit usage especially among commuters and better service for transit dependent residents; add Policy 7:

- 7. The City shall promote the Solano360 Transit Center as a viable alternative for park and ride commuter transit and as alternative transit access for visiting the Solano360 Plan Area and Six Flags Discovery Kingdom.*

XII. Action Program

Page XII-1, Action Program, B. Specific Area Plans and Special Studies; amend language as follows:

Beside the five area plans completed in the late 1970's, specific plans have been prepared for Sky Valley, Northgate, White Slough, Downtown, Mare Island, and the Solano360 Specific Plan Area.

XIII. Economic Element

Page 10, Citywide Goals and Policies, Goal 4: Increase Workforce Preparedness of Vallejo Residents, amend last paragraph to include text specific to Solano360; amend as follows:

Create Citywide First Source Hiring Program Building Upon Mare Island Program: Creation of a First Source Hiring program to prioritize and assist in hiring Vallejo citizens was called for in the Economic Vision. Developers and tenants on Mare Island are already obligated to target Vallejo residents for job openings. The City could support diverse First Source hiring programs and other programs targeting geographic, linguistic, and culturally diverse populations or create a centralized City-sponsored program that could be implemented citywide. *The City shall promote financial incentives for prospective developers of the Solano360 Specific Plan Area that support First Source hiring programs of Vallejo citizens.* Some cities have initiated this type of effort by tying it to receipt of City financial subsidies, with great success.

Page 11, Citywide Goals and Policies, Goal 5: Expand Visitor Attractions and Services, add new Policy 9.

Policy 9: Support development of recreation, specialty entertainment retail, commercial and restaurant uses in the Solano360 Specific Plan Area.

Page 12, Citywide Goals and Policies, Goal 5: Expand Visitor Attractions and Services, add new language to Implementation Strategies.

Increase Physical and Transportation Links between Key Sites, add new last sentence: *Promote public transit and bicycle access routes to the Solano360 Specific Plan Area.*

Provide Vallejo Specific Visitors Guide - Create a visitors guide that provides for one-day and multiple day tours of historic features, waterfront, and local museums (including emerging attractions on Mare Island *and in the Solano360 Specific Plan Area*). Police and visitor docents can distribute these, as well as shops display them.

Page 23, Goals and Policies for Focused Page 23, Goals and Policies for Focused Economic Activity Areas, Goal 10: **Develop North Vallejo as a Premier Visitor and Resident Gateway**, edit Policy 2 to include language specific to Solano360 Plan Area.

Policy 2: Encourage and support year-round utilization of the fairgrounds property ~~Solano360 Specific Plan Area~~ to enhance visitor attraction, support a gateway image, and provide highest economic return to Vallejo.

Page 23, Goals and Policies for Focused Economic Activity Areas, Goal 10: **Develop North Vallejo as a Premier Visitor and Resident Gateway**, edit Policy 2 to include language specific to Solano360 Plan Area:

Policy 2: Encourage and support year-round utilization of the fairgrounds property ~~Solano360 Specific Plan Area~~ to enhance visitor attraction, support a gateway image, and provide highest economic return to Vallejo.

Page 23, Goals and Policies for Focused Economic Activity Areas, Goal 10: **Develop North Vallejo as a Premier Visitor and Resident Gateway**, edit language in third paragraph under Background section:

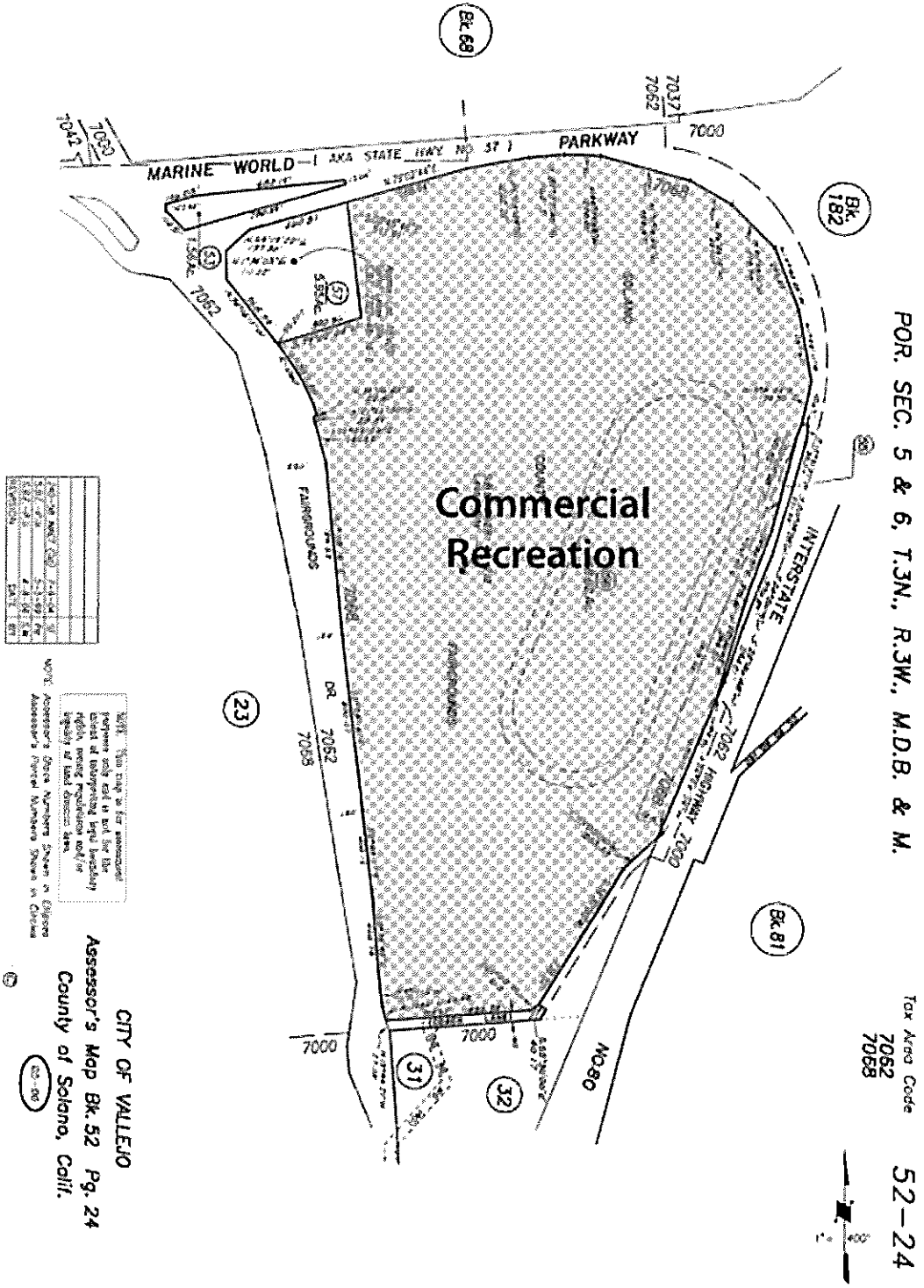
The Solano County Fairgrounds ~~is moving forward with the preparation of a master plan for~~ will be redeveloped *as part of the Solano360 Specific Plan Area reuse* and economic self-sufficiency, with a preferred alternative being developed. ~~The existing Fairgrounds will undergo a phased renovation and development of new facilities and structures to achieve economic self-sufficiency.~~ The City, through its land use jurisdiction over the site, has communicated its preference for maximizing economic return to Vallejo and its residents will *collaborate with Solano County to assist in the phased redevelopment of private portions of the Fairgrounds site in efforts to maximize the economic return to Vallejo and its residents.*

Page 24, Goals and Policies for Focused Economic Activity Areas, Goal 10: **Develop North Vallejo as a Premier Visitor and Resident Gateway**, edit first bullet under Potential Implementation Strategies section:

Potential Implementation Strategies

The City has key roles to play in directing development in North Vallejo, including:

- Active partnership with *Solano County and the* Solano County Fair Board and private developers to ~~reuse~~ *redevelop* the fairgrounds *as the Solano360 Specific Pan.*



POR. SEC. 5 & 6, T.3N., R.3W., M.D.B. & M.

Tax Area Code
7062
7068

52-24

Parcel No.	Area (Ac.)	Assessed Value	Prop. Value
7052	1.00	100,000	100,000
7053	1.00	100,000	100,000
7054	1.00	100,000	100,000
7055	1.00	100,000	100,000
7056	1.00	100,000	100,000
7057	1.00	100,000	100,000
7058	1.00	100,000	100,000
7059	1.00	100,000	100,000
7060	1.00	100,000	100,000
7061	1.00	100,000	100,000
7062	1.00	100,000	100,000
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7088	1.00	100,000	100,000
7089	1.00	100,000	100,000
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7091	1.00	100,000	100,000
7092	1.00	100,000	100,000
7093	1.00	100,000	100,000
7094	1.00	100,000	100,000
7095	1.00	100,000	100,000
7096	1.00	100,000	100,000
7097	1.00	100,000	100,000
7098	1.00	100,000	100,000
7099	1.00	100,000	100,000
7100	1.00	100,000	100,000

NOTE: This map is for assessment purposes only and is not for the purpose of determining legal boundary rights, zoning, or other matters. Assessor's Office Numbers Shown in Circles

CITY OF VALLEJO
Assessor's Map Bk. 52 Pg. 24
County of Solano, Calif.

ORDINANCE NO. _____ N.C. (2d)

AN ORDINANCE OF THE CITY OF VALLEJO AMENDING
TITLE 16 (ZONING ORDINANCE) OF THE VALLEJO
MUNICIPAL CODE AND ZONING MAP REGARDING
THE SOLANO360 PROJECT

THE COUNCIL OF THE CITY OF VALLEJO DOES ORDAIN AS FOLLOWS:

New text shown in *bold italic*, deleted text in ~~strike through~~;

SECTION 1: Section 16.80.106 of the Vallejo Municipal Code is hereby added as follows:

16.80.106 Solano360 exception permits.

Exception permits for the projects within the Solano360 specific plan shall be prepared consistent with the policies, standards and implementation program in the Solano360 specific plan and shall be reviewed for approval by either the development services director or the Solano360 Implementation Committee per Section 16.116.080 herein, as set forth in said plan.

SECTION 2: Section 16.116.078 of the Vallejo Municipal Code is hereby added as follows:

16.116.078 Solano360 unit plans.

Unit plans for the projects within the Solano360 specific plan shall be prepared consistent with the policies, standards and implementation program in the Solano360 specific plan and shall be reviewed for approval by either the Solano360 Implementation Committee per Section 16.116.080 herein or planning manager as set forth in said plan.

SECTION 3: Section 16.116.079 of the Vallejo Municipal Code is hereby added as follows:

16.116.079 Solano360 project unit plans.

Unit plans for the projects within the districts specified in the Solano360 planned development master plan shall be prepared consistent with the Solano360 Specific Plan and the development agreement (the "DA") between the city and Solano County. Pursuant to the DA, the city and Solano County are obligated to timely disputes regarding unit plans for major projects to the Solano360 Implementation Committee, per Section 16.116.080 herein.

SECTION 4: Section 16.116.080 of the Vallejo Municipal Code is hereby added as follows:

16. 116. 080 - Solano360 Implementation Committee.

- a. Solano360 implementation committee. The Solano360 Implementation Committee shall be comprised of two members from the Solano County board of supervisors and two members from the city council.*

- b. Scope of Authority. The Solano360 Implementation Committee will consider, provide direction on, and attempt to resolve minor disputes arising from the Solano360 Specific Plan.*
- c. Staff coordination meetings. City and county staff shall work cooperatively and establish support committees throughout the project, as needed, comprised of staff members with special expertise as required and available from its engineering, maintenance, operations, land use planning, economic development, legal, or other technical group to meet regularly to assist in the successful advancement of the project. These support committees will report to the city manager and county administrator, who in turn, will report to the Solano360 Implementation Committee.*
- d. Solano360 Implementation Committee meetings. At a minimum, the Solano360 implementation committee shall meet quarterly during the implementation of phase 1 of the Solano360 specific plan and not less than annually during Solano360 specific plan phases 2 & 3. All such meetings of the Solano360 implementation committee will be held in accordance with the Brown Act.*

SECTION 5. Zoning Map Amendment.

Assessor's Parcel Numbers 0052-240-550 and 0052-240-560 shall be rezoned from Public/Quasi-Public District to Mixed-Use Planned Development District, as described in Exhibit C-1 to Attachment 1.

SECTION 6. Compliance with the California Environmental Quality Act.

The potential environmental effects of the Code Text Amendment and the proposed Plan have been assessed in the Draft EIR and Final Environmental Impact Report prepared for the Solano360 Specific Plan Project (collectively the "FEIR"). The FEIR was certified by the Solano County Board of Supervisors (Board) as the lead agency on February 26, 2013. Based on the FEIR, the project would result in impacts that can be mitigated to a level of insignificance, and impacts that are significant and unavoidable. A Mitigation Monitoring and Reporting Program and Statement of Overriding Considerations and were also approved by the Board on February 26, 2013.

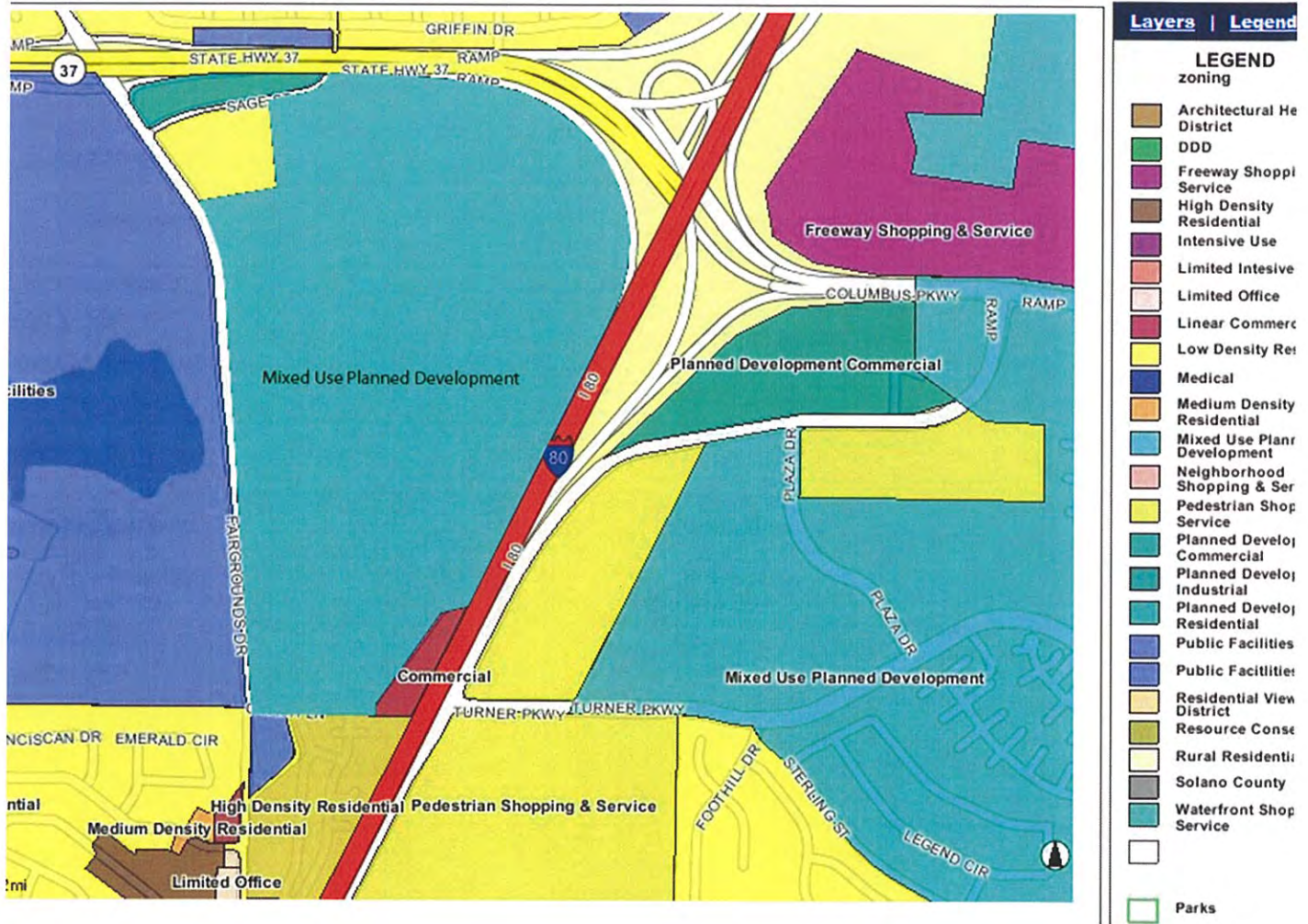
SECTION 7: Severability.

If any section, subsection, sentence, clause, phrase, or word of this Ordinance is for any reason held to be invalid by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance. The City Council hereby declares that it would have passed and adopted this Ordinance, and each and all provisions hereof, irrespective of the fact that one or more provisions may be declared invalid.

SECTION 8: Effective Date.

This Ordinance shall take effect and be in full force and effect from and after (30) days after its final passage.

FIRST READ at a regular meeting of the Council of the City of Vallejo held the _____ day of _____ and finally passed and adopted at a regular meeting of the Council held the _____ day of _____ by the following vote:



SOLANO360 SPECIFIC PLAN
PROPOSED ZONING MAP AMENDMENT

SOLANO360 PUBLIC DRAFT SPECIFIC PLAN

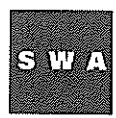
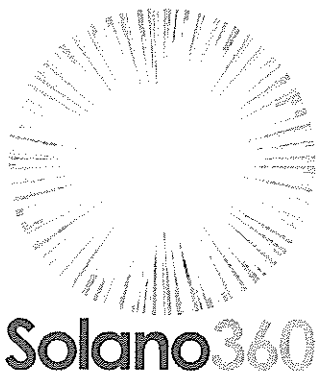
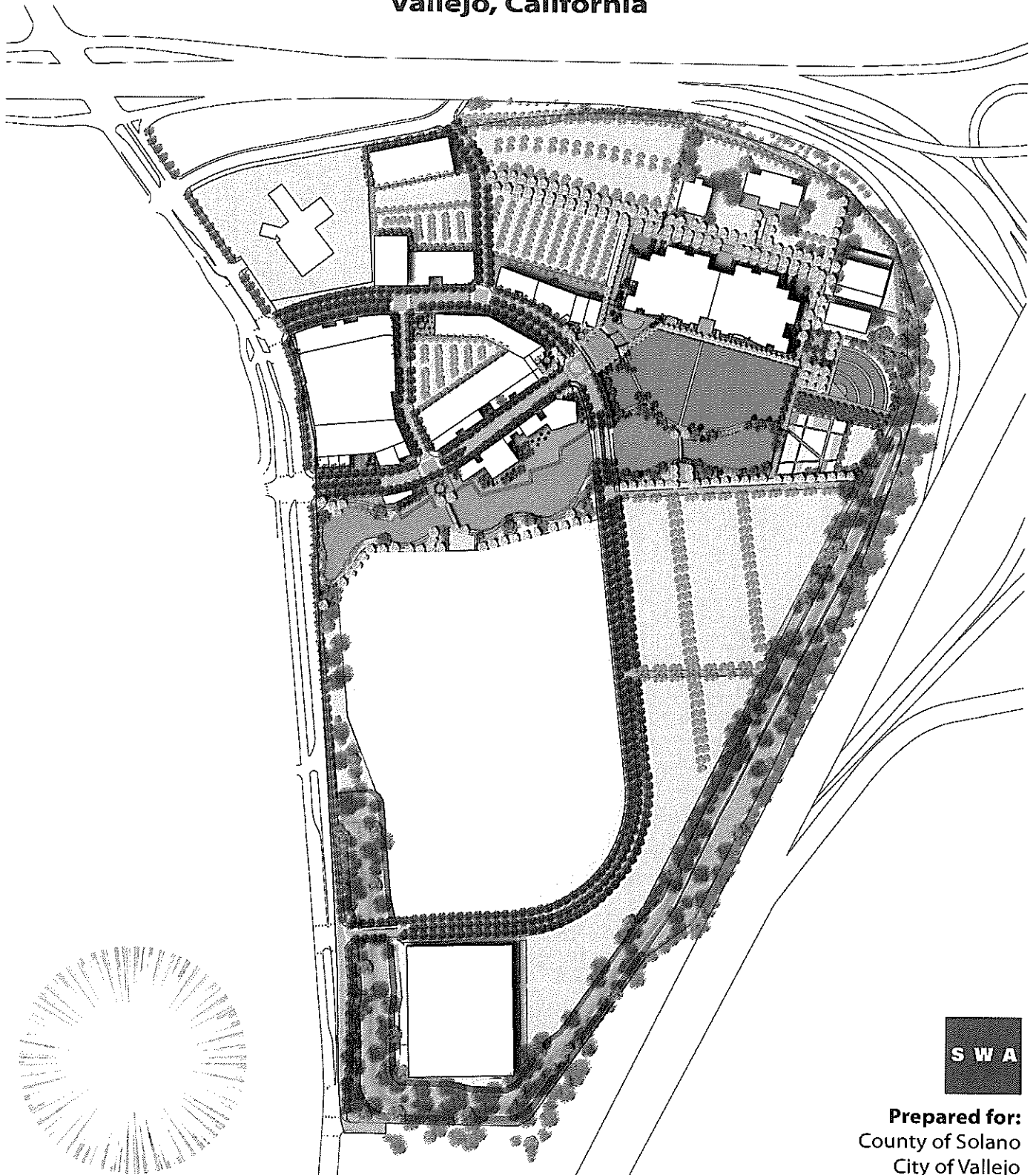
November 9, 2012

**Full colored version of Specific Plan and Appendices
can be viewed at www.ci.vallejo.ca.us.**

Solano 360

Public Draft Specific Plan

Vallejo, California



Prepared for:
County of Solano
City of Vallejo
Solano County Fair Association
November 9, 2012

Solano 360

Public Draft Specific Plan

Vallejo, California



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Prepared for:
County of Solano
City of Vallejo
Solano County Fair Association

November 9, 2012



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EXECUTIVE SUMMARY

Purpose and Scope

This Solano360 Specific Plan (the "Plan") is a product of a joint effort by the County of Solano ("County"), City of Vallejo ("City"), and the Solano County Fair Association ("Fair Association") to develop a flexible, long-term framework for redevelopment of the Solano County Fairgrounds, a 149-acre County-owned property located at the crossroads of Interstate 80 (I-80) and State Route 37 (SR-37) within the City of Vallejo.

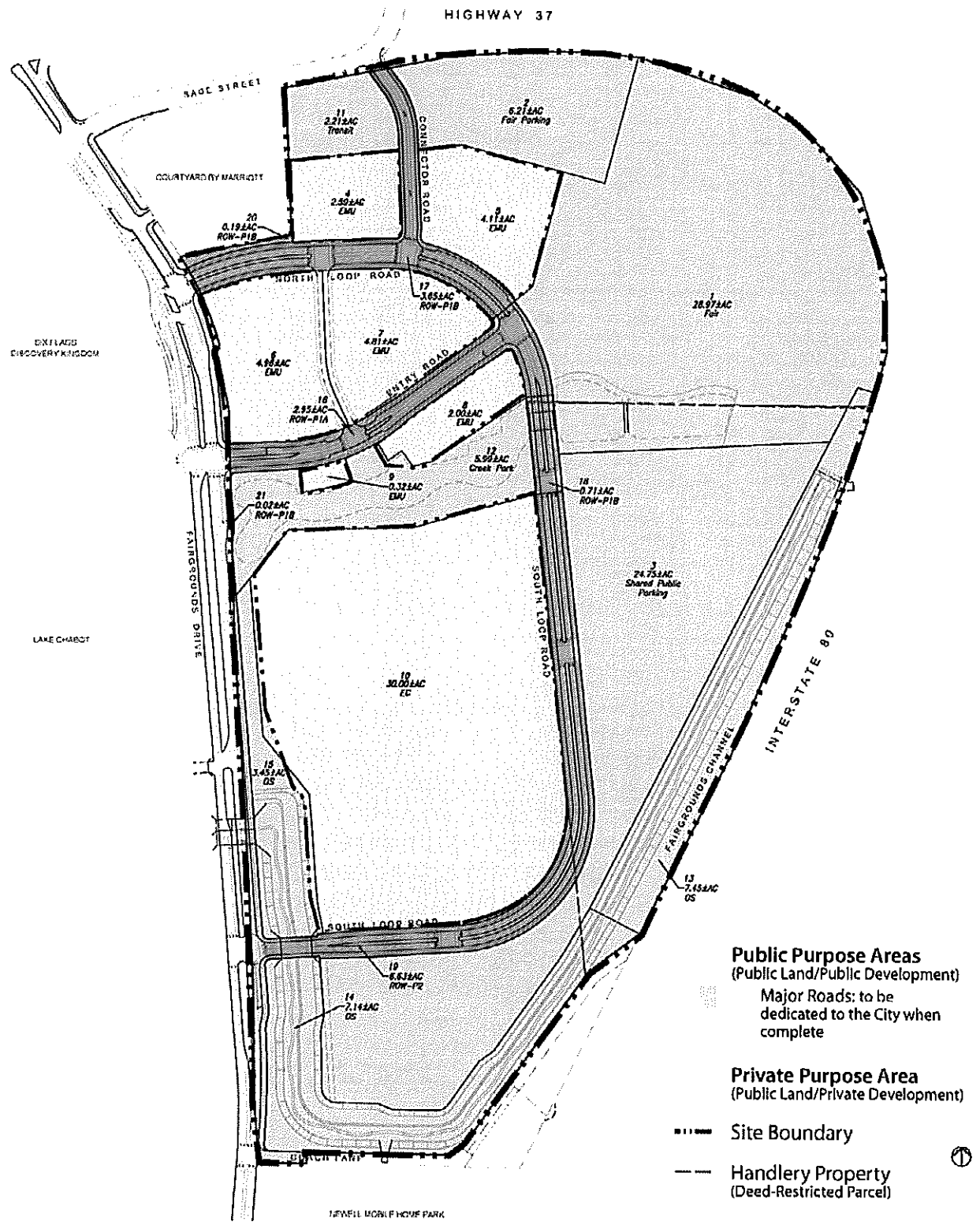
The Plan provides a flexible guide for land use and infrastructure improvements, public and private investments, and long-term, phased revitalization over the next 25 years. In addition, the Plan ensures consistency with the City of Vallejo General Plan, provides the basis for environmental review and subsequent entitlements, and supports County and City future actions as follows:

- **County of Solano** - this document serves as a master plan for development of Public Purpose Areas consisting of a new "Fair of the Future," an iconic, landmark destination that renews the 63-year heritage of the Solano County Fair, along with associated open space, parking, transit, and roadways. The Public Purpose Areas are proposed for primarily public purposes associated with the Solano County Fair and will be exempt from the City's land use authority.
- **City of Vallejo** – this document serves as a Specific Plan and Master Plan that satisfies requirements under the Vallejo Municipal Code (VMC), Title 16. It will provide flexible planning and design provisions for proposed mixed-use development to be undertaken for private, revenue-generating purposes, subject to the City's land use authority. The Private Purpose Areas of the Specific Plan are proposed for private development and will require a General Plan Amendment and Zoning Map Amendment, processed concurrently with this Plan.

The Solano360 Committee, a joint County-City-Fair Association group comprised of representatives from Solano County Board of Supervisors, City of Vallejo City Council, and Solano County Fair Association Board, provided direction for community outreach, planning and design principles, and implementation. Preparation of the Plan included a market study, a Public Facilities Financing Plan, a Fiscal Impact Analysis, a City of Vallejo General Plan Amendment, and technical evaluation of the proposed water feature.

Since 1949, the Solano County Fair Association has operated the annual County Fair on the property. Year-round activities include satellite wagering and a robust and diverse calendar of public and private events. Parking utilizes significant portions of the overall site, and drainage corridors form the eastern, southern and western boundaries. The 27-acre "Handlery Parcel" is limited by deed restriction to use for Fair and public purposes. Fairgrounds Drive provides primary access. Neighboring uses include Lake Chabot, the Six Flags Discovery Kingdom theme park, the Newell Mobile Home Park, and the I-80 and SR-37 corridors.

In conformance with CEQA, the Solano360 Environmental Impact Report (EIR) evaluated impacts associated with the project. The Plan and EIR were prepared concurrently so that project design could address and mitigate environmental conditions and constraints.



Public & Private Purpose Areas



Solano360 Vision

The proposed Solano360 project aims to redevelop the Plan Area by integrating a revitalized County Fairgrounds—the “Fair of the Future”, an iconic, region-serving public entertainment destination—with private mixed-use development. A public Visioning Process, conducted in 2008-2009, established the following Guiding Principles, which were jointly adopted by the Board of Supervisors, the City Council and the Fair Association Board to govern the development of the Solano360 project:

- Generate revenues for Solano County and the City of Vallejo, create jobs and ensure long-term economic sustainability.
- Establish a unique place with an unmistakable identity that serves as a destination for visitors as well as a pedestrian-friendly, community gathering place.
- Explore a mix of complementary land uses, including retail, commercial, hospitality, recreational, residential, family and youth oriented, educational and civic uses that seamlessly integrate with the the Fair of the Future.
- Explore increased physical connectivity and synergy with Six Flags Discovery Kingdom, downtown Vallejo, the waterfront and other existing commercial operations.
- Provide pedestrian, bicycle, vehicular and transit facilities that foster access to, from and within the site.
- Incorporate sustainable and green principles in all aspects of the development.

Land Use and Phasing

The Plan proposes a mix of region-serving entertainment and amusement attractions, along with complementary restaurant, retail and hospitality uses, that builds on the presence of the existing Six Flags Discovery Kingdom facility and Solano County Fairgrounds. The intent is to create a seamless integration of public and private areas, including Fairgrounds facilities and private mixed use development.

The land use mix allows a range of entertainment options and supporting commercial and residential uses that support the heritage of the Solano County Fair and facilitates logical and cost-effective implementation. The Plan targets opportunities for revenue generation and job creation; project amenities that establish an appealing visitor destination; a circulation system that manages parking demand and encourages pedestrian connections; and sustainable principles for landscape, infrastructure and building systems.

The Plan designates land use areas, as follows:

- **Fair:** 35 acres for the revitalized Solano County Fairgrounds area, or Fair of the Future, including built and open space venues and parking. In Phase 1, the new Exposition Hall provides approximately 50,000 net square feet of exposition space (approximately 72,000 gross square feet) to replace the existing Exposition Hall building. Phase 3 expansion would double the size of that facility. Site improvements include a new water feature, demonstration farm, arrival plaza and midway/event lawn. Parking facilities expand on a phased basis to serve events and activities at the Fair.
- **Transit/North Parking Center:** 2.2 acres for a transit and parking facility in the northwest area of site, with surface parking in Phase 1 and a parking structure constructed as part of Phases 2.
- **Parking and Roads:** 24.7 acres for major roadways and shared public parking to support the continuing viability of entertainment uses within and near the Plan Area. Phase 1 makes use of existing surface parking areas. Phase 2 improves surface parking lots to serve approximately 2,600 cars. In Phase 3, a multi-level parking structure replaces the southern portion of the shared public surface parking to support higher intensity development.

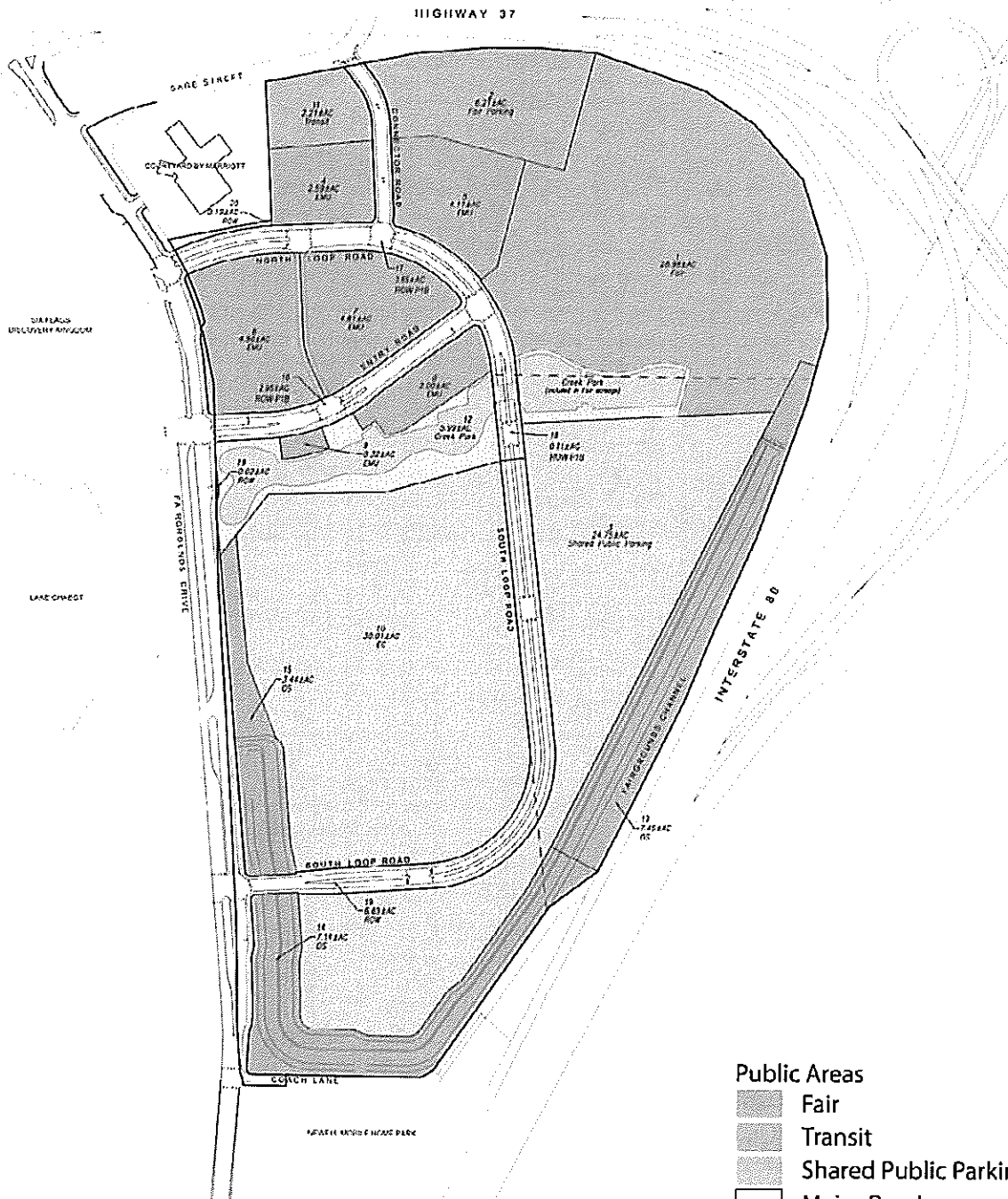
- **Open Space:** Six acres for the Creek Park and its water feature that form the spine of the “Public Entertainment Core” connecting the Fair of the Future with mixed use development areas, Entry Road, and Fairgrounds Drive. The water feature provides a visual amenity and water quality feature for onsite stormwater. In addition, the 17.9-acre Fairgrounds Channel alleviates existing flooding problems within the Plan Area and provides opportunities for riparian/wetland habitat and trails.
- **Entertainment-Mixed Use (EMU):** 18.8 acres for entertainment-oriented commercial uses, such as “Family Entertainment Centers”, and associated restaurant and retail activities. EMU parcels are clustered in the northern portion of the site near the Creek Park water feature and Entry Road. Buildings are expected to consist primarily of ground-floor commercial (retail, restaurant, or entertainment) uses with possible incidental office and/or residential space occupying upper stories. Parking is provided within the EMU areas and on major roads. In Phase 3, a parking structure allows intensification of EMU uses from 0.2 to 0.4 FAR.
- **Entertainment Commercial (EC):** 30.0 acres for a major entertainment use that requires a large undivided site. The Plan locates this parcel on the west side of the site, with primary vehicular access from the South Loop Road/Fairgrounds Drive intersection. Entertainment Commercial structures, outdoor rides, and other entertainment attractions are expected to be concentrated in the northern portion of the parcel in order to increase visibility and make best use of the Creek Park amenity. In Phase 2, parking is expected to be located within the southern portion of the parcel. In Phase 3, the EC venues could expand by making use of the shared public parking structure.

Land Use Program

LAND USES	Acres	Building Square Feet	Housing Units	Parking Stalls
Public Development Areas				
Fairgrounds	35.2	149,500		775
Transit/North Parking Center Bus Docking	1.1			
Transit/North Parking Center Parking Structure	1.1	121,600		380
Shared Public Parking Structure	5.0	800,000		2,500
Shared Public Surface Parking	19.7			1,980
Creek Park (w/water feature)	6.0			
Fairgrounds Channel (peripheral drainage)	17.9			
Major Roads	14.3			73
SUBTOTAL FOR PUBLIC DEVELOPMENT AREAS	100.3	1,071,100		5,708
Entertainment Mixed Use (EMU)	18.8	327,571		804
EMU Parking Structure (included in EMU area)		320,000		1,000
Residential (included in EMU area) ¹			50	
Entertainment Commercial (EC) ²	30.0	n/a		750
SUBTOTAL FOR PRIVATE DEVELOPMENT AREAS	48.8	647,571	50	2,554
TOTALS	149.1	1,718,671.2	50	8,262.0

Table Notes:

1. Housing is allowed within EC or EMU as a Conditional Use Permit from the City of Vallejo (see land use policies).
2. Square foot totals do not include Entertainment Commercial uses, which may include both outdoor venues and buildings. EC parking assumes 750 onsite surface spaces and 1,250 Shared Public Parking spaces at build-out (see parking program).
3. Shared Public Parking serves the Fair and other entertainment venues; includes 19.7 acres of surface parking and a 5-acre (2,500 car) parking structure (see parking program).
4. Square footages include parking structures as noted.



NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR-37/Redwood interchanges, as modified for Solano360

Land Use Plan

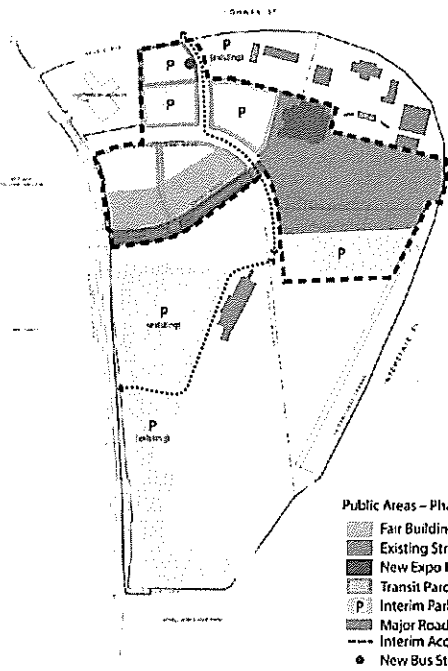


Within the private development areas (EMU and EC parcels), the Plan allows up to 220,000 square feet of office space as a permitted use and up to 50 housing units as a conditionally permitted use. These uses would substitute for other private development uses on a square foot basis.

Project phasing is designed to be flexible while establishing a strong initial character for the project, providing logical and cost-effective infrastructure investments, supporting development of the Fair of the Future and enhancing marketability. Phase 1 projects establish a strong and appealing sense of place through construction of the Creek Park, Entry Road, and Exposition Hall with outdoor venues. Project phasing coordinates levels of development intensity with required infrastructure including improvements to the SR-37/Fairgrounds Drive interchange.

The proposed phasing of major uses may be summarized as follows:

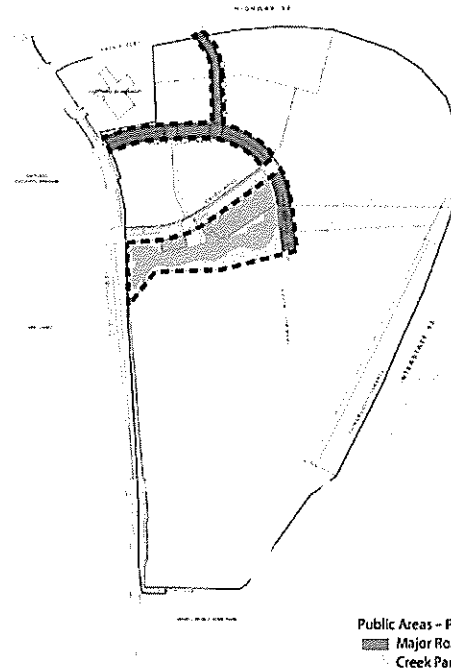
- **Phases 1a and 1b (years 1-5):** Upgrading and expansion of the Fairgrounds including the new Exposition Hall, outdoor venues, and public amenities in the Entertainment Core; creation of Entertainment-Mixed Use venues and facilities that may be feasible in the near term.
- **Phase 2 (years 6-15):** Creation of a larger parcel for a major Entertainment-Commercial user and additional Entertainment-Mixed Use development; build-out of the Plan Area at a density supported by surface parking.
- **Phase 3 (years 16-25):** Further intensification of Fairgrounds venues and Entertainment-Mixed Use and Entertainment-Commercial development along with expanded structured parking facilities.



- Public Areas - Phase 1A**
- Fair Building/Landscape
 - Existing Structures
 - New Expo Hall
 - Transit Parcel (Interim Parking)
 - Interim Parking
 - Major Roads
 - Interim Access
 - New Bus Stop
- Private Areas - Phase 1A**
- Entertainment - Mixed Use Parcel (Preferred Phase 1 Location)
 - Entertainment - Mixed Use (Phase 1 Parking)
 - Entertainment - Mixed Use (Interim Parking)

NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR 37/Redwood interchanges, as modified for Solano360.

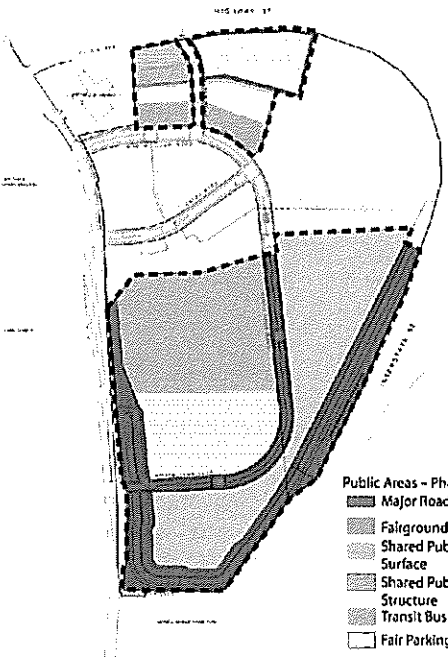
Phase 1A Projects



- Public Areas - Phase 1B**
- Major Roads
 - Creek Park
- Private Areas - Phase 1B**
- Entertainment - Mixed Use Parcel (Preferred Phase 1 Location)

NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR 37/Redwood interchanges, as modified for Solano360.

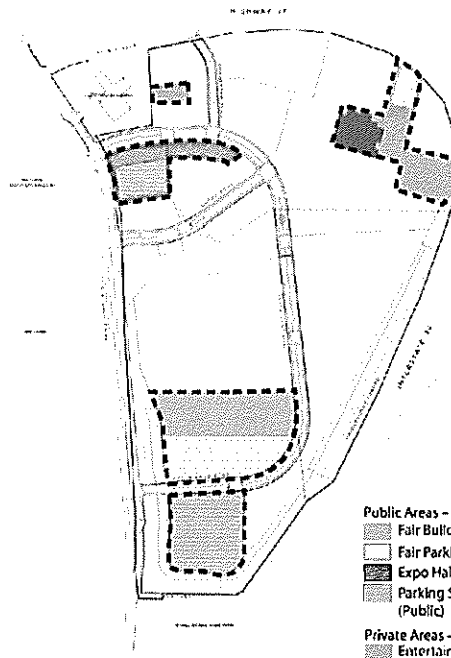
Phase 1B Projects



- Public Areas - Phase 2**
- Major Roads
 - Fairgrounds Channel
 - Shared Public Parking: Surface
 - Shared Public Parking: Structure
 - Transit Bus Docking
 - Fair Parking
- Private Areas - Phase 2**
- Entertainment - Mixed Use (Building Area)
 - Entertainment - Mixed Use (Parking Area)
 - Entertainment - Commercial (Building/Venue Area)
 - Entertainment - Commercial (Surface Lots)

NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR 37/Redwood interchanges, as modified for Solano360.

Phase 2 Projects



- Public Areas - Phase 3**
- Fair Building/Landscape
 - Fair Parking
 - Expo Hall Expansion
 - Parking Structure (Public)
- Private Areas - Phase 3**
- Entertainment - Mixed Use (Building Area)
 - Entertainment - Commercial (Building/Venue Area with parking as needed)

NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR 37/Redwood interchanges, as modified for Solano360.

Phase 3 Projects



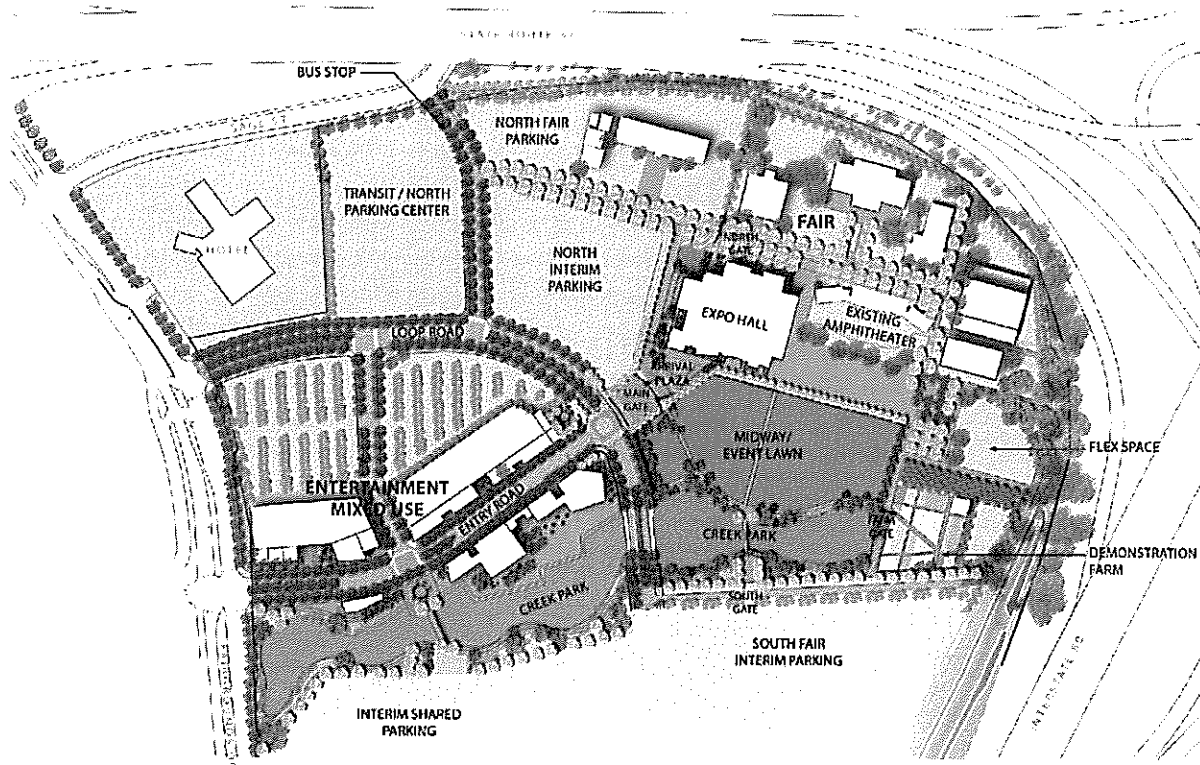
Design Character

The Plan sets forth urban design concepts and guidelines to shape and facilitate redevelopment of the Plan Area, consistent with the Guiding Principles and land use provisions described above.

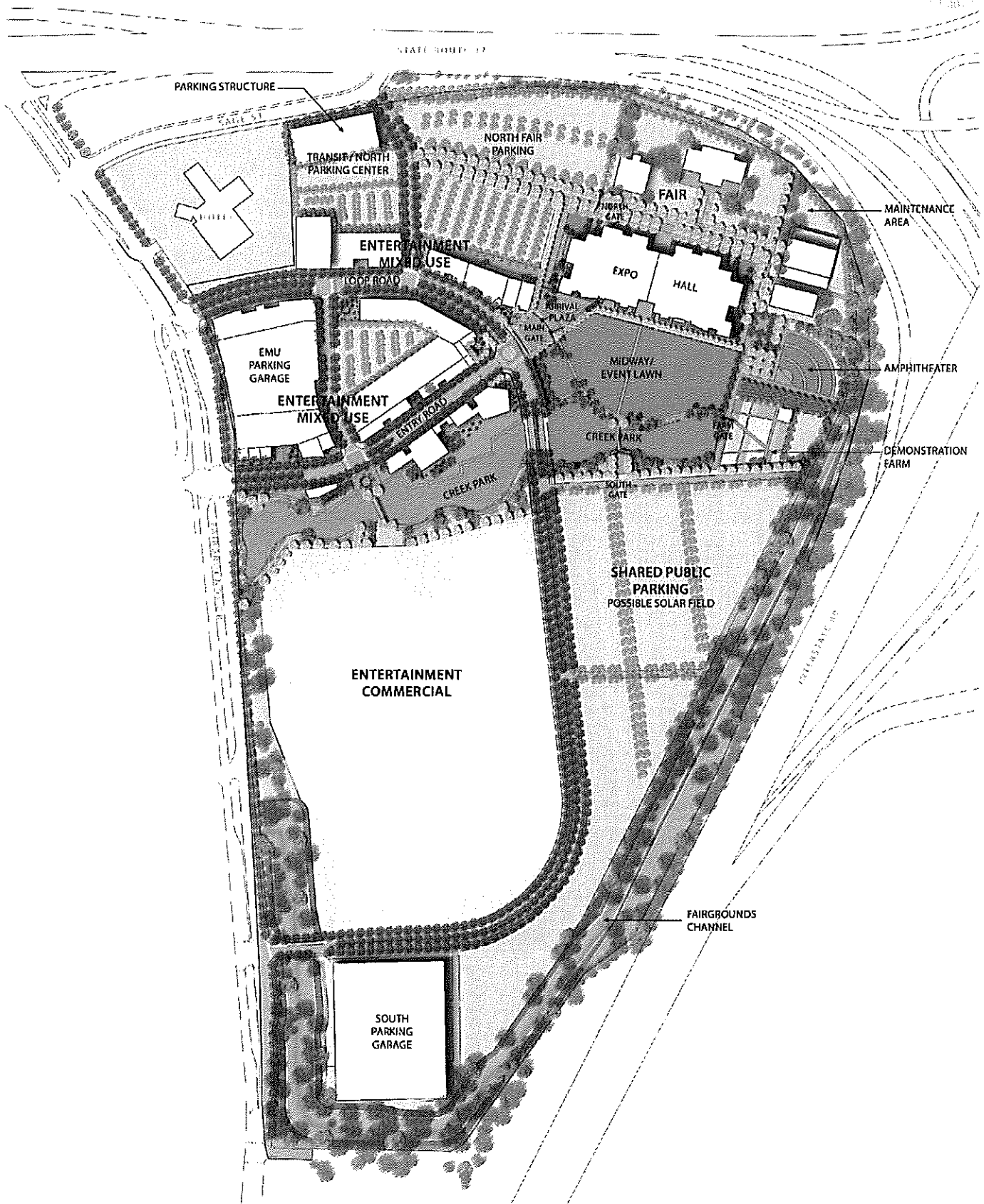
Key to overall character is the Public Entertainment Core, envisioned as a lively, mixed use entertainment corridor. The Core includes the Creek Park with its water feature, promenades, plazas and pedestrian bridges; the thematic "Main Street" or Entry Road aligned with Creek Park; and the Fair of the Future with arrival plaza and midway/event lawn with terraced seating.

The Plan proposes tree-shaded sidewalks and streets to reinforce a pedestrian-friendly character and complement a system of trails within the Creek Park, along Fairgrounds Channel, and within the Fair. The Creek Park forms a new open space corridor with waterfront promenades, picnic areas, lawn terraces, water view plazas, wetlands, and bridges. Consistent treatment of landscape, street character including plazas and paving, site drainage, parking, signage and lighting, walls and fencing, and loading/service areas reinforce a strong and appealing environment for both public and private uses.

The Fair of the Future is a focus for design, due to the impact of Phase 1 facilities on the image of Solano360 as a whole. The Plan replaces the existing Expo Hall with a new Exposition Hall offering approximately 50,000 net square feet of exhibition space in a highly marketable venue integrated with the existing fair concourse and other facilities. This flexible space can be subdivided to accommodate a range of events including conventions, consumer shows, festivals, large parties, and other special events.



Fair Illustrative Plan - Phase 1
Building areas depicted here are conceptual only.

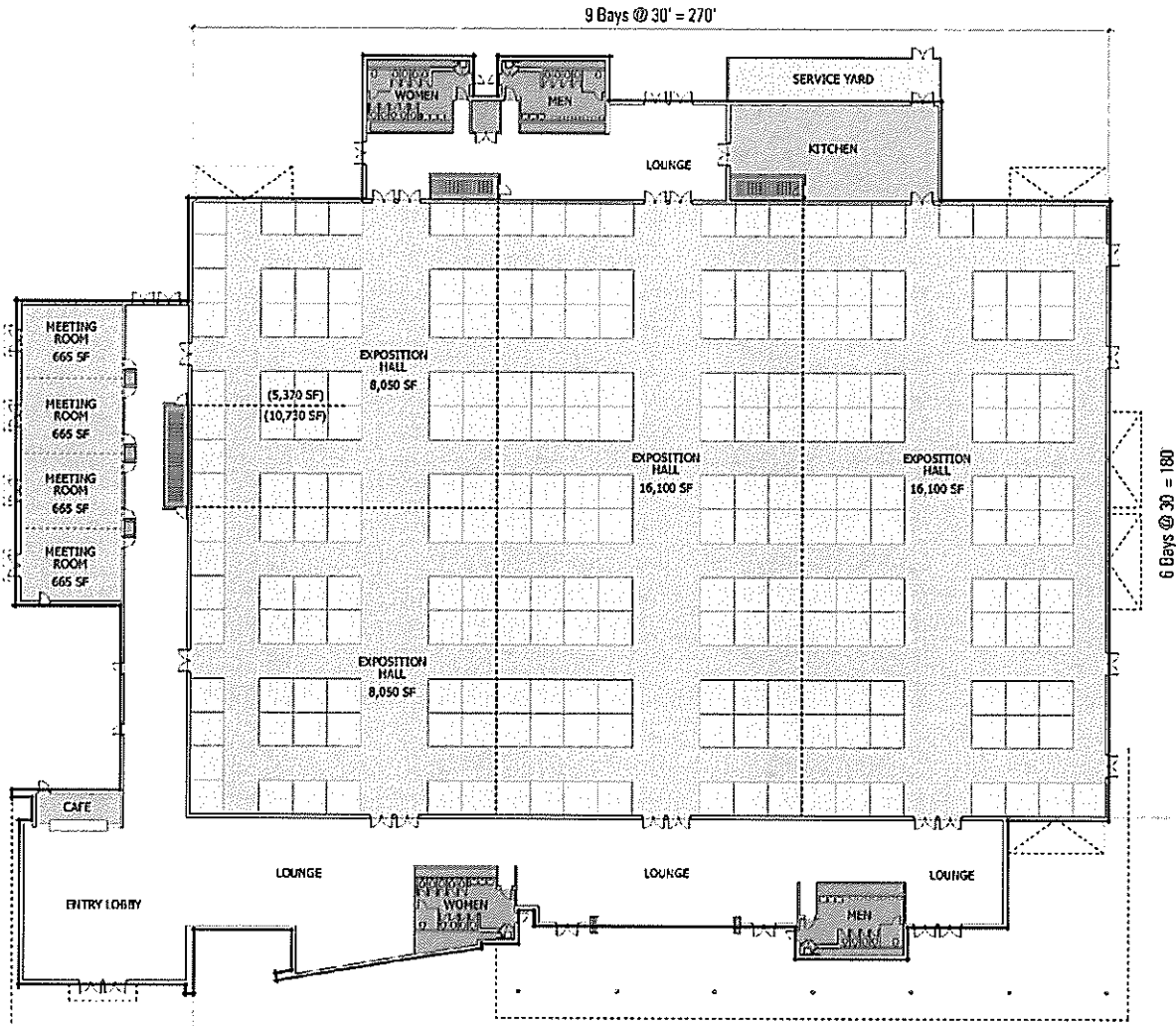


Illustrative Plan
Building areas depicted here are conceptual only.

The Exposition Hall also provides lobbies, circulation, meeting rooms, kitchen, storage of movable wall panels, and restrooms for a total of 72,000 square feet. The building's conceptual design represents a functional, economical building design that provides an architecturally distinct and compelling landmark facility.

New outdoor spaces reinforce the Fair as an event and recreational destination. These include an arrival plaza at the eastern terminus of the Entry Road, with portable ticket booths and a major gateway feature; rain gardens and plazas around the Exposition Hall; a four-acre midway/event lawn between the hall and the water feature; and a demonstration farm at the eastern end of the waterway. With expansion of the Exposition Hall in Phase 3, a new amphitheater replaces the existing outdoor concert hall.

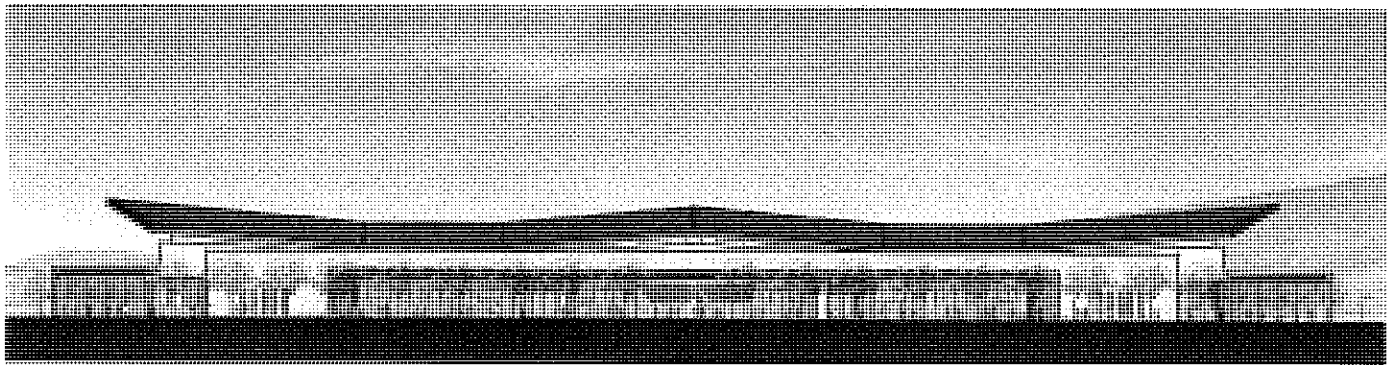
The Plan describes fencing and gates to secure the Fairground perimeter during events while maintaining an open, park-like appearance. It provides guidelines for private purpose areas to ensure a high quality, unified character between the "Fair of the Future" and thefor Entertainment Mixed Use and Entertainment Commercial development, with sustainable measures for site and building design, health, water quality, transportation, and energy.



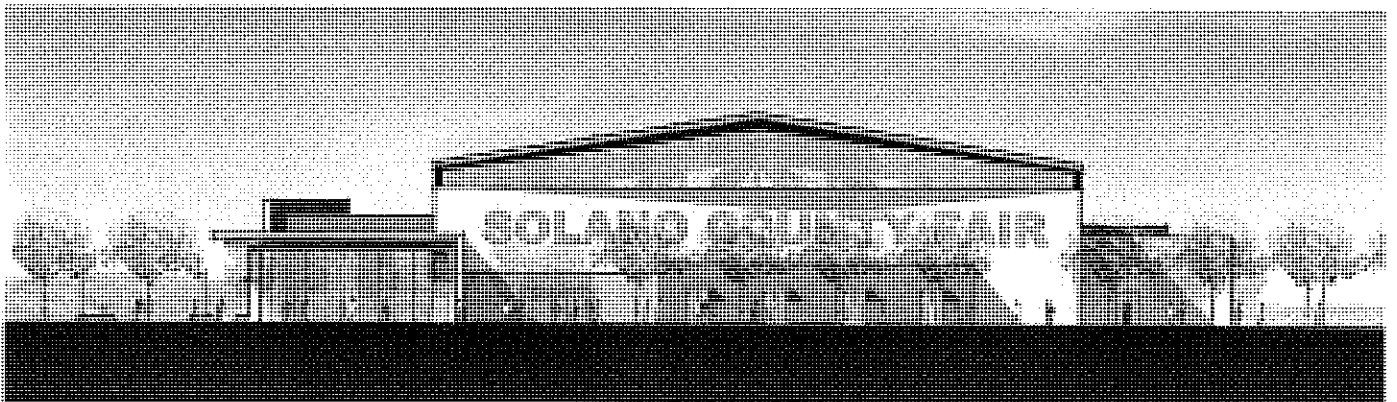
Exposition Hall –Schematic Floor Plan



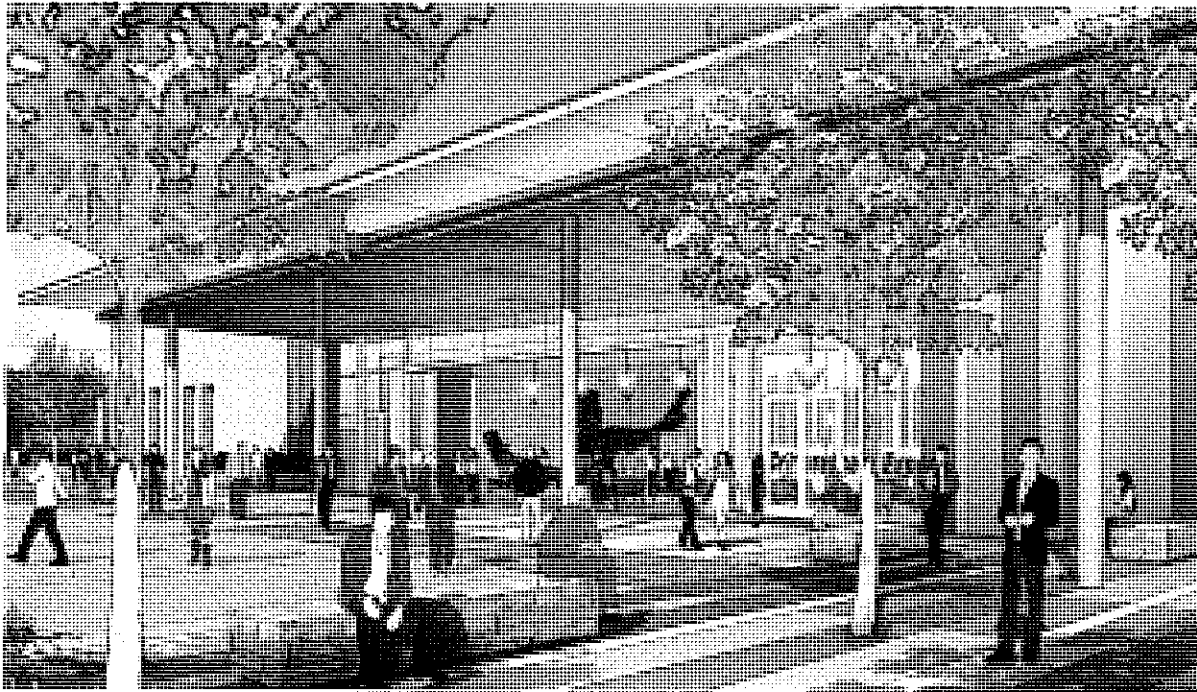
Arrival Plaza Perspective



South Elevation



East Elevation

**South Lobby Perspective****Transportation and Infrastructure**

The Plan Area gains access from adjacent freeways (I-80 and SR-37) via Fairgrounds Drive. To minimize traffic impacts, project phasing is tied to Solano Transportation Authority (STA) plans for Redwood Parkway/Fairgrounds Drive Improvement Project. As a result, the Phase 1 level of development is configured to avoid the need for off-site transportation improvements; subsequent phases are also linked to the capacity of off-site facilities.

Three intersections on Fairgrounds Drive and one on Sage Street provide access into the Plan Area. The Entry Road connects from the existing signalized intersection, located on Fairgrounds Drive opposite the entrance to Six Flags Discovery Kingdom, to the Fair's new arrival plaza. This establishes the Entry Road as a thematic "main street" with wide sidewalks and retail frontages. The Loop Road provides primary site circulation, connecting to Fairgrounds Drive at two locations. The Sage-Loop Connector Road serves service and transit vehicles.

A system of traffic calming, pedestrian, and bicycle features encourages non-vehicular circulation. Parking includes Phase 1 surface lots and interim parking; Phase 2 construction of the Transit/North Parking Center structure and paved surface lots within Shared Public Parking, the Fairgrounds, and private development areas; and Phase 3 construction of parking structures within both the Shared Parking and EMU areas. A Parking Operations Management Plan will be developed by the County to address parking facilities, joint-use, and scheduling. Travel Demand Management measures and a Fairgrounds Events Management Program will define strategies to avoid traffic congestion on peak event days.

In addition to transportation, the Plan proposes cost-effective infrastructure improvements for storm drainage (including grading), potable and non-potable water, wastewater, electricity, natural gas, telecommunications, wireless communications, and waste management. Existing utilities within the fair concourse area will remain in-place, but will be connected to new infrastructure along the Sage-Loop Connector Road.

Proposed drainage improvements will remove the Plan Area from the flood plain and address high off-site flows from the east and south by placing fill material in the northern end of the Plan Area and



enlarging the existing Fairgrounds Channel. These measures will also alleviate flooding at the Newell Mobile Home Park.

Sustainable infrastructure measures include harvesting runoff for onsite irrigation, installation of a non-potable water system within backbone roadways, and possible installation of wastewater facilities under surface parking areas.

Implementation and Administration

The Plan includes strategies and actions to be undertaken by the County and City to achieve high quality Private Purpose Area and Public Purpose Area development. These measures include definition of Public Purpose Areas, which are owned by the County and utilized for a public purpose, and are exempt from City land use authority.

The Solano360 development strategy assumes that the County will have the following Property Owner responsibilities in addition to any set forth in the Conditions of Approval.

- The County and City will enter into a Development Agreement/Implementation MOU.
- The County may issue an RFP for a single Developer or multiple Developers for the site. Such agreement(s) may include a ground lease of land.
- The County, or its Developer(s), will have responsibility for constructing all "horizontal development" (including grading, roads, and utilities) necessary to serve the Plan Area. Major roads will be built by the County and dedicated to the City of Vallejo once constructed to City standards.
- The County, or its Developer(s), will have responsibility for the preparation of finished pads for the EMU and EC parcels.
- The County, or its Developer(s), may seek others to develop the vertical buildings on the EMU and/or EC parcels, or may "build to suit" (develop, maintain and manage).
- The County, or its Developer(s), will sub-lease the EC and EMU parcels.
- EC and EMU end-users will build vertical improvements, or the County, on its own or through its Developer(s), will build-to-suit.

The County/Fair Association and the City will enter into agreements necessary for successful implementation, including the Implementation Memorandum of Understanding (MOU), Development Agreement, and Cost and Revenue Sharing Agreement.

The Plan is consistent with the land use policies and objectives contained in the City of Vallejo's General Plan, as amended to incorporate the Land Use Map and specifications. The Plan converts the property to the City of Vallejo Zoning Designation of Mixed-Use Planned Development (MUPD). Development within Private Purpose Areas will be subject to the City's regulatory and review process including preparation of Unit Plans, Subdivision Maps, and additional actions. Implementation of Public Purpose Area projects will be the primary responsibility of the County and will require full compliance with applicable building codes, ordinances and other regulatory authorities.

The Solano360 Specific Plan EIR has been prepared in accordance with the California Environmental Quality Act (CEQA) and provides environmental clearance on County and City approvals. Proposed private and / or public development may require additional environmental review and documentation in accordance with CEQA depending on the extent of consistency of the specific proposed development to the type and extent of development analyzed in the Specific Plan EIR.



CHAPTER TWO: SITE AND CONTEXT

2.1 INTRODUCTION

The following represents a brief summary of site characteristics and key issues related to opportunities for redevelopment of the Plan Area. Additional background information is available in the Solano360 Specific Plan EIR.

2.2 SITE CONTEXT

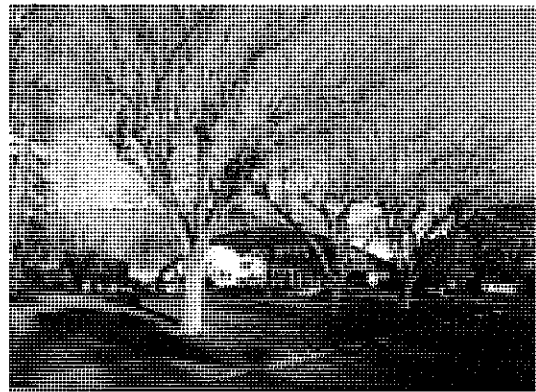
The Plan Area consists of 149.1 acres bounded by Interstate 80 (I-80) to the east, State Route 37 (SR-37) and Sage Street to the north, Fairgrounds Drive to the west and Coach Lane to the south. Approximately 265,000 cars pass the Fairgrounds each day on the I-80/ SR-37 freeway system, providing high visibility and easy access to both greater San Francisco Bay and Sacramento areas. The presence of Six Flags Discovery Kingdom, the County Fair, and existing hotel uses have established the site as a well-known venue for entertainment and special events within the region.

In addition to Six Flags Discovery Kingdom located to the west, the site is bordered by the Newell Mobile Home Park immediately to the south. Other residential neighborhoods are located across Fairgrounds Drive to the southwest and across SR-37 to the north. The Gateway Plaza shopping center is located east of I-80.

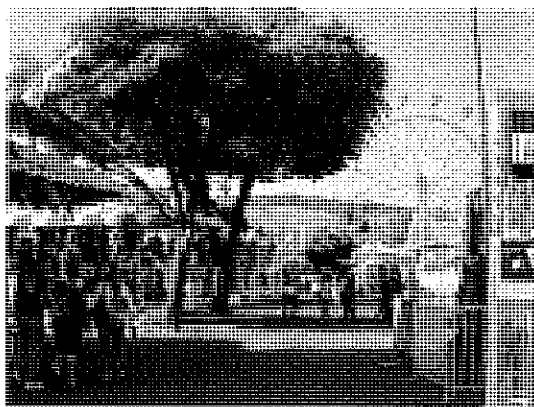


2.2.1 Land Use and Ownership

The Solano County Fairgrounds is owned by the County, subject to certain reversionary interests owned by the City of Vallejo, and is located within the Vallejo city limits. The property provides a fairgrounds and events site operated by the Solano County Fair Association. It also has supplied overflow parking for the adjacent Six Flags Discovery Kingdom. The Plan Area includes four parcels, all of which are owned by Solano County. The 27-acre "Handlery Parcel" is limited by deed restriction to use for Fair and public purposes (see Figure 2.1).



Since 1949, the Solano County Fair Association has operated Fair Week on the project site, a one to two week mid-summer event offering a traditional program of entertainment, midway, livestock demonstration, and crafts. In



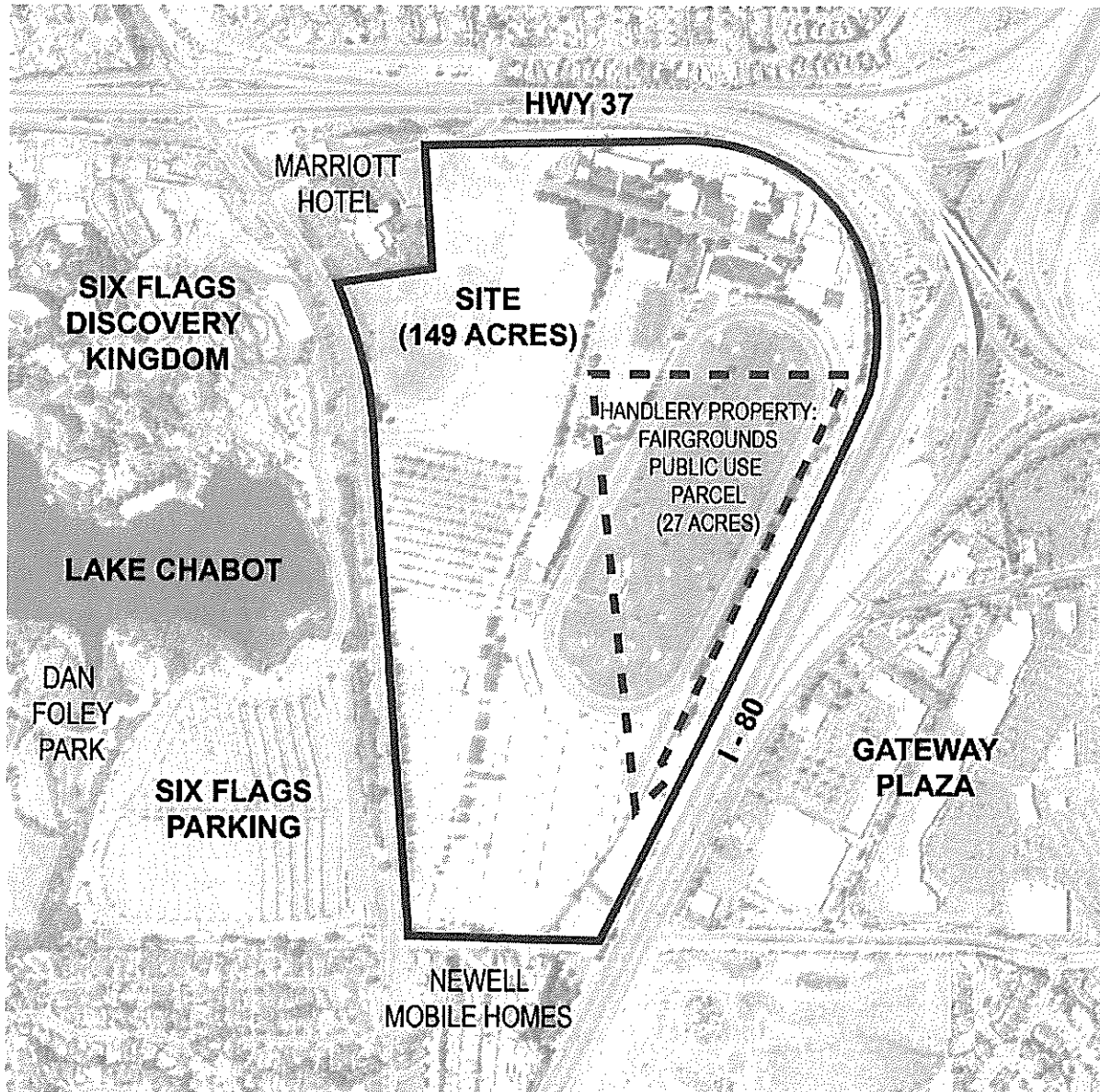


Figure 2.1: Site Aerial

2012, the Fair drew a crowd of 42,613 people.

In addition to Fair Week, year-round activities include satellite wagering, a robust and diverse calendar of public and private events including facilities rental, and a public-use 9-hole golf course located in the infield of the former horse racetrack. Outdoor venues include the carnival/midway area, paved and lawn areas, and the concourse. Parking utilizes significant portions of the overall site, with peak use of approximately 3,500 cars through the course of one weekend day during Fair Week.

The primary existing Fairgrounds facilities include a total of approximately 425,000 square feet of building space concentrated in northeast portion of site adjacent to an east-west landscape concourse. Horse racing, which formerly occupied a large portion of the property, was discontinued after the 2009 season; vacant secondary stables and horse racing facilities occupy the eastern and southern portions of the site. Surface parking and overflow parking occupy western portions of

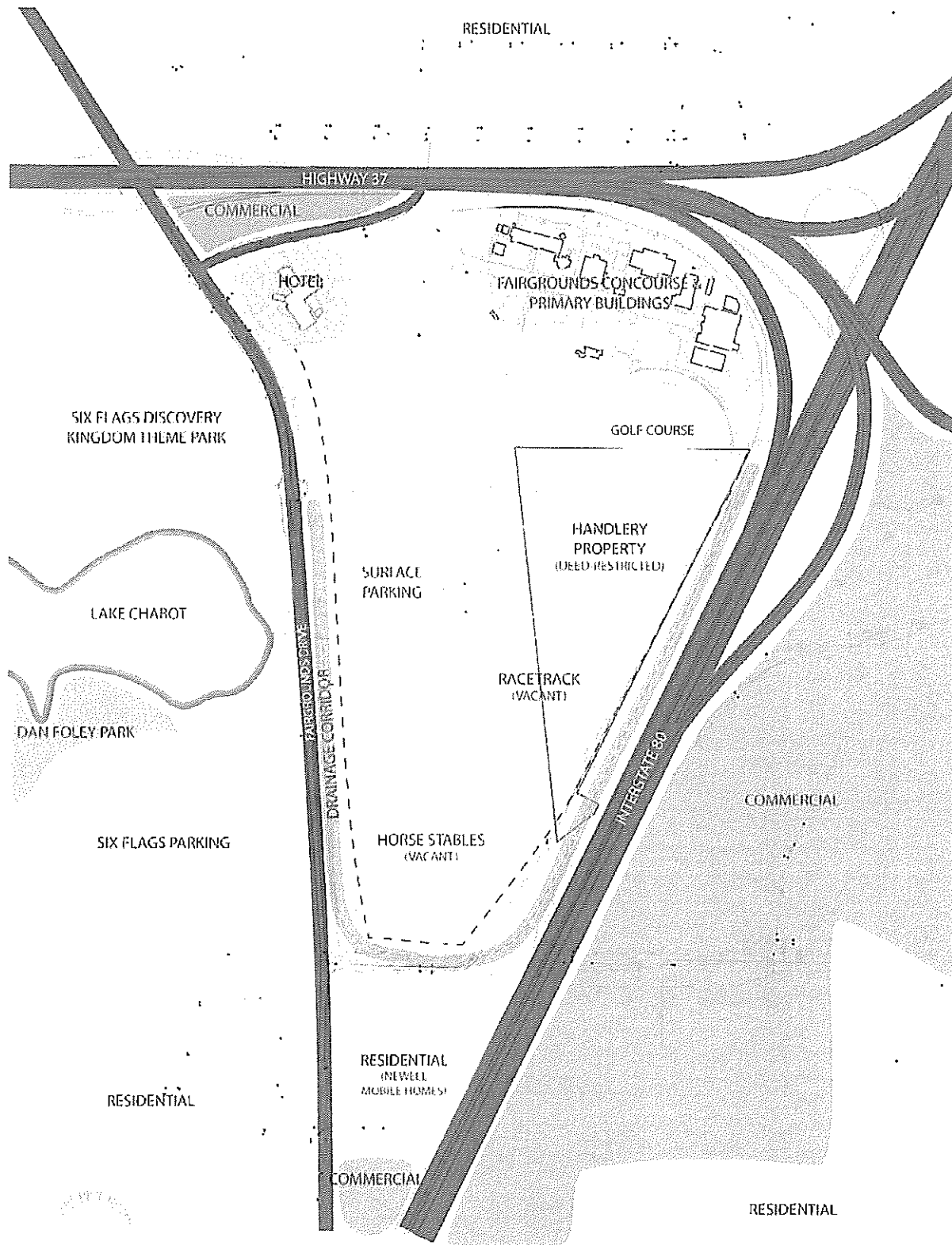


Figure 2.2: Existing Land Uses and Context

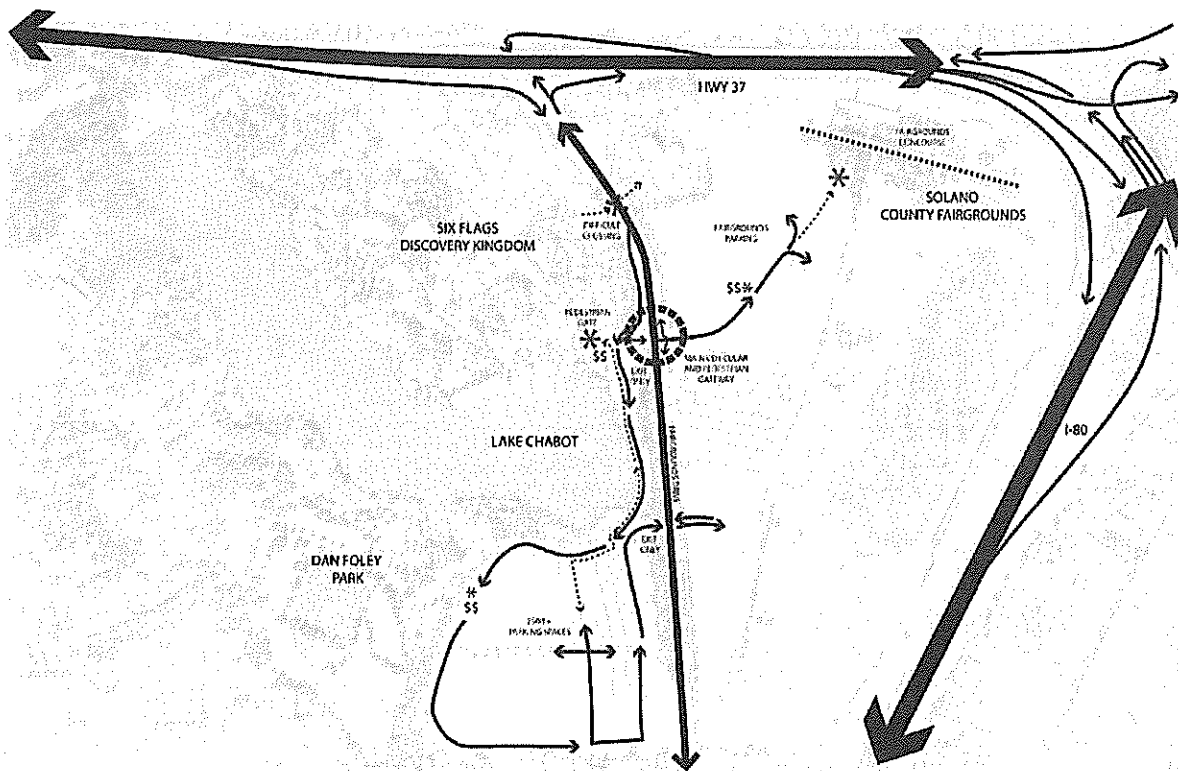


Figure 2.4: Existing Site Access

the site. Drainage corridors form the eastern, southern and western boundaries.

2.2.2 Access

The Solano County Fairgrounds site is located adjacent to the junction of I-80) and SR-37. Key transportation conditions are summarized below:

- Direct site access to the Plan Area is provided only via Fairgrounds Drive.
- Access to adjacent neighborhoods is limited. Sage Street provides an entry to the Courtyard by Courtyard by Marriott Hotel and a route to residential areas located north of SR-37, but currently does not connect to the Plan Area. Access from the Plan Area to Coach Lane is blocked by the existing drainage channel along the southern property boundary, and the freeway corridors form access barriers to the east and north.
- The site is highly visible from both freeways (I-80 and SR-37). Access from the freeway to the local street network serving the site is provided by two existing interchanges:
- The SR-37 / Fairgrounds Drive interchange provides the closest (less than a quarter-mile) and most visible access to the site via Fairgrounds Drive.
- The I-80 / Redwood Parkway interchange provides less direct access to the site via Redwood Parkway and Fairgrounds Drive.
- Solano Transportation Authority (STA) is currently developing plans for the widening of Fairgrounds Drive and SR-37/ Fairgrounds Drive interchange improvements (see Chapter Five: Transportation).
- Existing traffic congestion at SR-37/ Fairgrounds Drive interchange related to Six Flags Discovery Kingdom and Fairgrounds events may exceed acceptable levels of service



during certain peak hours.

- Existing intersections on Fairgrounds Drive provide access to Six Flag Discovery Kingdom and Solano County Fairgrounds, and their respective parking areas. Pedestrians currently cross Fairgrounds Drive from the Courtyard by Marriott Hotel to the Six Flags Discovery Kingdom entry.
- Public transit service and access is very limited to the site. The Solano County Fairgrounds is currently served by Vallejo Transit, which operates one bus route (#85) along Fairgrounds Drive. Two stops are located within a quarter-mile of the site: one at the Six Flags Discovery Kingdom entrance and one on Sereno Drive south of the site.

2.2.3 Natural Features

The Plan Area ranges in elevation from approximately 106 feet above mean sea level in the northeastern portion of the Fairgrounds concourse/building area to 83 feet in the southwest area, with a gentle slope from northeast to southwest. Four existing creeks (North Rindler Creek, Center Rindler Creek, South Rindler Creek and Blue Rock Springs) have been diverted into a combination of underground pipes and open channels (see Figure 2.4: Existing Drainage Pattern).

Issues associated with existing hydrology and flooding include:

- Some western and southern portions of the site are located within the 100-year flood zone as identified by the FEMA maps, calling for the need to widen existing channels and/or create new waterways. Via an existing storm drain system, Lake Chabot receives storm water runoff from the creeks as well as runoff from the Fairgrounds property.
- Existing water quality issues result from off-site watershed flowing through the site and from previous horse stabling on fairgrounds.
- Off-site flooding issues have been identified on Coach Lane/Newell Mobile Home Parks located south of the site.
- Shallow existing ground water limits the potential deepening of the peripheral channel and constrains options for water quality improvements.
- Seasonal and perennial wetlands exist within the site.
- Undocumented fill and soft compressible materials exist within the site.
- A berm currently separates the racetrack from the peripheral drainage.

2.3 MARKET FACTORS

The Solano360 Vision Report (2009) set forth a conceptual program of entertainment, commercial and mixed-use development. This conceptual program established a starting point for planning, in accordance with the Guiding Principles for the project.

As part of the Plan process, the County commissioned a market study to evaluate and focus the Vision Report assumptions. The market study concluded that it would be challenging for the amount and type of retail, office and hotel uses previously proposed in the Solano360 Vision Report to be feasibly supported in the foreseeable future, given current and projected economic and real estate market conditions. In addition, the market study indicated that market demand for convention, trade shows, corporate meetings and other events is unlikely to support a 100,000 square foot exposition building and additional fairgrounds facilities in the near future.

As a recommended direction for the Plan, however, the market study identified an opportunity to create a synergetic mix of region-serving entertainment and amusement attractions, along with complementary restaurant, retail and hospitality uses, that would build on the presence of the

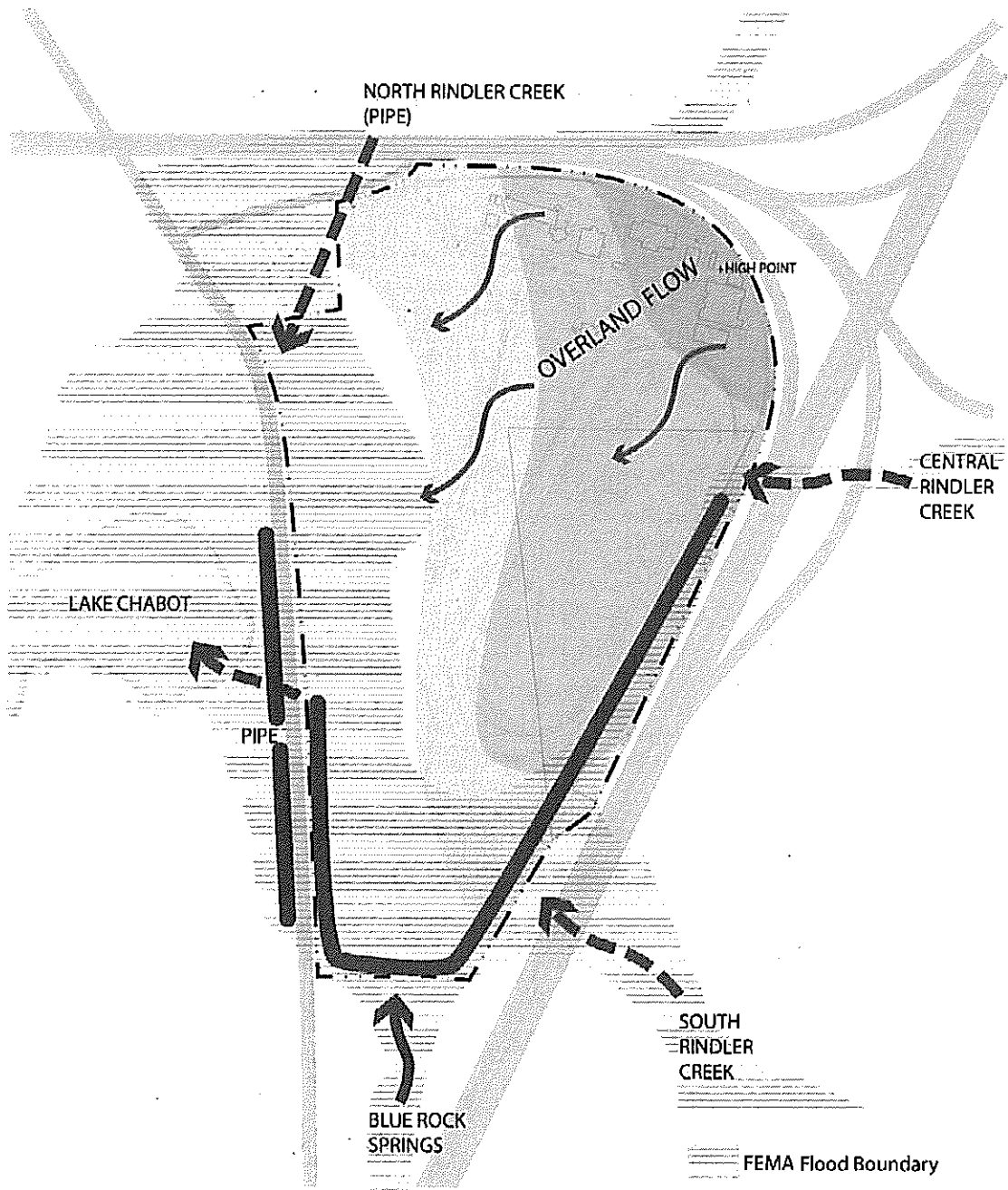


Figure 2.5: Existing Drainage Pattern



existing Six Flags Discovery Kingdom facility and Solano County Fairgrounds. These opportunities would include experiential entertainment (including education and recreational), shopping, and food uses.

Entertainment uses are particularly well-suited to this particular site, given its excellent freeway visibility and access, relatively limited local and neighborhood access, large single ownership pattern and adjacency to existing major entertainment attractions. In the mid-term and long-term of approximately the next 10 years (2012-2022), entertainment uses will likely generate demand for support uses such as restaurants, retail stores, and hotels. Office use and a limited quantity of residential development may also be considered for portions of the site.

The market study informed the subsequent planning efforts in the following ways:

- The Plan land use mix emphasizes themed entertainment park and family entertainment uses, with flexibility to accommodate retail, restaurant, office, housing, and/or hospitality uses as demand arises.
- Instead of demolition/rebuilding of the entire Fairgrounds, the Plan proposes replacement and upgrading of facilities along the existing concourse, and construction of a new Exposition Hall that will initially provide 50,000 net square feet of exposition space with possibilities for expansion in later phases.
- The Plan provides for continuation of public parking available to lease to nearby existing major entertainment uses, in order to support viability of those uses.
- Subsequent research into the functional requirements and industry standards for themed entertainment uses focused on development types that would build on the site's regional freeway visibility and the presence of the Fairgrounds and Six Flags Discovery Kingdom. These included:
 - Entertainment park-type uses requiring approximately 20 to 40 acres in a single parcel (including parking); these could consist of a water park, amusement park, commercial recreation or an entertainment center offering outdoor and outdoor venues and attractions. Ideally, sites should accommodate opportunity for expansion. Parking can either be contained within the parcel or provided nearby.
 - Family entertainment centers (FEC's) requiring smaller sites of approximately one to seven acres (including parking). These uses typically provide activities located within buildings (e.g., combined video game/restaurant attraction) or outside (e.g., go-kart or miniature golf). They can provide some street-oriented retail frontage and require parking in close proximity.
 - Limited retail and restaurant uses, such as a "restaurant row" connecting the Fairgrounds with the Six Flags Discovery Kingdom entry.

These entertainment-oriented uses can enhance year-round programming possibilities for the Fairgrounds, with increased opportunities for traveling exhibitions and events that encourage repeat visitation and offer diverse, multi-generational attractions.

The co-location of the uses presents the opportunity for shared parking and linked trips; a single trip in the family car, for example, might lead to visits to multiple destinations within the project area. Initially, the project should provide for surface parking in close proximity to each entertainment attraction. In the long-term, however, increased demand may lead to the need for increased public transit, shuttles connecting through the Plan Area and to nearby entertainment attractions, and structured parking solutions.

Other requirements for these uses include design of backbone infrastructure, parking and ingress/egress, and installation of phased infrastructure to encourage prospective end-users.





CHAPTER THREE: LAND USE

3.1 INTRODUCTION

This chapter establishes land use objectives, plan and program, phasing, and policies for the Plan Area. The provisions of this chapter shall be used to regulate all land uses for both private and public areas. The Plan is intended to provide flexibility for a range of entertainment options and supporting commercial uses, in a way that supports the heritage of the Solano County Fair and creates synergy with existing major entertainment uses and lodging.

3.2 LAND USE POLICIES

The following land use policies provide consistency with the Solano360 Guiding Principles and establish a basis for the plans, programs, and policies of the Plan.

The project should be structured to maximize opportunities for revenue generation, job creation, and long-term economic sustainability.

- Establish Solano360 as an entertainment site with multiple attractions, including a varied set of destinations for family activities, a year-round program of events for the Fair of the Future, and a pedestrian-oriented Public Entertainment Core as the defining feature.
- Provide a flexible and synergistic mix of uses that can be phased over time.
- Emphasize entertainment-oriented commercial, recreational, and civic uses, with flexibility to allow incidental residential uses and office and hotel development.
- Define land uses that will complement the Fair of the Future, with opportunities for a "critical mass" of entertainment-related activities and destinations in the Plan Area.
- Provide flexibility in parcel sizes and land use relationships to help attract the types of commercial enterprises identified by the market analysis.
- Define land uses that will generate net positive fiscal impacts for the County, City and Fair.
- Define land uses that will create job opportunities for City and County residents.
- Project amenities and features should be designed to establish a unique and appealing destination for visitors.
- Initiate an early program of site amenities and Fair of the Future improvements to establish strong initial character, including a Public Entertainment Core that encourages social gathering and fosters a strong sense of place.
- Design the Public Entertainment Core to encompass the Entry Road and a Creek Park that includes a central east-west water feature and pedestrian promenades connecting from Fairgrounds Drive to the Fair of the Future (see Figure 3.1).
- Establish the Fair of the Future as a community gathering area, with a variety of open spaces for recreational and civic engagement as well as enhanced commercial and entertainment functions.
- Establish the Plan Area as a regional attraction and destination.
- The project's circulation systems should be designed to increase pedestrian and vehicular connections with existing major entertainment uses, downtown Vallejo, and other destinations.



- Provide efficient access and ample parking to attract and support entertainment commercial uses.
- Emphasize direct access to parking areas, with primary circulation along a Loop Road and a pedestrian character for the Entry Road and promenade, including wide urban sidewalks for trees and outdoor seating as well as trails along the water feature.
- Align the Main Entry Road with Six Flags Discovery Kingdom's main gate.
- Designate locations for transit and shuttle facilities that link the Plan Area with existing major entertainment uses, Downtown Vallejo, the waterfront, and other destinations; serve commuters; and augment the parking supply for events on weekends.
- Provide pedestrian and bicycle routes along roadways and within a comprehensive trails system, including along the Fairgrounds Channel if possible.
- Allow for shared parking facilities that provide the capacity to accommodate full development of the Plan Area.
- The project should incorporate sustainable and green principles in its landscape, infrastructure, and building systems.
- Create an enduring place that fosters a strong sense of community while contributing to the positive well-being of the environment.
- Reduce the use of energy, water and materials by making best use of existing facilities, creating multi-use buildings and open spaces, and integrating measures for onsite energy generation and energy savings.
- Protect and restore the existing habitat while solving flooding and drainage issues.
- Provide shared parking, transit, bicycle-pedestrian, and shuttle systems to reduce vehicular impacts.
- Create opportunities to build housing above commercial locations in an effort to limit commuter trips to and from the Plan Area.

3.3 LAND USE PLAN AND PROGRAM

The land use plan, program and phasing incorporate a comprehensive analysis of the Plan Area's physical conditions, the results of public outreach and visioning, and research into the needs of the Solano County Fair and the prospects for near and long-term development over the next 25 years.

Figure 3.1: Land Use Plan illustrates the distribution of uses within the Plan Area. Parcel acreages, as defined by the colored and labeled areas on Figure 3.1, are exclusive of major roadways. Table 3.1: Land Use Summary provides an overall summary of land uses, and Table 3.4: Phasing Program provides additional detail including proposed phasing of development and on-site parking.

The proposed mix of development, open space, and infrastructure is intended to facilitate the following principle actions:

- Phases 1a and 1b (years 1-5): Upgrading and expansion of the Fairgrounds and associated public amenities in the Entertainment Core; creation of "Entertainment-Mixed Use" (EMU) venues and facilities that may be feasible in the near term. (Note: References in this Plan to "Phase 1" assumes Phases 1a and 1b together.
- Phase 2 (years 6-15): Creation of a larger parcel for a future "Entertainment-Commercial" (EC) user, such as a theme park anchor, and additional EMU development



Table 3.1: Land Use Program

LAND USES	Acres	Building Square Feet	Housing Units	Parking Stalls
Public Development Areas				
Fairgrounds	35.2	149,500		775
Transit/North Parking Center Bus Docking	1.1			
Transit/North Parking Center Parking Structure	1.1	121,600		380
Shared Public Parking Structure	5.0	800,000		2,500
Shared Public Surface Parking	19.7			1,980
Creek Park (w/water feature)	6.0			
Fairgrounds Channel (peripheral drainage)	17.9			
Major Roads	14.3			73
SUBTOTAL FOR PUBLIC DEVELOPMENT AREAS	100.3	1,071,100		5,708
Entertainment Mixed Use (EMU)	18.8	327,571		804
EMU Parking Structure (included in EMU area)		320,000		1,000
Residential (included in EMU area) ¹			50	
Entertainment Commercial (EC) ²	30.0	n/a		750
SUBTOTAL FOR PRIVATE DEVELOPMENT AREAS	48.8	647,571	50	2,554
TOTALS	149.1	1,718,671.2	50	8,262.0

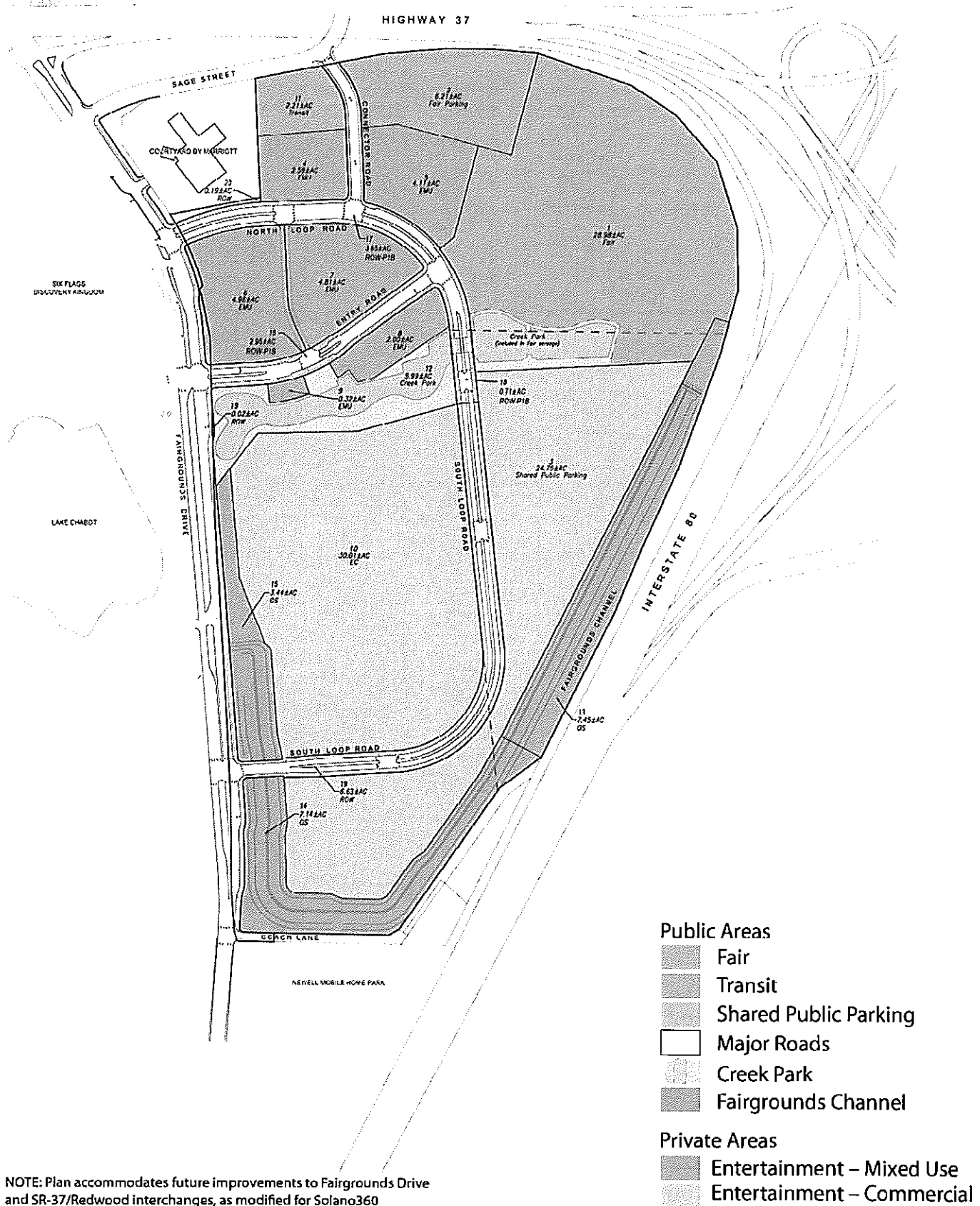
Table Notes:

- Housing is allowed within EC or EMU as a Conditional Use Permit from the City of Vallejo (see land use policies).
- Square foot totals do not include Entertainment Commercial uses, which may include both outdoor venues and buildings. EC parking assumes 750 onsite surface spaces and 1,250 Shared Public Parking spaces at build-out (see parking program).
- Shared Public Parking serves the Fair and other entertainment venues; includes 19.7 acres of surface parking and a 5-acre (2,500 car) parking structure (see parking program).
- Square footages include parking structures as noted.

Table 3.2: Public-Private Acreages

LAND USE	Acres at Buildout	Subtotals
Public Areas		
Fairgrounds (Facilities, Waterway, Parking)	35.2	
Creek Park	6.0	
Open Space/Channel	17.9	
Transit/North Parking Center	2.2	
Shared Public Parking	24.7	
Major Roads	14.3	
<i>Subtotal Public Areas</i>		100.3
Private Development Areas		
Entertainment-Mixed Use (EMU)	18.8	
Entertainment-Commercial (EC)	30.0	
<i>Subtotal Private Areas</i>		48.8
TOTAL	149.1	149.1





NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR-37/Redwood interchanges, as modified for Solano360

Figure 3.1: Land Use Plan



Table 3.3: Parcel Acreages

Parcel #	Land Use	Acres	Subtotals (acres)
1	Fair	28.97	
2	Fair	6.21	35.2
3	Shared Parking	24.75	24.8
4	EMU	2.59	
5	EMU	4.11	
6	EMU	4.96	
7	EMU	4.81	
8	EMU	2.00	
9	EMU	0.32	18.8
10	EC	30.00	30.0
11	Transit/N. Parking	2.21	2.2
12	Creek Park	5.99	6.0
13	Open Space	7.45	
14	Open Space	7.01	
15	Open Space	3.45	17.9
16	Right-of-Way	2.95	
17	Right-of-Way	3.65	
18	Right-of-Way	0.71	
19	Right-of-Way	6.76	
20	Right-of-Way	0.19	
21	Right-of-Way	0.02	14.3
		149.11	149.1

- Phase 3 (years 16-25): Further intensification of Fairgrounds venues and EMU and EC development along with expanded parking facilities.

The parcelization indicated in Figure 3.1 and Table 3.3 is intended to be illustrative. The land use plan envisions a flexible framework for development, and parcels within the Private Development areas may be combined or adjusted in size to fit a proposed building program. Each phase includes adequate parking to maintain a successful entertainment district.

This development program will be implemented in accordance with the policies contained in this section and Chapter Four: Urban Design and Guidelines.

3.4 LAND USE DESCRIPTIONS

3.4.1 Fair of the Future

Referred to as the “Fair of the Future,” the Solano County Fairgrounds area will include approximately 35 acres of built and open space venues and parking. The Fair of the Future is intended to continue the 60-year tradition of the annual Solano County Fair, offering a world-class Exposition Hall and other built and open space venues to support a variety of events and gatherings.

As envisioned, the existing Fair facilities will remain generally in their current locations along the existing landscape concourse, with upgrades and building replacement planned through a program of cost-effective, incremental, and phased improvements over time. New buildings and open spaces will relate to the existing concourse as well as the new Midway/Events Lawn and

Table 3.4 Phasing Program

Summary Description of Phases	PHASE 1a				PHASE 1b				PHASE 1 Total (years 1-5)				PHASE 2 (years 6-15)				PHASE 3 (years 16-25)			
	New Program				New Program				Cumulative Program (includes prior phases)				New Program				Cumulative Program (includes prior phases)			
	New Acres	New Square Feet (net)	New Square Feet (gross)	New Acres	New Acres	New Square Feet	Cumulative Acres	Cumulative Square Feet	New Acres	New Square Feet	Cumulative Acres	Cumulative Square Feet	New Acres	New Square Feet (net)	New Square Feet (gross)	Cumulative Acres	Cumulative Square Feet	Cumulative Housing Units		
Public Development Areas																				
Expansion Hall (50,000 net sq ft of open space) and outdoor venues at Fair, partial EMU development	1.5	50,000	72,000			1.6	72,000			1.6	72,000				1.6	72,000				
New Outdoor Arena/Outdoor Venues/Landscape	12.4					12.4				12.4					12.4					
New Fair Parking/Roads				2.2					4.0											
Existing Fair Facilities	4.5					14.5			-4.0	10.5					5.3					
Existing Fair Parking/Roads						4.5				4.5					4.5					
Subtotal - Fair	18.5	72,000		2.2		35.2	72,000	0.0		35.2	72,000	0.0		77,500	35.2	149,500				
Transit Center-Bus Docking				2.2		2.2			1.1	1.1					1.1					
Transit Center-Surface Parking/Bus Stop																				
Transit Center - Parking Structure									1.1	1.1	221,600									
Shared Public Parking - Surface ¹									24.7	24.7										
Temporary South Fair Parking	7.0					7.0		0		7.0										
Major Roads	2.5			4.0		6.5		7.8		14.3					14.3					
Subtotal Roads & Public Parking	9.5			6.2		13.5	0	25.5		41.2	221,600			800,000	41.2	921,600				
Creek Park (w/new water feature)				6.0		6.0		17.9		23.9					23.9					
Fairgrounds Channel (peripheral drainage)	0.0			6.0		6.0		17.9		23.9					23.9					
Subtotal Open Space & Waterways	28.0	50,000	72,000	14.4		54.7	72,000	43.4		76.4	193,600			877,500	100.3	1,071,100				
Private Development Areas ^{1,2}																				
Entertainment Mixed Use (0.2 FAR)	9.8		85,378			9.8	85,378	7.0		16.8	146,362									
Entertainment Mixed Use (0.4 FAR)				2.0		2.0	34,848			2.0	34,848									
EMU Parking Structure																				
Housing Units in EMU																		50		
Entertainment Commercial - venue area																				
Entertainment Commercial - parking area																				
Subtotal Public and Private	9.8		85,378	2.0		11.8	120,226	37.0		60,984	181,210			466,362	48.8	647,571				
Undeveloped Site & Overflow Parking	37.8			16.4		66.5		80.4		149.1					149.1					
TOTALS	157,378		157,378	34,848		149.1	197,226	182,584		449.1	253,210	50		1,343,862	149.1	1,718,671		50		

Table Notes:
 1. Housing is allowed within EC or EMU as a Conditional Use Permit from the City of Vallejo (see land use policies).
 2. Square foot totals do not include Entertainment Commercial uses, which may include both outdoor venues and buildings.
 3. Shared Public Parking serves the Fair and other entertainment venues; includes 19.7 acres of surface parking and a 5-acre (2,500 car) parking structure in Phase 3 (see parking program).
 4. Square footages include parking structures as noted.



the Creek Park with its water feature (see Section 3.4.4, below). As illustrated in Chapter Four, proposed Fairgrounds improvements include:

- A new Phase 1a Exposition Hall with approximately 50,000 net square feet of exposition space (approximately 72,000 gross square feet including meeting rooms, lobbies, restrooms and other support space) that will replace the existing Exposition Hall building; potential for expansion in Phase 3 to 100,000 net square feet (approximately 144,000 gross square feet).
- Improvements to the grounds, including a new Arrival Plaza and Midway/Event Lawn adjacent to the new Exposition Hall.
- Continuation of the east-west Creek Park including a water feature, trails and a pedestrian bridge.
- A family and student-oriented demonstration farm at the eastern terminus of the Creek Park.
- New promenades and plazas, an amphitheater, and other flexible open spaces.
- Parking facilities in the north (North Fair Parking) and south (Shared Public Parking) with separate gates that can serve multiple activities; additional parking, loading and vehicular circulation around the outer perimeter of the area.
- Phased upgrading and modification of existing buildings, as needed, with in-kind replacement of buildings that are no longer usable in their current physical condition.

Permitted uses are described in Section 3.5 of this chapter. Chapter Four provides additional descriptions of design concepts and phasing.

3.4.2 Transit / North Parking Center

The Plan proposes 2.2 acres for a transit/parking facility in the northwest area of site, with access from Sage Street and the North Loop Road. In Phases 1A and 1B, this site is expected to serve as surface parking. Phase 2 is proposed to include development of approximately half the site for a bus docking facility to serve commuters, with the balance of the site utilized for a multi-level parking garage that would serve commuters during the weekdays and provide overflow parking for entertainment uses on weekends.

3.4.3 Parking and Roads

The Plan allocates 24.7 acres for Shared Public Parking to support the continuing viability of entertainment uses within and near the Plan Area. Phase 2 includes proposed improvements to this area for surface parking of approximately 2,600 cars. In Phase 3, approximately five acres in the southern portion of the Shared Public Parking area is anticipated to be converted to a multi-level parking structure to support a higher intensity of entertainment and / or supporting commercial uses within the Plan Area.

The large-scale surface parking areas could include solar arrays to provide for onsite energy generation and a possible revenue source. Parking facilities are described further in Chapter Five.

The Plan proposes major roadways for access to all parcels and parking areas (see Chapter Five). These roadways have been sized and located to accommodate projected traffic demands generated by the build-out of the Plan Area. Major roadways would also include site infrastructure as described in Chapter Six.

Chapter Four provides additional descriptions of roads and parking areas.



3.4.4 Open Space

Figure 3.1: Land Use Plan indicates the location of open space proposed by the Plan. Chapter Four provides additional detail on the Creek Park, Fair-related outdoor areas, streetscape and other open space elements.

Creek Park, Water Feature and Public Entertainment Core

The Creek Park and its central water feature would extend east-west through the central portion of the Plan Area, forming the spine of a “Public Entertainment Core” that connects the Fair of the Future with the mixed use development areas, Entry Road, and Fairgrounds Drive (see Figure 4.4: Public Entertainment Core). The Public Entertainment Core is intended to provide an active gathering place with a waterside pedestrian trail, restaurants, public art, shops, and terraced seating. Rental of small pedal boats may be possible within the Fair portion of the Creek Park.

The Creek Park water feature is envisioned as a multi-purpose amenity that provides the key visual amenity within the Public Entertainment Core. The water feature is also designed to provide:

- Onsite stormwater hydro-modification (matching pre- and post-development runoff rates) will likely be required to meet water quality permit requirements.
- The ability to collect and use stormwater for onsite irrigation, which reduces potable water use (capture and reuse).
- Improve onsite stormwater quality prior to discharging water into downstream systems that lead to Lake Chabot.
- Cut material to fill the northwest portion of the Plan Area to alleviate existing flood plain issues.
- High “onsite lake” water quality by incorporating wetland planters, biofilters, aeration and circulation in addition to maintaining an appropriate water temperature through depth and water volume.

Onsite stormwater will be routed through the Creek Park water feature that will discharge into an existing storm drain system and then into Lake Chabot. Offsite stormwater flows from Rindler Creek and/or Blue Rock springs will not be diverted through the onsite water feature but will continue to flow through the Fairgrounds Channel.

Chapter Six (see Section 6.2 and Appendix F) describes the hydrological functions of the water feature in more detail. Chapter Four provides additional guidelines for landscape (see Section 4.4).

Fairgrounds Channel

Along the eastern, southern, and western boundaries of the site, the Plan sets aside acreage for the Rindler Creek drainage and adjacent buffer. The size and configuration of this area are designed to address area-wide storm drainage and flooding issues as described in Chapter Six. To improve habitat values and aesthetic appearance of this significant channel, the Plan proposes landscape and grading measures as described in Chapter Four.

In summary, the Fairgrounds Channel design provides:

- Alleviation of flood plain problems at the south end of the Plan Area, on Coach Lane, on Fairgrounds Drive and within the Newell mobile home park caused by the 3,300+ acre upstream watershed.
- A multi-level channel to provide riparian habitat and wetland benefits as well as flood protection (meandering low flow level, frequent storm event level and 100-year storm event level).



- A corridor for pedestrian trails.
- The ability to allow for a future creek restoration project (potentially with grant funds or other funding sources).

3.4.5 Entertainment-Mixed Use (EMU)

This land use is expected to include “Family Entertainment Centers” (FEC’s) as well as associated restaurant and retail activities. Examples of FEC anchor uses within the EMU area include John’s Incredible Pizza, Dave & Buster’s, and other businesses that combine eating, entertainment, small amusement park, gaming, animatronic shows, and similar uses, either within buildings and/or as outdoor venues.

FEC’s typically require parcels of one to seven acres, including surface parking provided within each parcel or nearby. Some parking lots may be available for joint use, according to the provisions of a Parking Operations Management Plan to be prepared separately by the County.

The Entertainment-Mixed Use parcels are clustered in the northern portion of the site in association with the Creek Park water feature and the Entry Road. Parcels may be combined or adjusted to respond to requirements of future users. As described in Chapter Four, EMU uses should orient entries and amenities to public streets, namely the Entry Road and Loop Road.

Initially, this land use is expected to develop at a density of 0.2 Floor Area Ratio (“FAR”), with limited higher density development proposed adjacent to the Creek Park. In Phase 3, a higher density of 0.4 FAR is proposed for the entire EMU area. A multi-level parking structure within the EMU area will be needed to accommodate this higher intensity of development.

A limited number of housing units may be permitted above the ground floor of FEC’s or other permitted uses (see Section 3.6.4).

Permitted uses are described in Section 3.5 of this chapter.

3.4.6 Entertainment-Commercial (EC)

The Entertainment-Commercial land use provides for a major entertainment use that requires a large single, undivided site of up to 30 acres in size. The Plan locates this parcel on the west side of the site, with major vehicular access from the South Loop Road/Fairgrounds Drive intersection, and with proximity to adjacent parking. This land use parcel provides the opportunity to create a future entertainment venue with a common entry or identity, with expanded and coordinated parking. The concept provides sufficient acreage for one large venue or multiple smaller venues to develop facilities over time. The EC use can include pedestrian gates at the northern and southern ends of the parcel, as suggested in Figure 4.3: Urban Design Elements.

Surface parking is proposed for the southern portion of the parcel through Phase 2. In Phase 3, joint use of the South Parking Garage (located within the Shared Public Parking area) would allow a portion of the EC surface parking to be replaced by expansion of the EC entertainment venues.

3.5 PERMITTED USES

The implementation of the Solano360 project will result in the conversion of existing Solano County Fairgrounds property to the City of Vallejo zoning designation, Mixed-Use Planned Development (MUPD). The intent and purpose of the MUPD Zoning designation for the Solano360 Plan Area is to allow flexibility for the entirety of the site, consistent with the Plan.

Under this proposed zoning designation, the following public uses will be allowed by right: Fair, public transit centers, parking facilities, drainage facilities, reclaimed wastewater facilities, and other infrastructure, roadways, and recreational open space.



Private uses allowed in the MUPD zoning include Entertainment Mixed-Use and Entertainment Commercial. The Entertainment Mixed-Use and Entertainment Commercial areas are designed to allow for a range of uses consistent with the amusement park and entertainment uses envisioned. In addition, office uses are allowed within private purpose development areas up to a total of 220,000 square feet of office space.

While the list described below is intended to be inclusive, additional uses may be proposed provided they meet the general intention of the Plan and are approved by the City Development Services Director.

Whenever the development regulations for private purpose areas contained herein conflict with those contained in the City of Vallejo Municipal Code, the development regulations contained within this Plan shall take precedence. When any issue, condition or situation arises or occurs for private purpose areas that are not specifically covered or provided for by these standards, those provisions in the Zoning Ordinance that are most similar to the issue, condition, or situation, as determined by the City Development Services Director, shall apply.

Permitted uses for the three primary land use areas (Fair, Entertainment Mixed-Use, and Entertainment Commercial) are as follows.

3.5.1 Permitted Uses – Fair

The area designated as “Fair” on Figure 3.1: Land Use Plan shall be used for fair and/or fair-related uses including, but not limited to, public gatherings, midway and thematic ride activities, trade and display shows, competitions and pageants, music and theater performances, trade industries and other organizational conferences. The parcels with a Fair designation will be limited to fair and fair-related activities, and may include commercial activities that generate rental income from Fair buildings and/or are associated with Fair activities, such as continuation of existing uses (e.g., day care facility, satellite wagering) and new uses (e.g., boat rentals for the water feature, operation of a theater venue at the future amphitheater).

Permitted Uses for the Fair area are:

- Fairgrounds and fair related uses, including thematic rides and mid-way entertainment.
- Exhibition and Exposition Halls.
- Amphitheaters.
- Natural resource areas, water channels, preserves and protective buffer areas.
- Public water features and trails.
- Public/private utility buildings, structures and facilities (as needed for infrastructure services).
- Recreational facilities, including parks, recreation areas and buildings for recreational use.
- Picnic facilities.
- Playgrounds and play apparatus.
- Playing fields and courts; participant sports and facilities.
- Public Transit Centers, park and ride lots, and related surface or structured parking.
- Surface and/or structured parking.
- Reclaimed wastewater facilities (under surface parking).
- Loading and servicing for fair-related events.



- Recreation Vehicles (RV) parks and storage.
- Private food or beverage concessions.
- Photovoltaic arrays or other energy-generating facilities.
- Agricultural uses, including demonstration farms.
- Other uses similar in nature that benefit the public and reinforce the viability of the Fair, as recommended by the Solano County Fair Association Board and approved by the Solano County Board of Supervisors.

Interim Uses for Fairgrounds

Prior to full buildout of the Plan Area, the Fair may operate interim uses on parcels not slated for development until later phases. These interim uses are expected to be limited in duration and may include the following:

- Commercial recreation activities such as go-carts or other land-intensive activities.
- Outdoor performances or events utilizing the existing grandstand or other existing facilities.
- Temporary signage and billboards.
- Parking.
- Any use permitted for the Fair, as described above.

Reconfiguration of Phase 1 parking and access will be allowed to accommodate these uses if determined practicable by the County.

3.5.2 Permitted Uses – Entertainment-Mixed Use and Entertainment Commercial

While the permitted uses for Entertainment-Mixed Use and Entertainment-Commercial areas are the same, the end users are expected to be different. EMU is expected to attract the smaller FEC-type businesses and associated retail activity described in Section 3.4.5, while the 30-acre EC site is intended to accommodate a larger destination amusement or theme park.

Permitted Uses are as follows:

- Amusement Park Recreation.
- Amusement Park Retail.
- Specialty Entertainment Restaurants.
- Eating Establishments: restaurants and bars, fast food outlets (drive-through restaurant facilities and services are prohibited), delicatessens and snack bars.
- Specialty Retail stores.
- Outlet Retail stores.
- Entertainment including theaters; amusement centers, and indoor and outdoor participant sports facilities.
- Commercial Offices including but not limited to establishments that provide financial, insurance, real estate, legal, medical services, marketing management, architectural and engineering design, and other comparable professional services and support services; also Business Services including administrative and professional services, business support services, research services, telecommunications facilities, gas and electric services, correspondence schools and vocational schools, educational services, public administrative services, and research and development. Business Services and



Commercial offices are permitted up to a maximum of 220,000 square feet; these uses would substitute for other EMU uses.

- Surface and/or structured parking: public and private. Parking facilities may include photovoltaic arrays.
- Photovoltaic arrays or other energy-generating facilities
- Other compatible uses as approved by the City Development Services Director.
- Conditional Uses for EMU and EC
- The following uses are allowed with approval of a Conditional Use Permit by the City of Vallejo:
 - Lodging: hotels and motels (transient habitation) and bed and breakfast inns.
 - Wholesale trade.
 - Amphitheaters.
 - Recreation Vehicles (RV) parks and storage.
 - Up to 50 housing units.

3.6 LAND USE POLICIES

The Plan land use regulations and policies provide for the orderly and efficient development of the Plan Area and create a flexible range of uses while avoiding land use conflicts. Chapter Four: Urban Design and Guidelines provides additional criteria.

3.6.1 Overall Policies

- Development within the Plan Area shall be consistent with the Land Use Objectives of this chapter and the design provisions of Chapter Four.
- Development standards for Private Purpose Areas, including building heights and setbacks, shall be determined during subsequent entitlements as described in Chapter Seven: Implementation. Development standards will reflect the guidelines and other provisions of Chapter Four: Urban Design and Guidelines.
- The Plan shall permit the maximum amount of development in the Plan Area allowed by implementation of required mitigations, including onsite and offsite infrastructure.
- Onsite and offsite infrastructure shall be developed concurrently with project development, so that requirements for transportation, water, and other facilities are provided with each phase of development (see Section 3.7.4 for phasing policies).
- A total of up to 222,000 square feet of office uses is permitted within private purpose parcels and will substitute for other EMU development. Proposals for additional office space must be reviewed by the City and may be subject to additional environmental review.
- Within the Private Purpose Area, permanent surface parking may not be located adjacent to the water feature or Creek Park in order to maintain the open space character of these features. Within EMU parcels located between Entry Road and Creek Park, parking should be limited to handicapped and emergency parking and set back a minimum of 40 feet from Creek Park. Within the Entertainment Commercial parcel, vehicular parking and service areas should be set back a minimum of one hundred feet from Creek Park.



- The Plan shall permit adjustments to and flexibility within the phasing of development in the Plan Area, subject to required mitigations, including onsite and offsite infrastructure.

3.6.2 Entertainment Mixed Use Policies

- EMU buildings are intended to consist primarily of ground-floor commercial (retail, restaurant, or entertainment) uses with possible incidental office and/or residential space occupying upper stories of multi-level buildings. The primary retail/restaurant activity zone is intended for the Public Entertainment Core along the Entry Road and Creek Park, with a wider range of uses encouraged along the North Loop Road.
- To reinforce a walkable character, minor amounts of neighborhood-serving retail, such as food, grocery or drug stores, are encouraged within the EMU area to provide for the needs of potential Plan Area residents, employees, and/or hotel guests as well as visitors in need of such services.
- Development rights may be transferred between EMU parcels, provided that adequate parking is provided for the overall EMU area (including provisions for shared use) and the total Plan development program and infrastructure capacities are not exceeded. In addition, buildings should be oriented with entries and primary facades facing the Entry Road and Loop Road as indicated by Figure 4.1: Illustrative Plan.
 - The Phase 3 parking structure may be located within any EMU parcel adjacent to the North Loop Road and/or Sage-Loop Connector Road, but should not be located south of the Entry Road or adjacent to the water feature. Primary access into the parking structure should be from the North Loop Road or Connector. The parking structure should contain ground-level retail/commercial development along North Loop Road in order to avoid the appearance of a stand-alone parking garage.
 - Development of additional EMU development beyond the thresholds identified by the Plan and EIR would be subject to the appropriate environmental review and certification, including any required mitigation measures.

3.6.3 Entertainment Commercial Policies

- Entertainment Commercial structures, outdoor rides, and other entertainment attractions are encouraged to be concentrated in the northern portion of the EC parcel, in order to make use of high visibility from adjacent streets and create attractive views from project gateways and the Creek Park.
- Parking is anticipated to be located in the southern portion of the parcel, with easy access to the South Loop Road entrance from Fairgrounds Drive. No parking will be located adjacent to the Creek Park.
- In Phase 2, the Entertainment Commercial parking is proposed to be provided within the EC parcel. This is assumed to require approximately 40% of the 30-acre site, reflecting typical surface parking ratios derived from evaluation of similar entertainment venues.
- In Phase 3, the built venues may expand into the EC parking areas providing that adequate parking is available within the Plan Area, either within the Shared Public Parking area or within the EC parcel itself (see Section 5.3).
- Taller structures should be concentrated in the center of the EC parcel, as described by the design guidelines (see Section 4.5).
- Should the EC area be developed as a multi-parcel, mixed-use commercial center with



new roads, the land use and design provisions for EMU areas will apply. Conversion of the EC area to more intensive mixed-use development shall not be permitted if such conversion exceeds the infrastructure capacities described in this document and in the Solano360 EIR.

- Development of additional EC development beyond the thresholds identified by the Plan and EIR would be subject to the appropriate additional environmental review and certification, including any required mitigation measures.

3.6.4 Residential Policies

- Residential use of the Plan Area is intended to be subordinate to, and integrated with, the principal and conditionally permitted EC and EMU uses.
- Residential development requires Conditional Use Permit approval by the City of Vallejo.
- Approximately 50 dwelling units are allowed under this Plan.
- If more than 50 dwelling units are proposed, the additional units over 50 will be subject to the appropriate additional environmental review and certification, including any required mitigation measures.
- Residential units will displace an equivalent square footage of allowed commercial development.
- To avoid potential health risks associated with freeway emissions, as identified by Bay Area Air Quality Management District (BAAQMD) air quality modeling, residential uses should be located in the western portion of the project site. Additional air quality studies should accompany proposals to develop residences within the Plan Area.

3.7 PHASING

Phasing plays a key role in the programming and physical development of this long-term, multi-use project. Each phase must be flexible yet stand on its own, while accommodating future expansion and intensification of development activities.

Figures 3.2 to 3.5 illustrate the potential phasing for full buildout of Solano360 land uses (Phases 1A and 1B, 2 and 3). Figure 3.6 illustrates the related phasing for infrastructure and preparation of graded pads with utilities. Table 3.4 provides a tabulation of land uses by phase. Chapter Five and Table 5.1 provide additional information on phasing of parking facilities.

Phasing is contemplated according to the following approximate timeline, which is subject to change depending on market conditions and development opportunities.

- Phases 1a and 1b: 1 to 5 years.
- Phase 2: 6 to 15 years.
- Phase 3: 16 to 25 years.

The land use and site development program for the project phases is dictated by the desire to establish a strong initial character for the project, provide logical and cost-effective investments in infrastructure, support development of the Fair of the Future and enhance the marketability of private purpose areas. Each phase proposes a level of development that can be accommodated by the associated onsite and offsite infrastructure capacity. The intent of proposed phasing is to establish the ability to intensify land uses over time through structured parking and transit solutions that allow for higher floor area ratios in the later phase of build-out.

Phasing of development uses and related infrastructure is summarized as follows. The information



is this chapter is informed by the Plan's conceptual site plans and may be subject to change as more detailed plans and specifications are developed as part of the design and development process.

3.7.1 Phases 1a and 1b

For purposes of establishing more detailed phasing and costing, Phase 1 has been divided into two sub-phases (Phases 1a and 1b) as detailed below, in Table 3.4, and in Figures 3.2 and 3.3. References in this Plan to "Phase 1" assumes Phases 1a and 1b together.

Fair of the Future

Phase 1a:

- First phase of Exposition Hall, including approximately 50,000 net square feet of exhibition space combined with meeting rooms, lobbies, café, circulation space and restrooms for a total of 72,000 square feet.
- Fair open space venues, including the Midway/Events Lawn with Terrace Steps, Arrival Plaza/Main Gate, East Plaza, Creek Park and water feature, South Gate and Demonstration Farm (see Figure 4.11: Fair Illustrative Plan – Phase 1).
- Minor perimeter road improvements.
- Interim North and South Fair Parking.
- Demolition of existing Expo Hall, concourse restrooms, and other site/utility features.
- Mass grading and drainage improvements (including placing fill material to raise the ground elevations above the existing floodplain elevation).

Roads and Other Public Purpose Areas

Phase 1a:

- Entry Road, streetscape and intersection with Fairgrounds Drive.
- Surface parking at Transit/North Parking Center, with a new bus stop along Sage-Loop Connector Road.
- Creek Park and water feature within public development area

Phase 1b:

- Northern segments of the Loop Road and intersections with Fairgrounds Drive.
- Sage-Loop Connector Road including permanent bus stop and intersection with Sage Street.
- On-site intersections and streetscape.
- Creek Park including the water, trails, and open space within private development area.

Both Phases 1a and 1b:

- Retention of existing road access opposite the Six Flags Discovery Kingdom parking lot, with continued use of existing undeveloped parking.
- Retention of existing grandstand (to postpone demolition costs and support possible interim Fair activities)
- Site, utility and building demolition, mass grading, drainage improvements and backbone utilities for the above (including placing fill material to raise the ground



- elevations above the existing floodplain elevation).
- Relocation of existing public utilities (sewer, water and gas transmission).
- Temporary parking south of the creek park/water feature area
- Private Purpose Areas
- Phase 1 a:
 - Preparation of nearly all Entertainment Mixed Use (EMU) parcels as graded parcels with roadway access and utilities.
 - Initial 9.8 acres of EMU development.
 - Balance of EMU parcels available for overflow parking as needed.
 - Site and utility demolition for the above.
- Phase 1 b:
 - Two additional acres of EMU development.
 - Construction of Creek Park and water feature.

3.7.2 Phase 2

Phase 2 includes buildout of the entire Plan Area with designated uses at an intensity permitted by surface parking.

Fair of the Future

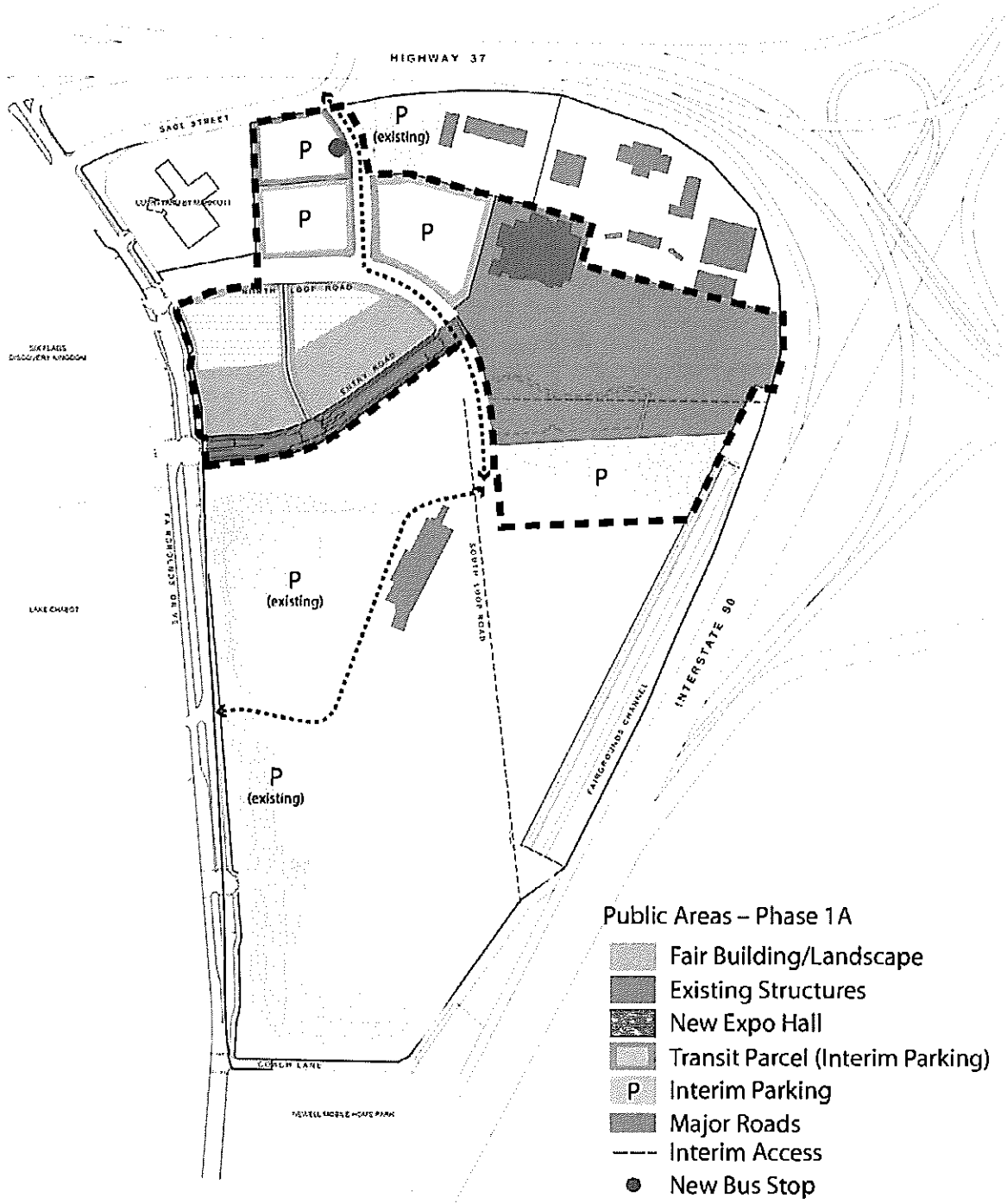
- Construction of expanded North Fair Parking and associated demolition of Administrative Office, Security Office, and Directors Trailer (relocation of these uses to portables if not accommodated in Phase 1 Expo Hall) and demolition of County Building.
- Continuing minor upgrades to existing facilities.
- Site and utility demolition, mass grading, and drainage improvements for the above.

Roads and Other Public Purpose Areas

- Transit Center Bus Docking and Parking Structure.
- Fairgrounds Channel improvements to address site floodplain issues.
- Completion of South Loop Road and intersection with Fairgrounds Drive.
- Shared Public Parking (surface parking lots and possible photovoltaic arrays).
- Site, utility and building demolition, mass grading, drainage improvements and backbone utilities associated with the above.
- Relocation of existing public utilities (sewer, water and gas transmission).

Private Purpose Areas

- Preparation of Entertainment Commercial (EC) parcel and Shared Public Parking parcel as graded parcels with roadway access and utilities.
- Additional EMU development to utilize all EMU parcels with onsite surface parking.
- Development of Entertainment Commercial (EC) 30-acre parcel with onsite surface parking.

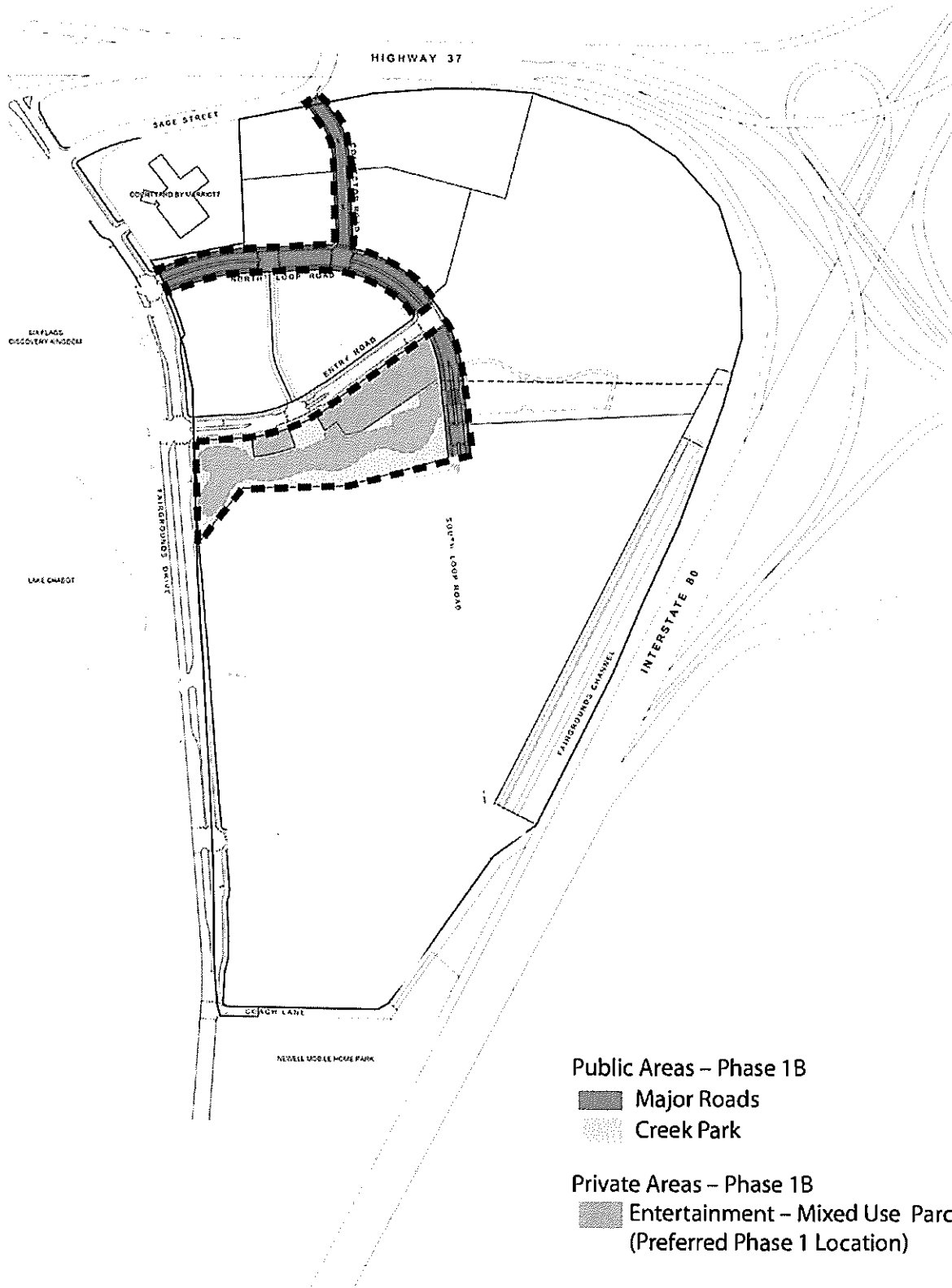
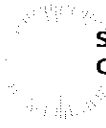


NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR-37/Redwood interchanges, as modified for Solano360

- Public Areas – Phase 1A**
- Fair Building/Landscape
 - Existing Structures
 - New Expo Hall
 - Transit Parcel (Interim Parking)
 - Interim Parking
 - Major Roads
 - Interim Access
 - New Bus Stop

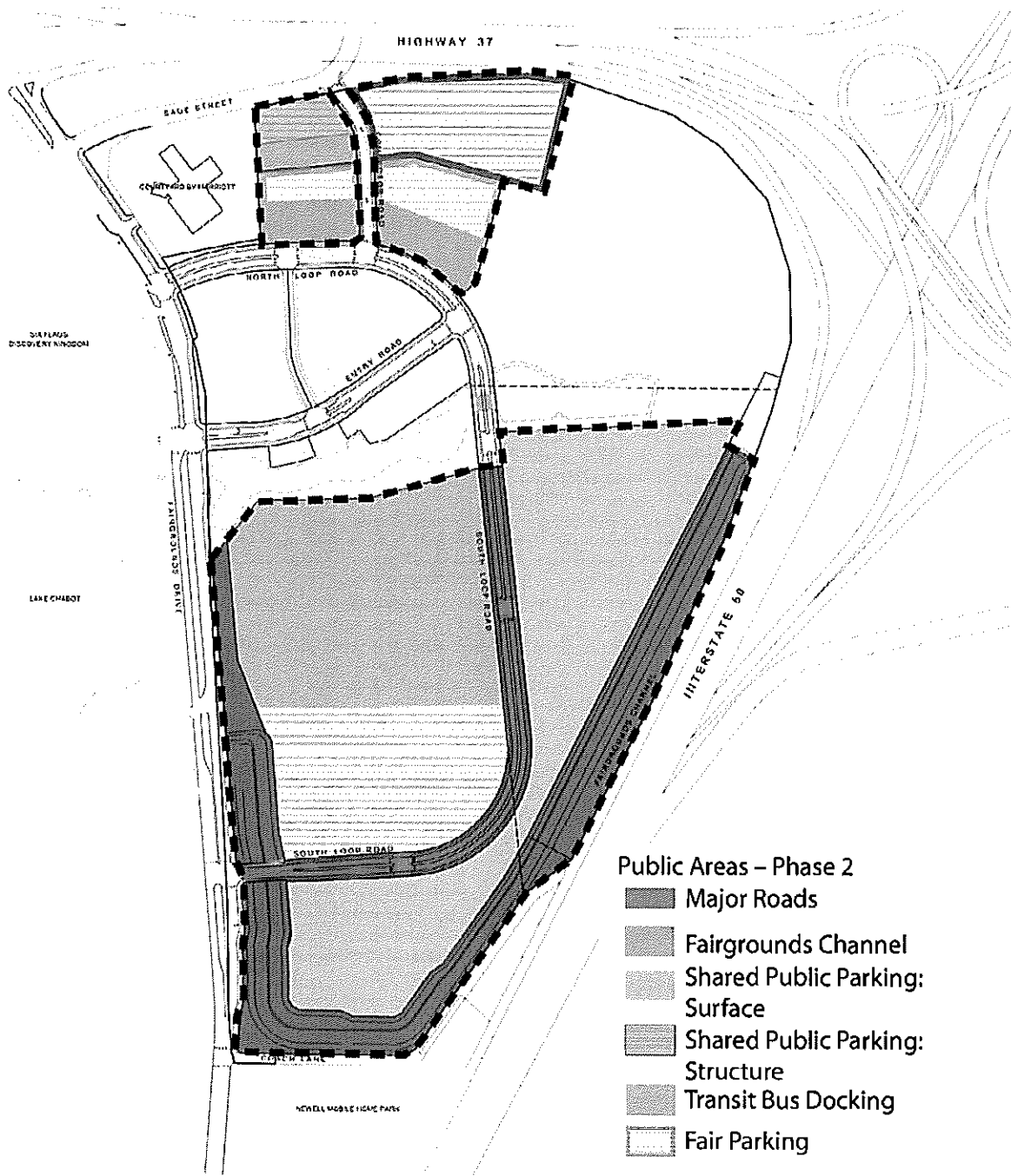
- Private Areas – Phase 1A**
- Entertainment – Mixed Use Parcel (Preferred Phase 1 Location)
 - Entertainment – Mixed Use (Phase 1 Parking)
 - Entertainment – Mixed Use (Interim Parking)

Figure 3.2: Phase 1A Projects



NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR-37/Redwood interchanges, as modified for Solano360

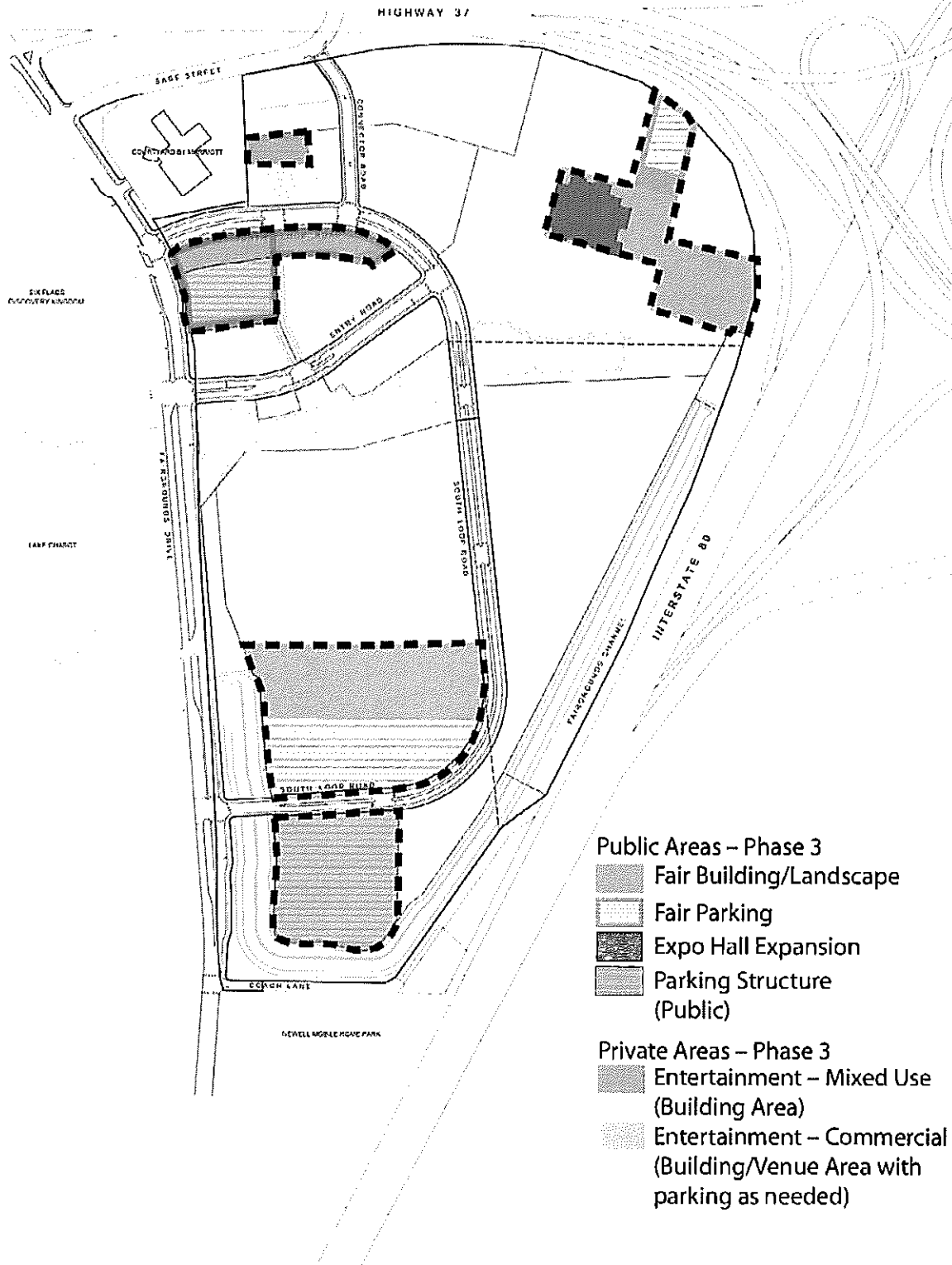
Figure 3.3: Phase 1B Projects



- Public Areas – Phase 2**
- Major Roads
 - Fairgrounds Channel
 - Shared Public Parking: Surface
 - Shared Public Parking: Structure
 - Transit Bus Docking
 - Fair Parking
- Private Areas – Phase 2**
- Entertainment – Mixed Use (Building Area)
 - Entertainment – Mixed Use (Parking Area)
 - Entertainment – Commercial (Building/Venue Area)
 - Entertainment – Commercial (Surface Lots)

NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR-37/Redwood interchanges, as modified for Solano360

Figure 3.4: Phase 2 Projects



NOTE: Plan accommodates future improvements to Fairgrounds Drive and SR-37/Redwood Interchanges, as modified for Solano360

Figure 3.5: Phase 3 Projects

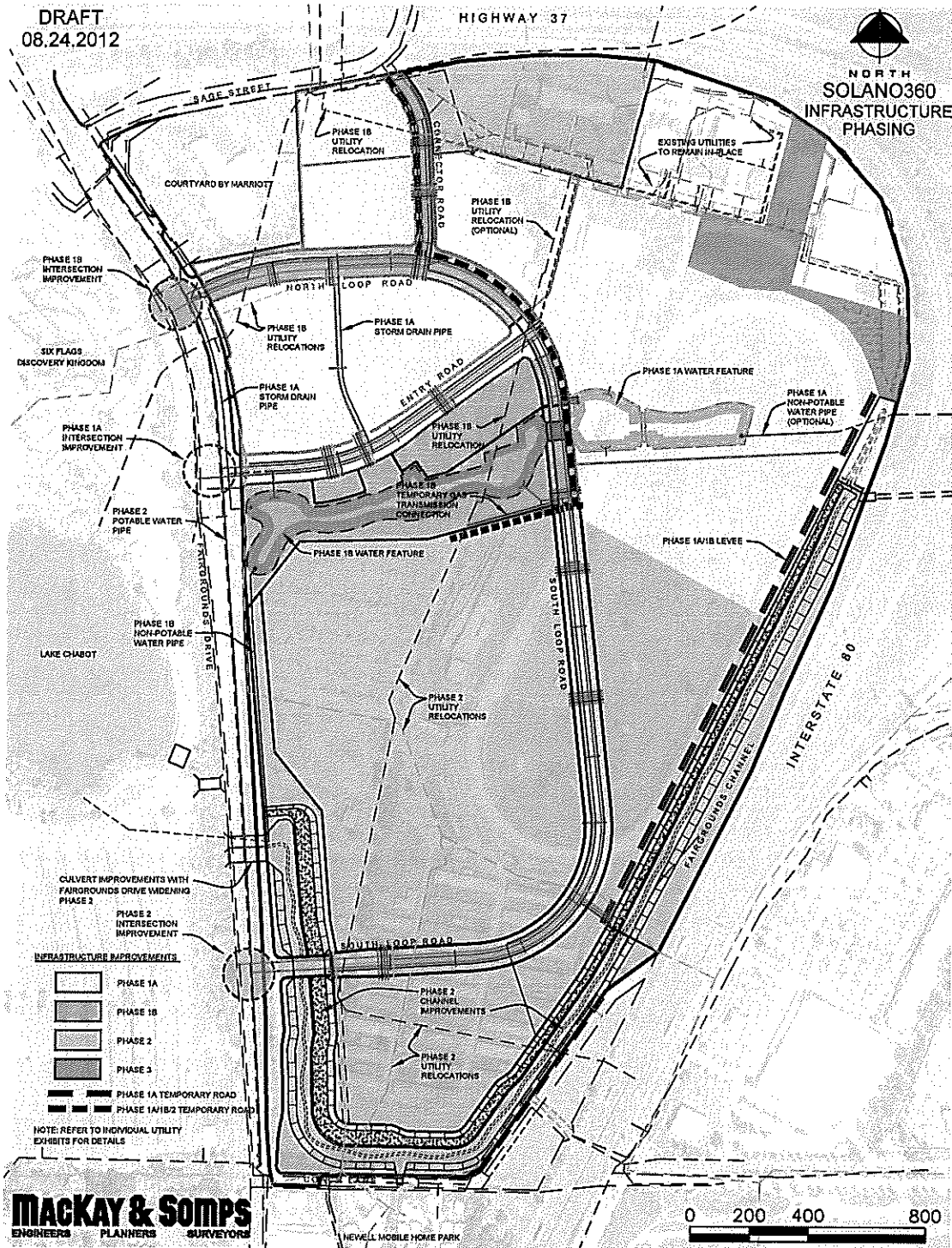


Figure 3.6: Phasing of Infrastructure



- Site and utility demolition for the above.

3.7.3 Phase 3

Phase 3 assumes the further intensification of the Plan Area with the addition of structured parking to allow more intensive infill and expansion of uses, including the enlargement of the Exposition Hall to approximately 100,000 net square feet of exposition space. See Section 5.3 for further discussion of parking.

Fair of the Future

- Additional 50,000 net square foot expansion of Exposition Hall.
- Associated outdoor promenades and connections.
- Demolition of existing concert venue; construction of new amphitheater.
- Demolition of Civic Building; new parking and maintenance area.
- Site and utility demolition, mass grading, and drainage improvements associated with the above.

Roads and Other Public Purpose Areas

- South Parking Garage (in Shared Public Parking).

Private Purpose Areas

- Intensification of EMU development with addition of EMU Parking Garage.
- Expansion of EC venues and joint use of Shared Public Parking/South Parking Garage.

3.7.4 Phasing Policies

Overall Phasing

- Project phasing should:
 - Establish the Public Entertainment Core, including the Creek Park and Entry Road, in the initial stages of development in order to create a strong and appealing sense of place,
 - Prioritize upgrade of Fairgrounds facilities, including a new Exposition Hall and complementary outdoor venues,
 - Allow logical and cost-effective construction and extension of infrastructure,
 - Continue to provide parking opportunities for nearby major entertainment uses,
 - Make best use of existing infrastructure, including maximizing the capacity of the existing Fairgrounds Drive/SR-37 interchange, and
 - Prepare parcels and site improvements to accommodate near-term market opportunities, while maintaining flexibility for later phase development.
- The phasing plans shown in Figures 3.2 to 3.6 are intended to guide efficient staging of development that makes best use of infrastructure and creates a strong initial character for the Plan Area. However, phasing may be modified to respond to changing market conditions and development opportunities, provided that adequate onsite and offsite infrastructure improvements are made available to accommodate the pace of development, and the impacts of the project do not exceed the levels analyzed by the EIR.



- Development of the Plan Area in excess of thresholds identified by the Plan and EIR would be subject to the appropriate additional environmental review and certification, including any required mitigation measures.
- Any changes to the phasing program must be approved by the County and City to ensure that the provisions of financing, fiscal, and cost sharing agreements are not adversely affected.
- Phasing should facilitate the replacement and upgrading of older fairgrounds facilities that no longer provide a competitive advantage for attracting users.
- Infrastructure improvements, including transportation, site drainage, and utilities, should be provided before or as part of development uses within the Plan Area in order to ensure a safe and orderly development process for each phase. The provision of infrastructure should be reviewed as part of subsequent entitlements through the County or City, with coordination between agencies to insure adequate services for each phase of development (see Chapter 7).
- Parking facilities and parking management/transportation management strategies should be phased to serve the needs of development areas within the Plan Area and the nearby major entertainment uses. Phasing of parking is addressed further in Section 5.3. Usage and financial terms will be defined by a Parking Operations Management Plan to be prepared by the County and by parking agreements between the County and Six Flags Discovery Kingdom.

Phase 1 Priorities

- The first phase of the project (Phases 1a and 1b) should be designed and implemented to provide a high level of amenity features to establish an appealing, highly marketable setting. These features include the landscape and site improvements proposed for the Entry Road, Creek Park and water feature, Fair of the Future, and other public areas.
- To the extent possible, the first phase should be concentrated in the northern portion of the Plan Area in order to a) establish a "critical mass" that builds on the concentration of existing and proposed Fair buildings and facilities, b) create cost efficiencies in the extension of roads and utilities, c) make best use of the proposed water feature and other amenity features, and d) integrate with existing nearby uses.
- To attract family entertainment and similar users, the first phase should include installation of horizontal improvements and backbone infrastructure and creation of parcels that are readied for vertical development.





CHAPTER FOUR: URBAN DESIGN AND GUIDELINES

4.1 INTRODUCTION

This chapter sets forth urban design concepts and guidelines to shape and facilitate redevelopment of the Plan Area, consistent with the Guiding Principles and land use provisions described above. The intent is to create an exciting, synergistic fusion of entertainment, fairgrounds, and mixed use destinations that builds on the regional visibility of the Plan Area and supports the ongoing success and long-term viability of the Solano County Fair, new Entertainment Mixed Use and Entertainment Commercial uses, and nearby major entertainment uses.

These design guidelines address both overall issues of site development and detailed issues of landscape, building form, walls and fences, and signage. Illustrative plans, photos and other materials are intended as guidelines and examples for review of future building approvals. Lastly, sustainability guidelines are included that both summarize sustainable project elements and provide suggestions for future development.

To assist future users of these design provisions, the following chapter contains separate sections for:

- The overall Plan Area,
- The Fair of the Future (Fairgrounds),
- Other Public Purpose Areas (Major Roads, Creek Park, Fairgrounds Channel, Transit/North Parking Center, and Shared Public Parking), and
- Private Purpose Areas (Entertainment Mixed Use and Entertainment Commercial parcels).



The information in this chapter is informed by the Plan's conceptual studies and may be subject to change as more detailed plans and specifications are developed as part of the design and development process. More detailed design guidelines will be incorporated into a Development/Implementation Agreement between the County of Solano and the City of Vallejo.



4.2 PLAN AREA DESIGN

4.2.1 Urban Design Concepts

The Land Use Plan (Figure 3.1) establishes a framework for the Plan's proposed urban design features. The intent is to create a seamless integration of public and private areas, including Fairgrounds facilities and private mixed use development.





Guidelines are as follows:

- The Public Entertainment Core, the defining feature of Solano360, encompassing a lively, mixed use entertainment corridor connecting from the gateway at Fairgrounds Drive in the west to the demonstration farm at the Fair’s eastern edge. The Public Entertainment Core includes:
 - The Creek Park with its walkways, promenades, plazas and bridges,
 - The Creek’s Park’s central water feature that connects public and private area and provides multiple benefits including visual amenity, wateredge promenades, onsite stormwater hydromodification, capture and reuse of stormwater for irrigation, and water quality treatment,
 - The thematic “Main Street” or Entry Road aligned with Creek Park, terminating at the new Exposition Hall and offering wide urban sidewalks and a pedestrian-friendly frontage for a “restaurant row”, retail associated with entertainment uses, and gathering areas, and
 - Within the Fair, a major Arrival Plaza at the entrance to the Exposition Hall, a Midway/Event Lawn with terraced seating, the water feature and Creek Park with pedestrian bridge, and a demonstration farm oriented toward families and school groups.
- Indoor and outdoor venues for the Fair of the Future, fostering a year-round program of activities within a variety of active and passive spaces.
- Transformative Phase 1 project that includes the Creek Park with its water feature and creates a new Exposition Hall located as a focal point for the Entry Road.
- Strong relationship to nearby major entertainment uses via roadway and pedestrian connections, including integrated design elements and synergistic land use opportunities.
- Pedestrian, bicycle and transit connections integrated into streets and open space systems.
- Creation of a Rindler Creek drainage and adjacent buffer along the eastern, southern and western boundaries of the site to alleviate floodplain issues, establish riparian habitat and wetland benefits, and provide the opportunity for pedestrian trails.

These features are described further in this chapter and in Chapters Five and Six.

4.2.2 Access and Circulation

Connections to the Plan Area

Figure 4.7 illustrates key features relating to site access, parking, and entries.

The configuration of roads, entries and parking is intended to facilitate efficient access to parking facilities while focusing views on the Creek Park and other destinations, with attractive streets defined by buildings.

Because the Plan Area has a direct, physical connection to Six Flags Discovery Kingdom, the project has also been designed to establish a strong pedestrian character to encourage walking between the theme park and the Fair of the Future. Visitors to the Plan Area will be able to park, shop, dine, relax and visit Fair programs with the option of walking or taking a shuttle.



Connections within the Plan Area

The Plan proposes an integrated system of internal connections that encourages shared use, walking, bicycling and transit. Features include:

- Walkable grid of tree-shaded sidewalks, including special Entry Road streetscape (see Figures 4.24 to 26).
- Pedestrian trails within the Creek Park, connecting to continuous perimeter trail along the Fairgrounds Channel.
- Multi-use paths along the South Loop Road, connecting parking areas with the Public Entertainment Core.
- Continuous perimeter trail for the south area of the Plan Area as shown on Figure 5:10.
- New promenades and plazas within the Fair of the Future.
- Raised intersection and pedestrian crosswalks at the Entry Road/Loop Road to calm traffic and provide safe pedestrian crossings.
- A potential parking shuttle serving internal destinations and connecting to Six Flags Discovery Kingdom and the Transit/North Parking Center (see Figure 5.15: Transit and Shuttle Routes).

Accessibility

According to the Americans with Disabilities Act of 1990 "ADA" standards, new facilities constructed by, on behalf of, or for the use of a public entity must be designed and constructed in such manner that the facility or part of the facility is readily accessible to and usable by individuals with disabilities.

Public purpose areas within Solano360 will be designed to provide for ADA access according to applicable ADA Standards for Accessible Design.

4.2.3 Landscape Plan and Guidelines

Figure 4.8: Landscape Character illustrates the location and variety of landscape areas and public spaces envisioned for the Plan Area, including:

- Streetscape planting.
- Buffer/riparian planting along the Fairgrounds Channel, using species that are compatible with the flood control function of the channel.
- Planting along soft or earthen water edges.
- Park landscape.
- Turf, both regular and reinforced (such as with mesh reinforcement material).
- Rain gardens.





- Demonstration Farm.
- Hardscape and plaza areas (including the Fairgrounds Concourse).
- Terrace seating at grade changes along the Creek Park water feature and in the Fairgrounds amphitheater.
- Surface parking areas.

Specific guidelines for Fair property landscape features as well as for the Fairgrounds Channel and Creek Park are included in Section 4.3: Fair of the Future and Section 4.4: Other Public Areas, respectively. The following general guidelines apply to the Plan Area as a whole.

Street Character

- Hardscape and plazas should be paved attractively, with paving patterns and materials conducive to pedestrian circulation and gathering.
- Tree planting should be designed to create shaded areas, especially in public areas such as sidewalks, parking lots, roadways, courtyards, plazas and parks.
- Trees along the Entry Road and at the Arrival Plaza should be of a different character than the streetscape trees on the other roads, and should be planted in tree grates.
- Street trees should be placed in park strips between the curb and sidewalk as shown by Figures 4.24 to 4.26.
- Parkway strips and medians should be planted with a variety of drought-tolerant species.
- Contrasting tree species should be used for perimeter trees and trees along pedestrian corridors and hardscape areas to clearly identify paths of travel.
- Street trees should be spaced at approximately one tree per 25 feet, or less if smaller trees are used.
- Trees for major streets should be a minimum of 24-inch box container size. Fifteen-gallon container size may be used for minor streets and buffers.

Planting Criteria

- Plant materials should be selected from the plant palette in Appendix E: Solano360 Plant Palette. Substitutions or additions may be considered based on the suitability of the species in terms of similarity of form, adaptability, tolerance to site soils, climatic conditions or water quality, or other pertinent characteristics. The plant list is not intended to be exhaustive but to provide a clear guide for selection. Additional plants may be used that are compatible with this list and are consistent with the intent of these guidelines.
- In order to establish a unique and cohesive image for the Plan Area, a limit range of plant material should be used for public roads, park and common areas, commercial sites, and the Fairgrounds. For these areas, the intent is to employ a limited number of plant species for the majority of the planting in each identified area.
- Plant materials should be selected to be at an appropriate scale for the surrounding area when at mature size. Larger, more dramatic species should be utilized for important public areas such as the Public Entertainment Core, major entries, and Loop Road.
- Plant materials should be selected to meet the criteria listed below.

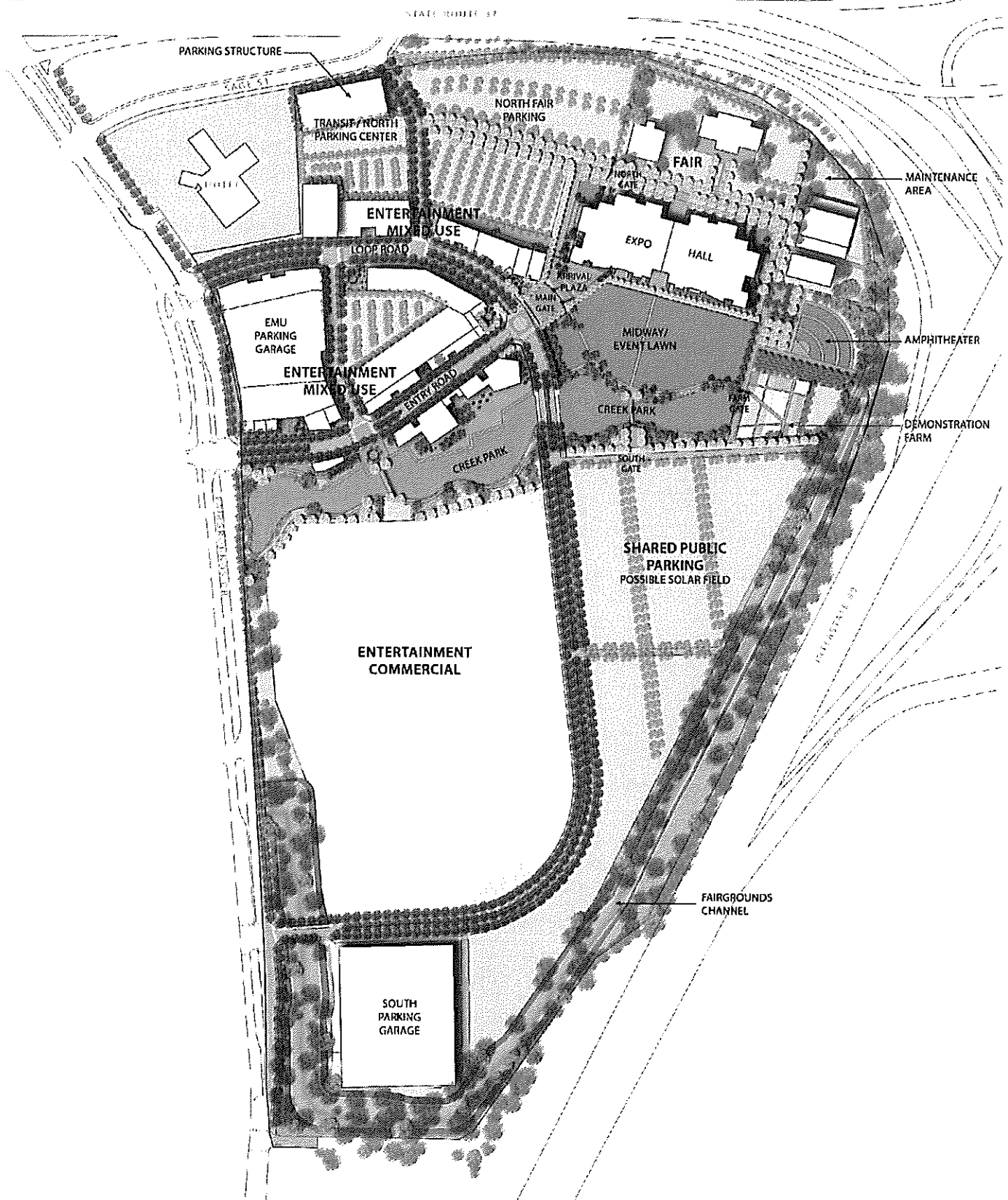
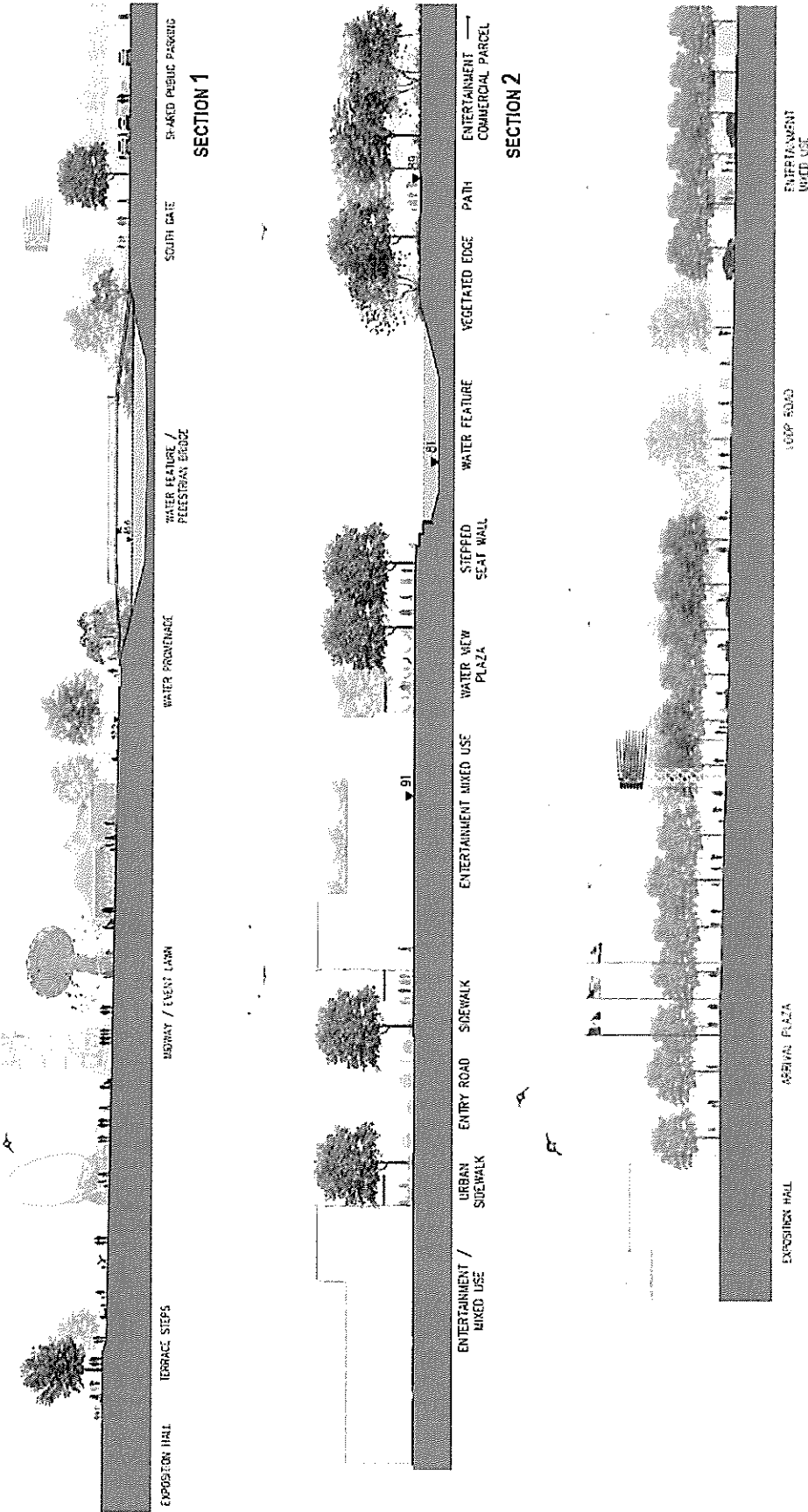
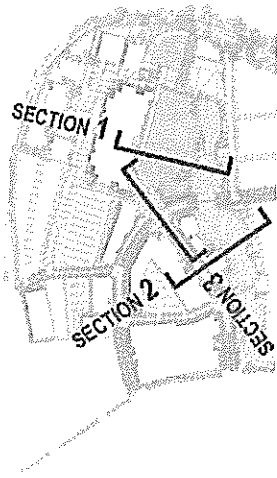


Figure 4.1: Illustrative Plan
Building areas depicted here are conceptual only.





Sections through Creek Park & Water Feature (at Fair and at Entry Road)

Figure 4.2: Illustrative Sections
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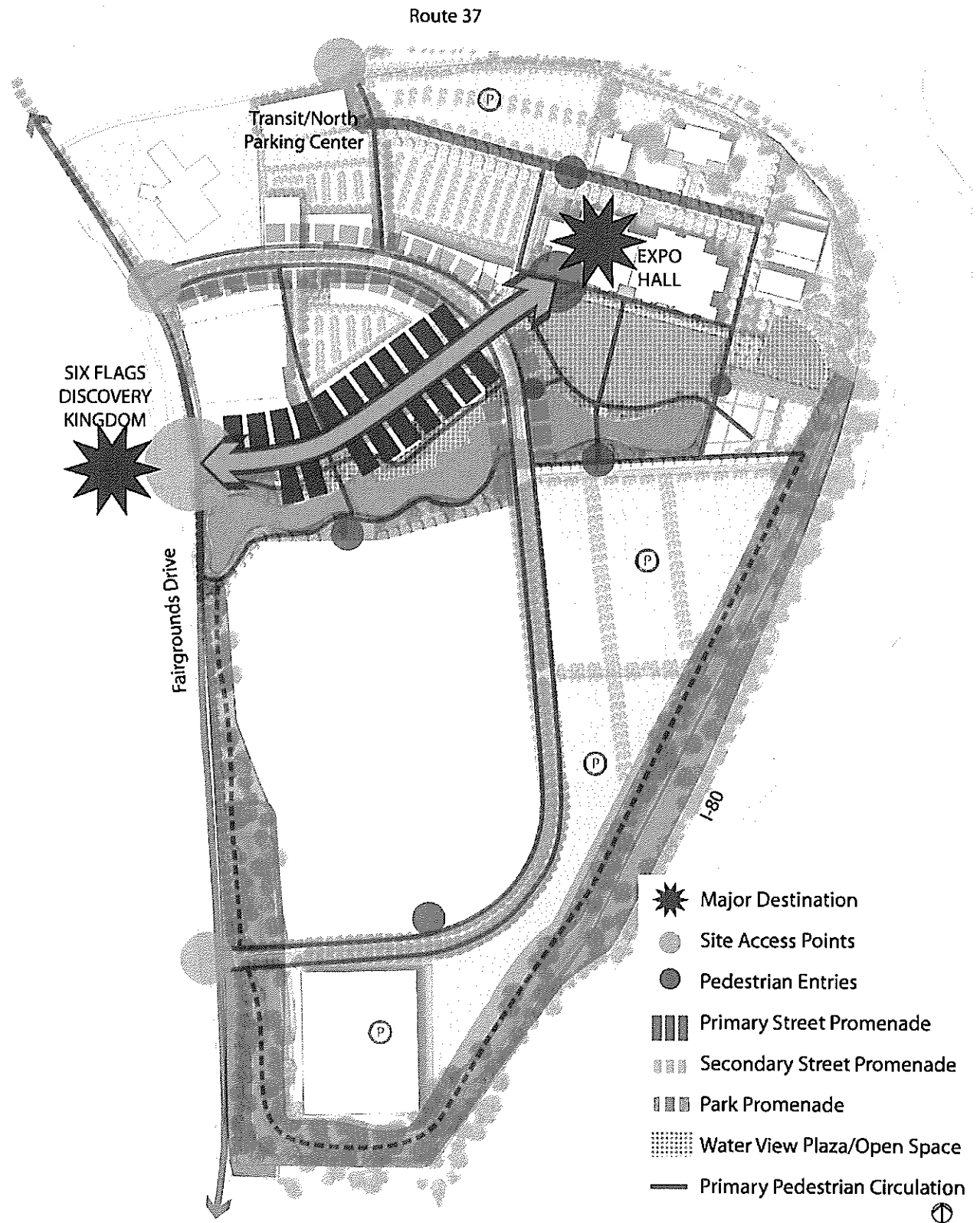


Figure 4.3: Urban Design Elements
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Route 37

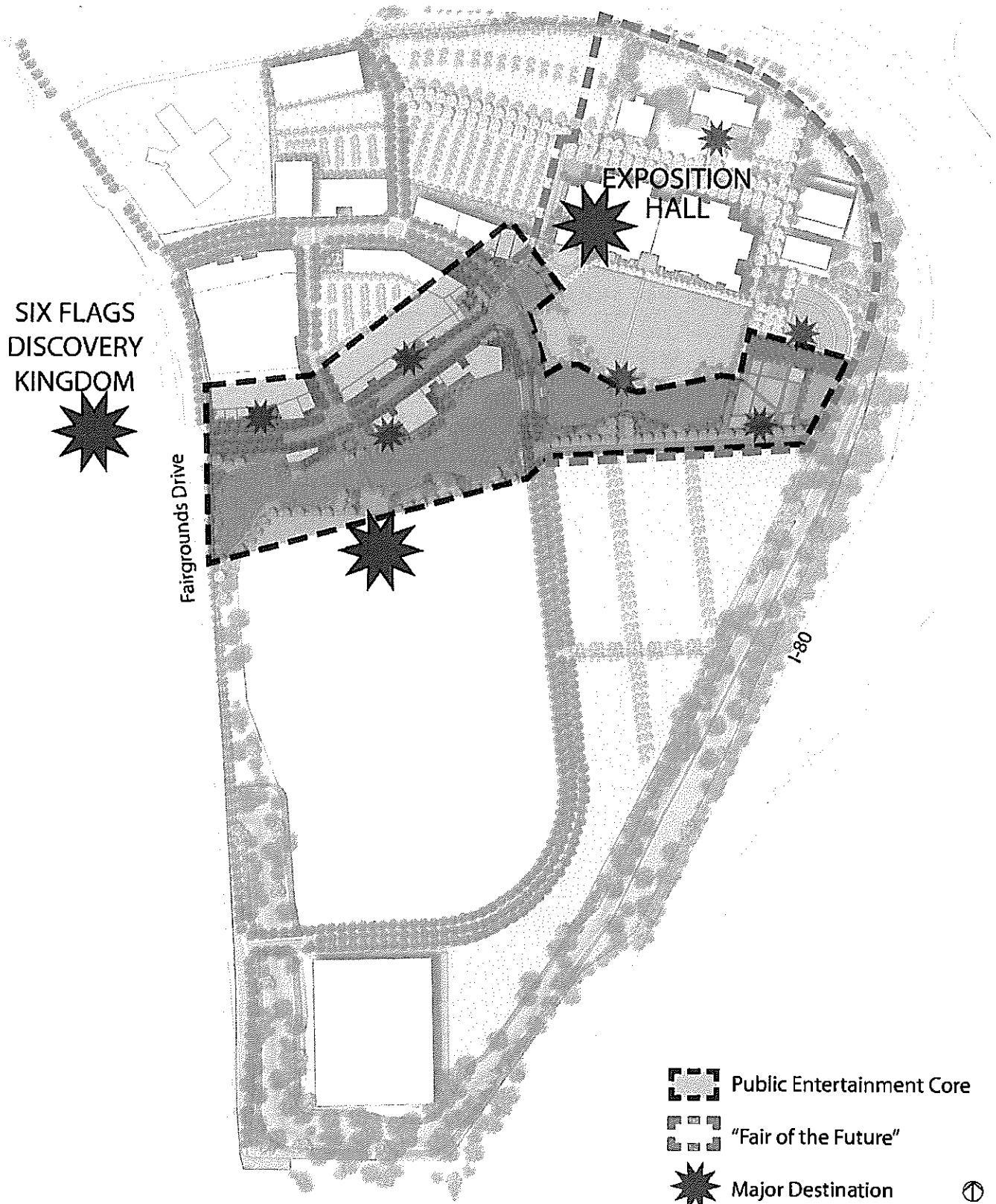


Figure 4.4: Public Entertainment Core

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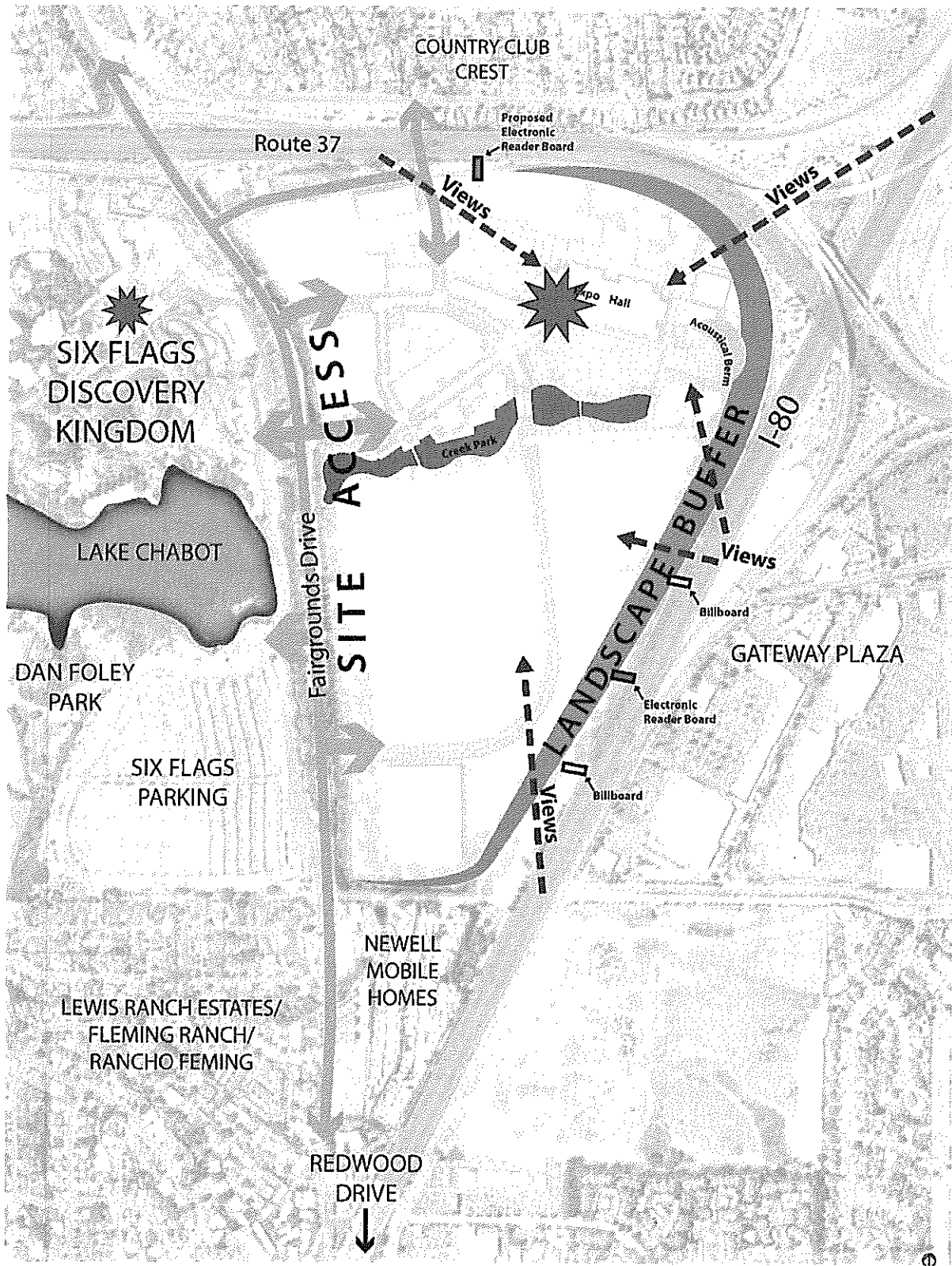
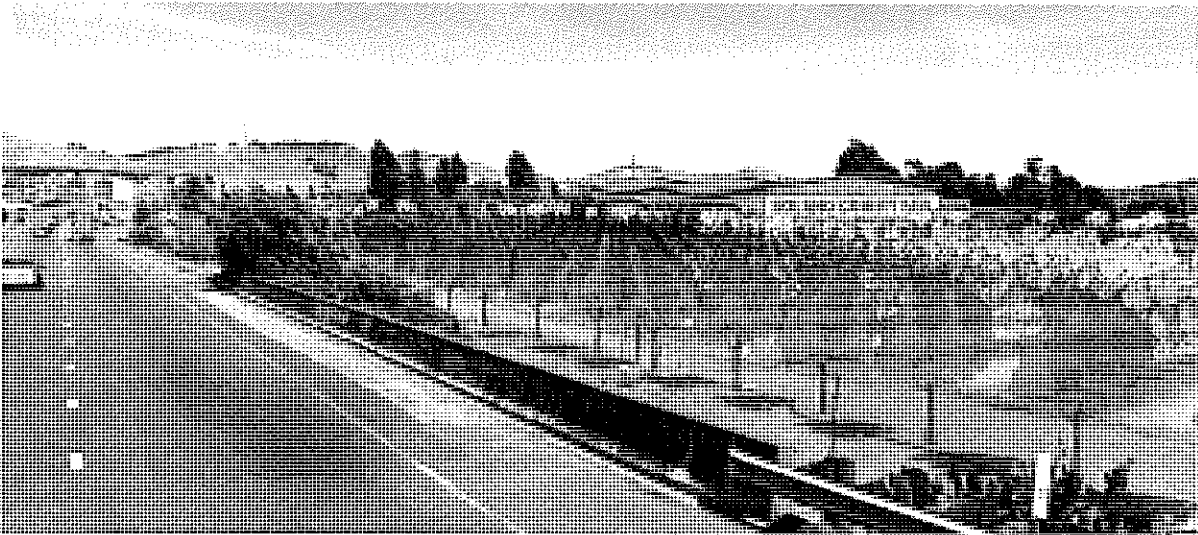


Figure 4.5: Site Relationships
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View from I-80

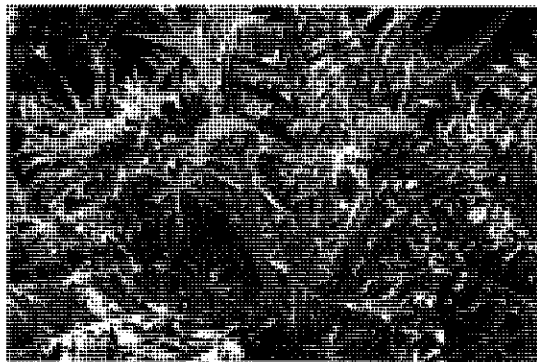
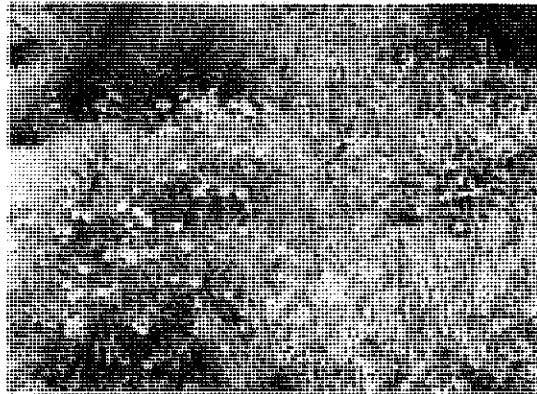


View from S.R. 37

Figure 4.6: Perspective Views



- Emphasize the planting of drought-tolerant, long-lived plant species that are native and/or well adapted to the climatic and soils conditions of the Plan Area and require minimal maintenance.
- Avoid planting tree species with invasive root systems near utility lines, concrete and other paving. Such species may be utilized in setback areas adjacent to roadways or in transition areas, provided there is adequate clearance.
- Avoid the use of non-native, invasive species that may spread into areas of permanent, undeveloped open space.
- Landscaping is required where development is visible from major public roadways and public facilities including trails. Tree planting should consider the need to preserve solar access and views and maintain fire safety requirements.
- All plants should be carefully selected to avoid toxic species that could be harmful to children or cause allergic reactions.
- Planting design should consider year-round interest and seasonal character through the careful use of flower and leaf color.
- Landscape design should provide effective screening of parking areas, retaining walls, utility enclosures, utility cabinets, service areas, or service corridors to reduce negative visual impacts. Screen landscaping should incorporate evergreen plant species in order to maintain year-round leaf cover.
- Plant materials along water edges at the water feature and in the foregrounds channel should be native vegetation capable of filtering water, preventing erosion, and providing habitat and food to native species.
- Landscaping within the Plan Area will be subject to any special requirements identified by future soils or drainage investigations.
- Landscape plans should be prepared by a landscape architect registered to practice in the State of California.



Irrigation and Maintenance

- The use of potable water for landscape should be minimized. It is anticipated that non-potable water from the onsite water feature will serve as the irrigation source



(refer to Chapter Six for additional details). If reclaimed water becomes available, it may be utilized as well. Any water-intensive planting should be concentrated in shaded areas, where natural runoff occurs, or at highly visible locations, such as within the Public Entertainment Core and at the Arrival Plaza.

- Groundcovers, grasses, or drought-tolerant turf should be used in place of standard lawn where possible.
- Existing vegetation is limited within the Plan Area; however, healthy existing vegetation along drainage ways or other areas should be retained to the extent feasible, with replacement provided where removal is unavoidable. In Phase 1, existing (and healthy) parking lot trees should be retained within parking areas if such trees do not interfere with site development.
- All public areas, rights-of-way and commercial project landscaping should have high efficiency, automatic irrigation systems. Low volume spray heads and drip irrigation systems should be utilized. Landscape improvements should be installed and maintained with a sustainable landscape maintenance plan that uses toxin-free organic or biological fertilizers and weed/pest control products.
- Landscape plans should be submitted to the City to insure water-efficient irrigation systems according to City requirements.

Transition Areas and Buffers

Grade transition areas between development and site edges are subject to the following:

- Transition areas should be landscaped to create a visually pleasing transition between development and common areas, and provide filtered views both from and toward the Plan Area. Landscaping of transition areas is required where development is visible from major public freeways or roadways and from public facilities.
- Landscaping of transition areas should emphasize trees and shrub planting and grasses. Irrigation should be provided for plant establishment.
- Site Drainage
- All site stormwater runoff must be treated consistent with the San Francisco Bay Region Municipal Regional Stormwater NPDES Permit (MRP) prior to discharging into an offsite drainage system. Treatment should utilize Best Management Practices (BMPs) and Low Impact Development (LID) principles as specified in MRP Provision C.3.
- Acceptable treatment measures within the Plan Area may include:
 - Infiltration
 - Evapotranspiration
 - Biotreatment (e.g., rain gardens, bioswales, biotreatment units, planter/tree boxes)
 - Minimizing impervious areas
 - Constructed riparian channel (see Section 4.4.3: Fairgrounds Channel)
- BMP's should be incorporated into parking lots, medians, and street/parcel edges.
- Sub-drains should be provided unless a percolation test shows such drains are unnecessary.

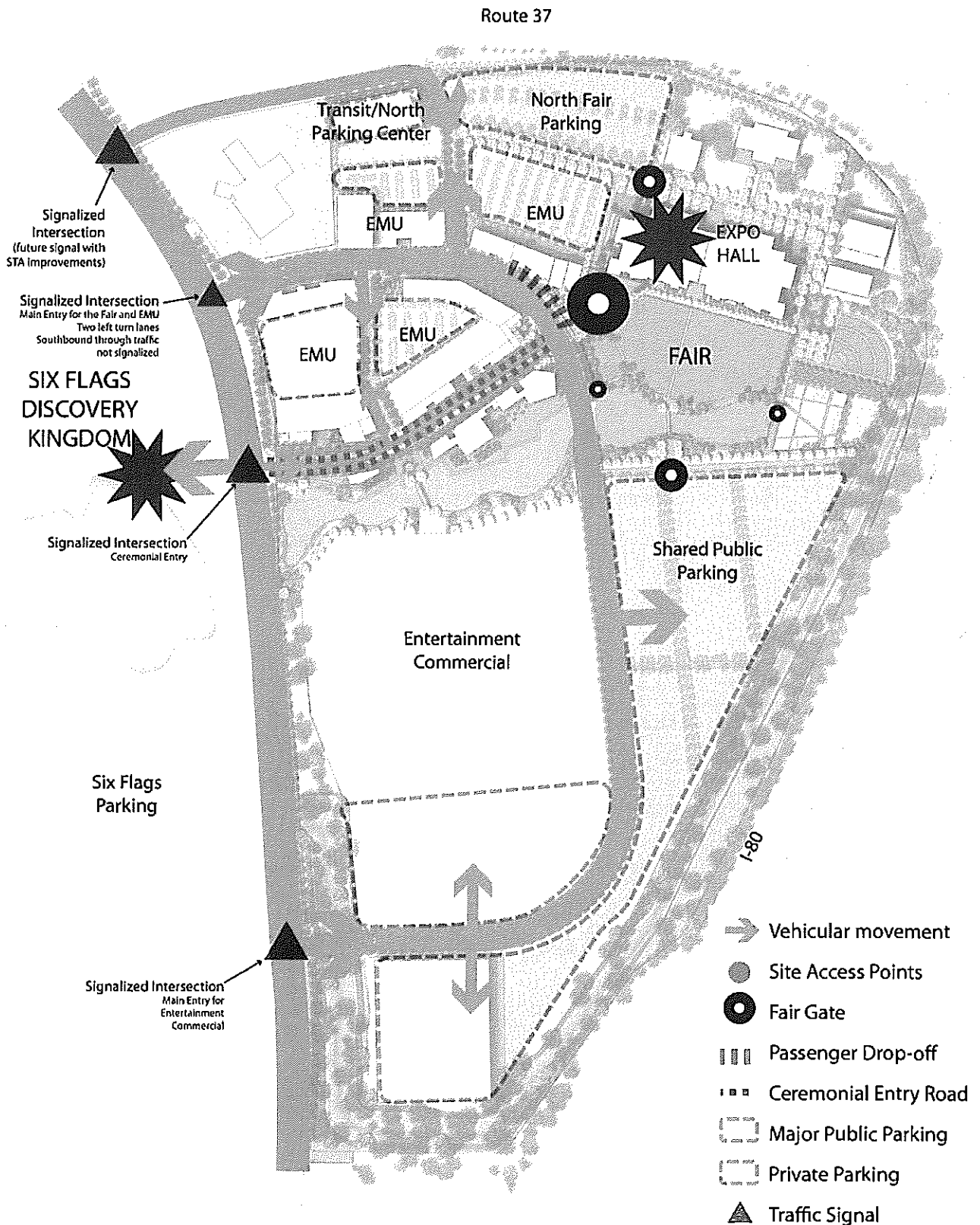


Figure 4.7: Site Access & Parking
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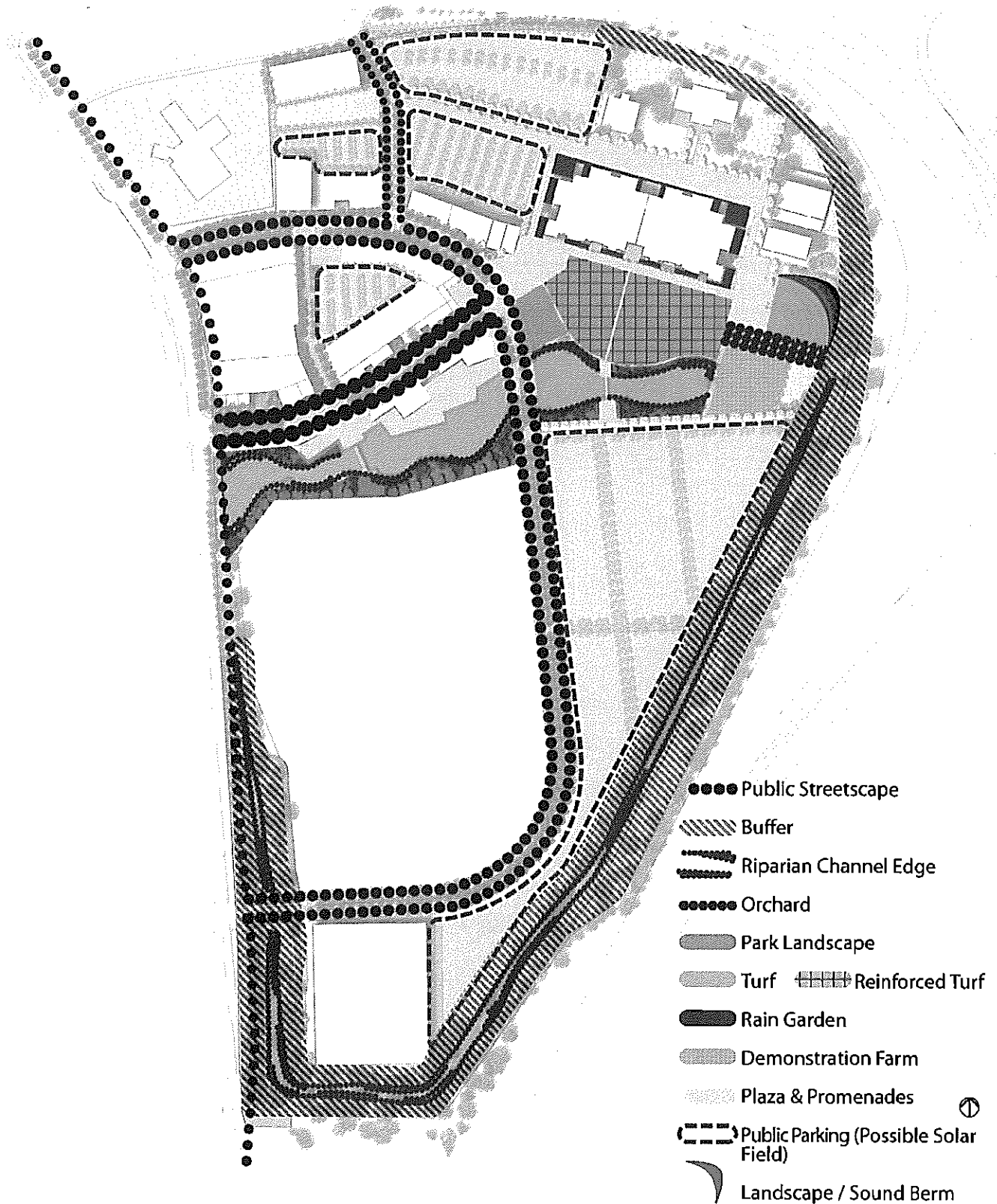


Figure 4.8: Landscape Character
Building areas depicted here are conceptual only.



Erosion and Sedimentation

- Grading operations should be planned and implemented to efficiently control erosion and sedimentation.

Berms, Channels and Swales

Berms, channels, and swales should:

- Be shaped to appear as an integral part of the graded or paved surface.
- Have smooth transitions between changes in slopes.
- Be designed so as to appear a natural part of the site topography.

Slopes and Retaining Walls

- Landscapes should incorporate smooth transitions between changes in slope.
- The maximum slope for a landscaped area should be 2:1 if the area is planted with a ground cover and 3:1 if planted with lawn.
- Where space constraints exist, terracing with retaining walls will be allowed.
- Retaining walls should not exceed three feet in height. For grade changes that exceed three feet, walls should be stepped in equal increments with three foot-wide planted terraces between.
- Retaining walls should be constructed of a low-maintenance, durable material compatible with nearby architecture.

4.2.4 Parking Areas

This section addresses design of parking facilities, located per Figure 5:14: Land Use and Parking. Chapter Five provides additional information on phasing of parking facilities.

Overall Guidelines

- In general, parking should be located and designed to allow buildings to be located directly along street frontages, with parking areas to the rear, while providing adequate parking facilities to serve commercial and public uses.
- During peak use periods, such as Saturdays and Sundays during Fair Week, parking may be augmented by shuttles to offsite locations.
- Parking facilities (including surface lots and structured parking) with pedestrian or vehicle access from Entry Road should be screened at the street level by buildings or significant amenity features to maintain an active street character and well-defined street edge.
- Signs indicating routes to parking should be displayed clearly along the Entry Road, Loop Road and Sage-Loop Connector Road in order to guide visitors.
- Shared parking between the Fairgrounds, nearby major entertainment uses, private development, and other parking users should be maximized and will be defined by a Parking Operations Management Plan to be prepared by the County and by parking agreements between the County and Six Flags Discovery Kingdom.
- Parking should not be located adjacent to the Creek Park or water feature in order to maintain the open space character of those areas (see Section 3.6.1).



Surface Lot Design and Landscaping

As described in Chapter Six, a majority of the Plan Area, including parking lots, will be designed to drain to the Creek Park water feature. The water feature will provide water quality treatment, but it is likely that bio-treatment will need to be integrated into the parking lot design as well.

- Surface parking lots should be planted with trees to minimize their visual impact, reduce heat gain, and create a more comfortable pedestrian setting.
- For private areas (EMU and EC development), trees should be planted at a rate of one tree per six parking stalls.
- Larger scale parking areas, such as Shared Public Parking, require more flexible landscape guidelines in order to serve multiple purposes such as temporary fairs and festivals; therefore, tree planting may be concentrated along perimeters, entries, and key pedestrian corridors.
- Parking lots may be developed with photovoltaic arrays (in place of trees) as described in Section 4.6.2 Next Step Sustainability Measures.
- Ample, well-lit and shaded (either by trees or solar collectors) pedestrian routes should be provided from parking areas to main destinations and building entries. Where possible, pedestrian circulation should be separated from vehicular areas.
- For interior parking lots, smaller trees should be selected to allow adequate visibility beneath mature tree canopies to building entries and storefronts.
- All surface lots should have landscape buffers at street or other public area edges. Landscape buffers should consist of trees and low plantings (to provide views into lot interiors) interrupted with regular pavers or other walkways for ease of pedestrian access.
- All major surface lots should incorporate bicycle parking facilities.
- Passenger loading areas for ridesharing vehicles and preferred parking for carpools and/or certified pure zero emission vehicles (100% battery electric and hydrogen fuel cell) and compressed natural gas (CNG) vehicles should be located near main building entrances.
- Two way parking lot drive aisles should be a minimum 24 feet wide.
- Parking lot landscape islands should be a minimum of eight feet wide at the aisle ends and a minimum of six feet wide elsewhere.
- Tree wells and planting strips should be a minimum of six feet diameter/ width and should be located between all doubled-loaded parking rows.
- Parking lots should incorporate handicapped spaces per ADA guidelines; such spaces should be located near entry points.

Design of Parking Structures

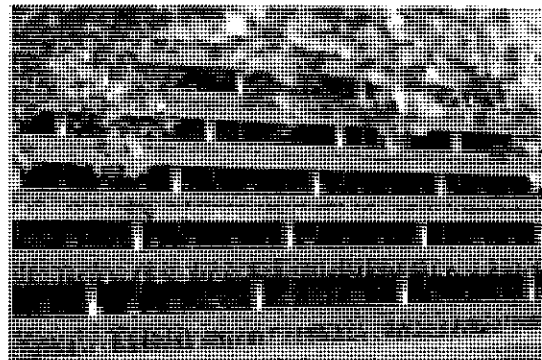
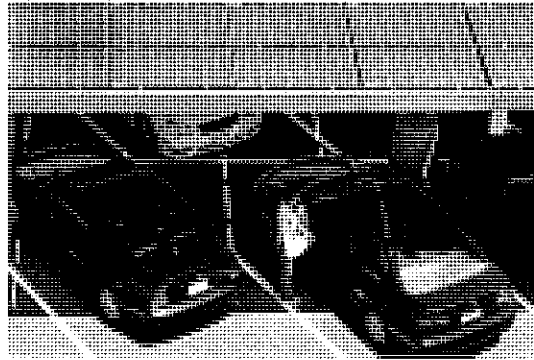
As parcels develop and land use intensifies, structured parking may replace surface lots in the southern end of the Plan Area (South Parking Structure), within the Transit/North Parking Center, and within the Entertainment Mixed Use area. These structures will support anticipated Phase 3 development including expansion of the Exposition Hall and expansion of the Entertainment Mixed Use and Entertainment Commercial development.

- Parking structures should be screened with planting of suitable scale and species.
- Parking structures located in the EMU area should be wrapped by ground floor retail



or entertainment uses along the North Loop Road or other public roads, and retail/commercial uses are encouraged for the ground floor of parking structures to activate streets and pedestrian corridors.

- The upper floors of parking structures should utilize planters, trellises, vegetated walls or other decorative screens along vertical walls at street frontages or other public area and open space frontages.
- Parking structures should be designed to complement nearby architecture in terms of style, massing, color and detailing, and should be located to prevent shadowy, windy canyons.



4.2.5 Signage and Lighting Guidelines

See Section 4.3.6 for Fair of the Future signage, lighting and site furnishing guideline; see Section 4.4.6 for guidelines addressing electronic reader board signage on the Fairgrounds adjacent to I-80 and SR-37.

Figures 4.22 and 4.23 provide examples of site furnishings and lighting.

Signage

Signs will aid in establishing the sense of quality and character for the Plan Area, in addition to conveying critical wayfinding information for visitors.

- Comprehensive signage programs should be developed for both the Private and Public Purpose Areas. These programs should be prepared together or, if prepared separately, should be coordinated to convey a unified identity for Solano360 including the Fair of the Future, Creek Park, and the entertainment and retail development.
- Permanent signs prepared as part of comprehensive signage programs should include entry signs, area signs, directional signs for vehicles, bicyclists/pedestrians, street signs, interpretive and educational signage within the Creek Park and Fair, and signs identifying businesses in the EMU and EC areas.
- Temporary signs may include special event signs, temporary signage during construction or at the opening of a new venue or business, real estate information signs, and parking controls for major events.
- In general, signs should be utilized only where necessary, emphasizing an image of permanence and quality; however, signs should offer adequate visibility and reflectivity, where appropriate, to provide for safety and orientation at night. The purpose of permanent signage is to convey information, to aid in identifying visitor destinations and to add an element of consistency.
- Entry signs may be integrated into entry pylons, arches, or other features.
- All permanent signs and monuments should be constructed of durable, high quality



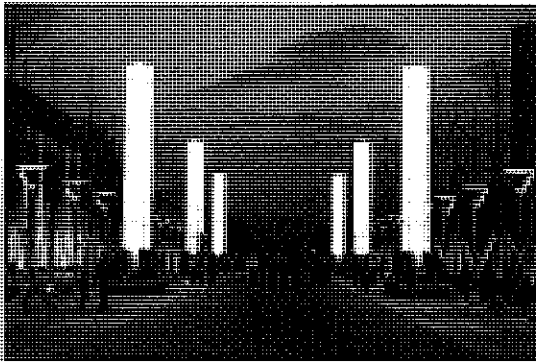
materials.

- Freestanding signs should be limited to directory-type signs with information limited to the name of the project for multi-parcel developments and building or address numbers.
- Access to parking should be adequately signed to guide visitors to parking facilities.
- All free-standing parcel or project signs along streets and common access drives should be designed as a 'family' of signs, consistent with the architectural style of related buildings.
- Small, free standing signs for individual buildings may be allowed near building entries; such signs should be consistent with the architectural style of the building. Other signs for individual buildings or tenants should be located on the building in a manner consistent with the architectural style.
- A digital kiosk or marquis sign at the Entry Road entry or other appropriate location may be allowed for use by the Fair Association for Fair and other Solano360 events.
- With the exceptions noted above, all signs within Private Purpose Areas should conform to the City Zoning Ordinance Chapter 16.64.

Lighting

Street-level and pedestrian lighting are important for safety and will also contribute to site identity and character within the Plan Area. Lighting elements should adhere to the following.

- Lighting should be designed to differentiate use areas, emphasize amenities and landscape features, provide continuity along street corridors and promote safety.
- Lighting may be combined with banners or incorporated into other pageantry and wayfinding features to create a festive setting.
- In general, lighting should provide sufficient levels of ambient light to create a safe and pleasant environment without causing light pollution or glare into adjacent properties.
- Low-level, cut-off, pedestrian-scale fixtures should be utilized to the degree possible.
- Street lighting should be directionally shaded to reduce off-site fugitive light and glare.
- Exterior building lighting should be shielded to minimize direct glare and reflections.
- Lighting should utilize LED or other energy-efficient fixtures with pleasing light color.



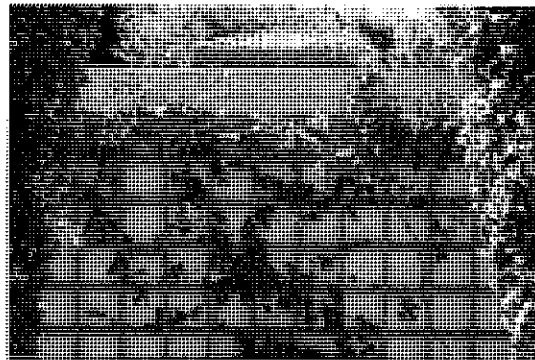


- Materials for lighting fixtures should be durable and low maintenance. Natural finishes like bronze, and nickel steel are recommended.
- Spacing and illumination levels should be calibrated to achieve IESNA standards (e.g., a 0.5 foot candle level for sidewalks in medium pedestrian activity areas), and local requirements, based on photometric studies prepared as part of design submittals for each street.
- Intersection lights should be on 22-foot tall poles.
- Pedestrian lighting along sidewalks should not exceed 15 ft in height.
- Parking lot lights should be no higher than necessary to provide efficient lighting of the area, but should not exceed 28 feet, including the base.

4.2.6 Walls and Fences

Walls and fences may be used to define public and private boundaries and spaces, as described below. See additional guidelines for Fairgrounds fencing and entries in Section 4.3.

- Where used, walls and fences should be open and/or low to maintain an inviting, attractive appearance and provide adequate sight distance for entries. Materials should be compatible with and complementary to principal buildings. Fence and wall panels may be divided into regular modules that reflect the module of the principal building.
- Thick and thin elements should be used, with thicker pieces for supports and panel divisions. Fence posts and support columns should be emphasized and/or built-up.
- Screen walls are intended to screen uses such as loading, service areas, and utilities, while maintaining a common architectural language with the buildings surrounding them. All screen walls connected to buildings should match the building style. Maximum height of a screen wall should be six inches higher than the object being screened.
- Masonry walls should have a base and coping.
- Fences visible from public areas should be wrought iron, cast iron, and welded steel ornamental fences or wood. Metal fences may be mounted on a low masonry wall, and/or spanning masonry piers. Wooden fences should be painted, preferably a light color.
- Security fences should not be visually prominent. Black, vinyl-clad chain link fencing (with matching posts) may be used for security fencing with a maximum height of seven feet; taller fences may be allowed along freeway edges. Evergreen hedges, flowering vines and/or trees should be planted along the base of all security fences.





- Black, vinyl-clad chain link fencing (with matching posts) may be utilized for storage or service areas that are not visible from public areas, including public roads.
- Plywood, un-clad chain link, barbed wire or razor wire fence are prohibited.

4.2.7 Loading and Service Areas

- Loading areas should be sited to the rear building or sides of buildings not visible from public areas, including streets.
- All service, loading, trash, storage areas, and utility equipment should be screened from public view utilizing a combination of planting and architectural elements that are compatible with the building architecture.
- Loading/garage doors are prohibited on building facades facing a public street.
- Service loading from public streets is prohibited except for parcels where other configurations are not feasible, such as adjacent to the Creek Park.
- No refuse or storage areas may be located between the front of a building and a primary road right-of-way except for parcels where other configurations are not feasible, such as adjacent to the Creek Park.
- Refuse collection and storage should be located to the rear and sides of buildings, covered with a roof, and sized to contain all refuse generated on site between collections.
- Common recycling bins should be provided for all commercial uses and must be readily accessible to all tenants/employees, and be screened in the same manner as refuse collection areas.
- Transformers and other utility equipment should not be placed in the public street setback area.
- All rooftop equipment should be fully screened with the same or similar materials of which the building is constructed.

4.3 FAIR OF THE FUTURE

4.3.1 Fairgrounds Programming

Throughout the planning process, Solano County Fair Association representatives provided input regarding near-term and mid-term plans to establish a new Fair of the Future that could offer a broad array of year-round activities while maintaining the traditions and community connections of the existing Fair.

Outdoor spaces, including lawn and hardscape plazas, are of critical importance to the Fair.

Following are the identified program uses for the Fair of the Future:

- Establishment of a new, flexible event hall of approximately 50,000 net square feet of exposition/event space, with potential for expansion to 100,000 net square feet in the future when demand warrants such an expansion.
- Ability to provide an array of event and entertainment venues to respond to market opportunities and region serving demand.
- Selective update, expansion and/or replacement of existing Fair facilities.
- Desire to have complementary program to Six Flags Discovery Kingdom and adjacent



mixed-use development.

- Convenient and proximate transitions from indoor to outdoor venues.
- Branding and image to focus on local culture and heritage of the Fair, with consideration of the County Fair roots/heritage: Livestock, Agriculture, Food and Community.
- Reinforcement of important County Fair themes including (1) heritage of Solano County Fair; (2) sustainability; (3) agricultural demonstration.
- Expression of the diverse character of Solano County, (urban / rural, ethnic/cultural diversity, lifestyle diversity) and effective use of the site's key location at the crossroads of major roads.



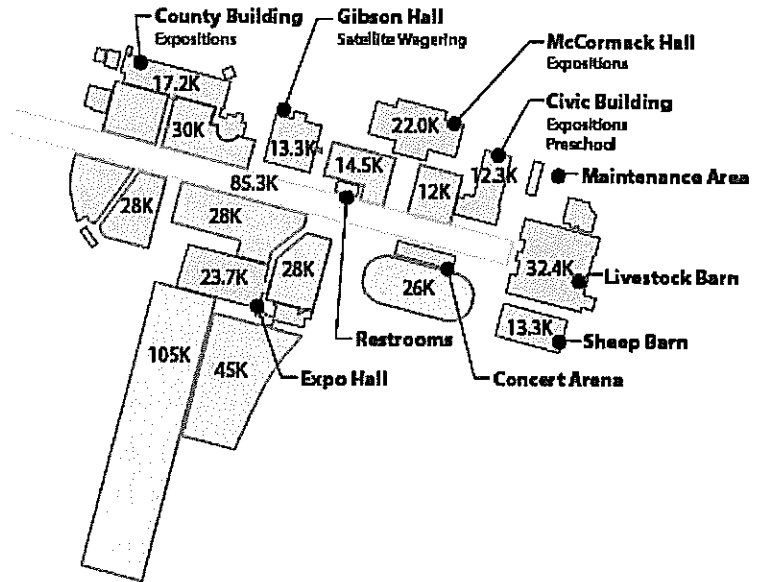
In addition to current events and activities at the Fair, specific new attractions and programming could include:

- A Ferris wheel or similar feature visible from I-80.
- "Mini-midway", or small amusement park, with year-around operation.
- "Festival-on-the-green" program of activities within a new event open space; consideration of an outdoor inflatable movie screen.
- Demonstration Farm that could attract school groups and take advantage of interests in micro-sustainability and urban farming.
- Wedding events with location for wedding 'photo op.'
- Tractor pulls, livestock shows and similar agriculture-related events and activities.
- Running or walking races.
- Flea markets and farmer's markets.
- Complementary operational relationships with Six Flags Discovery Kingdom, local hotels, and other businesses, such as providing exhibit or meeting space to help hotels attract larger scale meetings or convention business.

4.3.2 Fairgrounds Design Objectives

Figures 4.11 and 4.12 illustrate the conceptual plans for the Fair's outdoor and building venues for Phases 1 and 3. As envisioned, the Fair of the Future plan upgrades the Fairgrounds in its current location, with long-term flexibility to expand southward into parking areas as additional space for event venues is required beyond the scope of this Plan.

The overall objectives of this conceptual-level design are as follows:



EXISTING FACILITIES

	BUILDING AREA.....	134,200
	SHADED PLAZA AREA.....	28,000
	PAVED VENUE AREA.....	45,000
	LAWN VENUE AREA.....	110,300
	OTHER OPEN SPACE (ARENA)...	26,000
	CARNIVAL / MIDWAY AREA.....	105,000
	CONCOURSE.....	83,300

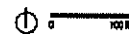
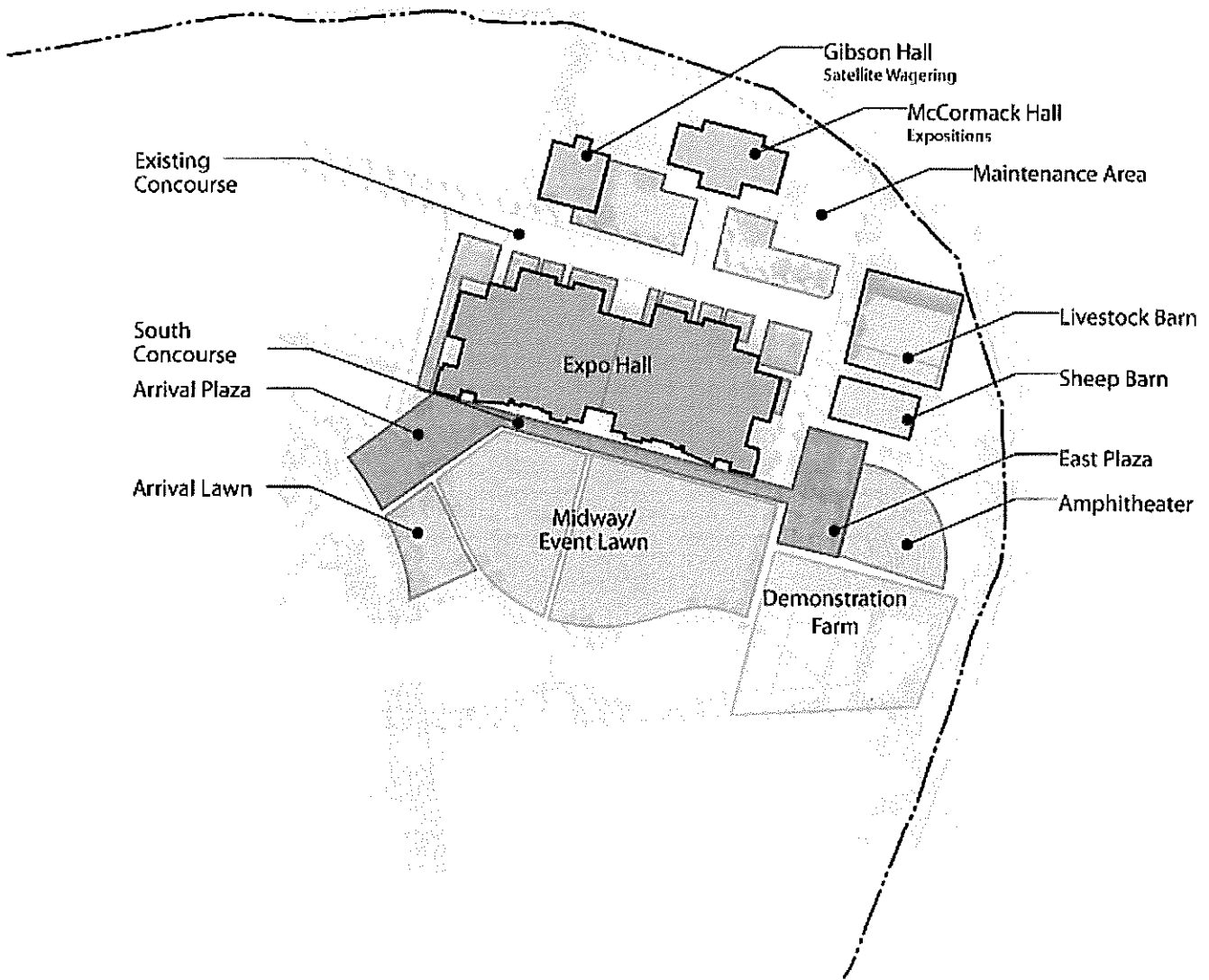
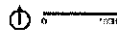


Figure 4.9: Existing Fairgrounds Facilities



**PROPOSED FACILITIES (Sq.Ft.)
At Buildout/Phase 3**



Building Area	233,060	
New Hardscape Venue Area	59,102	
Lawn Venue Area	60,000	
Concert Amphitheater	60,700	
Midway/Multi-Purpose	164,621	
Concourse (Existing)	55,000	
Demonstration Farm	90,770	
Gardens/Courtyards	48,768	

Figure 4.10: Proposed Fairgrounds Facilities



- Provide new, multi-functional event facilities that expand the Fair’s abilities to market to a wide variety of entertainment, educational, commercial, and civic programs on a year-round basis.
- Create new outdoor venues adjacent to and in association with the new Exposition Hall to support the Fair’s program of outdoor events and create appealing and durable outdoor public spaces. For maximum usability, these venues should include both turf and paved spaces and should be designed as “outdoor rooms” with simple, outdoor areas framed by trees and/or buildings.
- Distribute parking areas and entry gates, with clear wayfinding signage to enable flexible event programming and allow the Fair facilities to serve multiple, concurrent events.
- Develop options for year-round uses and products at the Fair; require that events and attractions stay relevant and relate to contemporary preferences for food, entertainment and education.
- Consider the selective update, expansion, and/or replacement of existing Fair facilities in a phased program that allows each incremental stage to function effectively.
- For intermediate/interim enhancements to Fair facilities, consider “facelifts” to key buildings and enhancements to the grounds.

4.3.3 Fairgrounds Phasing

Flexibility is a critical objective for the Fair of the Future. The phased upgrade of structures and open spaces is intended to allow multiple and shared uses, allowing the Fair to operate and generate revenue throughout the year and providing for maximum synergy with non-public and public uses on the overall site.



• **Phase 1** (Phases 1a and 1b) includes the demolition of the existing Expo Hall and construction of the new Exposition Hall providing approximately 50,000 net square feet (approximately 72,000 to 77,000 gross square feet, depending on whether Administrative and Security Offices are included). Associated outdoor venues, including Arrival Plaza and Midway/Event Lawn and Creek Park with water feature, are scheduled for Phase 1. If funds are available, Phase 1 could include relocation of the existing Administrative and Security Offices into the building; alternatively, this may occur in Phase 3.

• In Phase 2, in order to provide for North Fair Parking expansion, the existing County Building will be demolished. The Fair’s Administrative and Security Offices will also be demolished and housed in portable buildings, if not already located within the Exposition Hall in Phase 1.



- In Phase 3, or if sufficient demand arises in Phase 2 and if supported by onsite and offsite infrastructure and mitigations, the Exposition Hall will be expanded to approximately double the Phase 1 footprint and program. The Phase 3 expansion will require demolition of the existing concert arena and construction of a new amphitheater for concerts and theater events as shown in Figure 4.12. If Administrative and Security Offices are still housed in portables, they would be relocated into permanent space within the expanded Exposition Hall.

Together with the existing facilities that will continue to function (including Gibson, McCormack, the livestock and sheep buildings), this phased approach provides essential facilities that will allow for the efficient operation and financial sustainability of the Fair of the Future.

Table 4.1: Fair Building Program & Phasing

EXISTING BUILDINGS AT CONCOURSE (Note: does not include facilities for horse racing or golf course)		EXISTING QUANTITY (sq. ft.)	PHASE 1 (sq. ft.)	PHASE 2 (sq. ft.) ¹	PHASE 3 (sq. ft.) ¹
Admin/Directors Trailer/Security Office		5,110			
County Bldg		17,170	17,170		
Gibson Hall		13,325	13,325	13,325	13,325
Concourse Restroom		1,650			
McCormack Hall		22,000	22,000	22,000	22,000
Civic Bldg		12,325	12,325	12,325	
Trash Shed		2,000	2,000	2,000	2,000
Maintenance Shed		4,550	4,550	4,550	4,550
Livestock Bldg		32,400	32,400	32,400	32,400
Sheep Barn		13,285	13,285	13,285	13,285
Concert Arena/Grandstand Cover		5,200	5,200	5,200	
Twilight Patio Office/Concessions/Storage		1,800			
Existing Exposition Hall		23,730			
Guard Shack (adjacent to director's trailer)		1			
TOTAL Existing		154,545	122,255	105,085	87,560
NEW BUILDINGS (based on project description)			PHASE 1	PHASE 2 ¹	PHASE 3 ¹
New Exposition Hall ²			72,000	72,000	144,000
Temporary Administrative Offices (Phase 2)				5,000	
New Concert Arena/Grandstand Cover					5,500
TOTAL New			72,000	77,000	149,500
TOTAL Existing and New		154,545	194,255	182,085	237,060

Notes

1. Totals are cumulative and include prior phases

2. The Exposition Hall replaces existing Expo Hall and concourse restrooms; also adds lobby, circulation, kitchen, and meeting rooms. In Phase 2, existing Admin offices would be demolished to provide North Fair parking; if not provided in Phase 1 Expo Hall, Admin office would be housed in portables until Expo Hall expansion in Phase 3 provides permanent admin space.

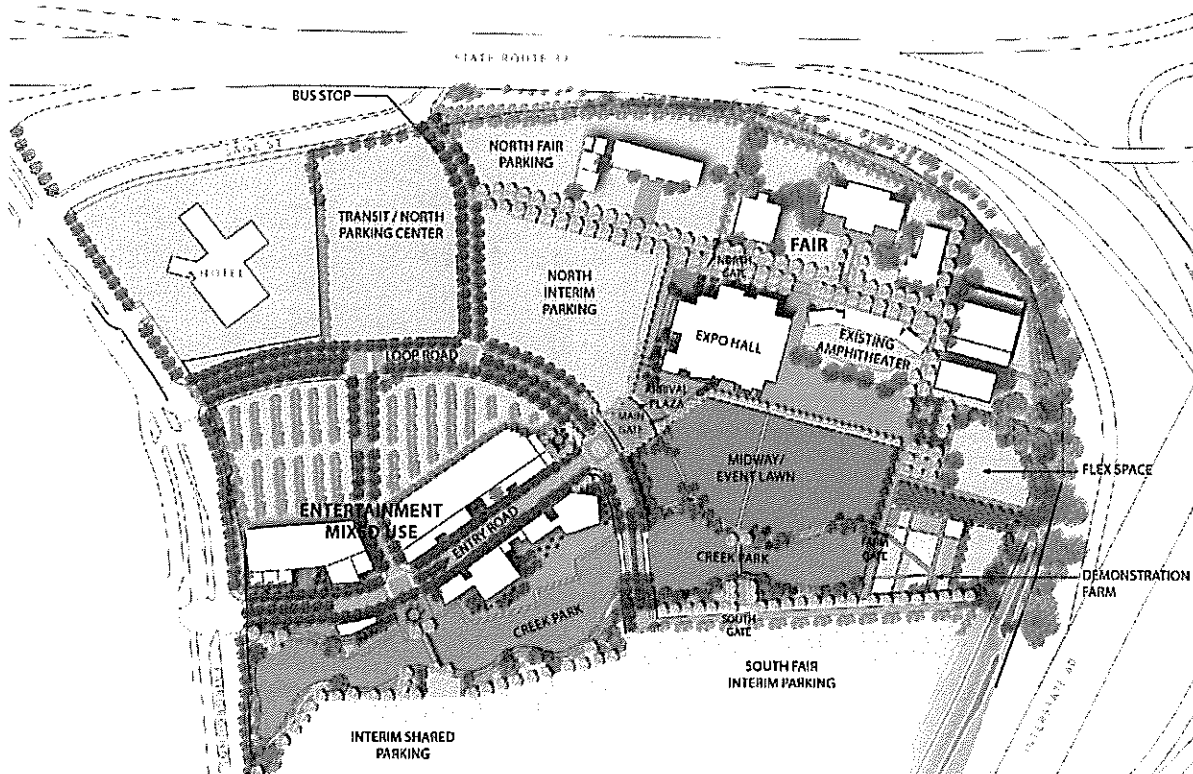


Figure 4.11: Fair Illustrative Plan - Phase 1
Building areas depicted here are conceptual only.

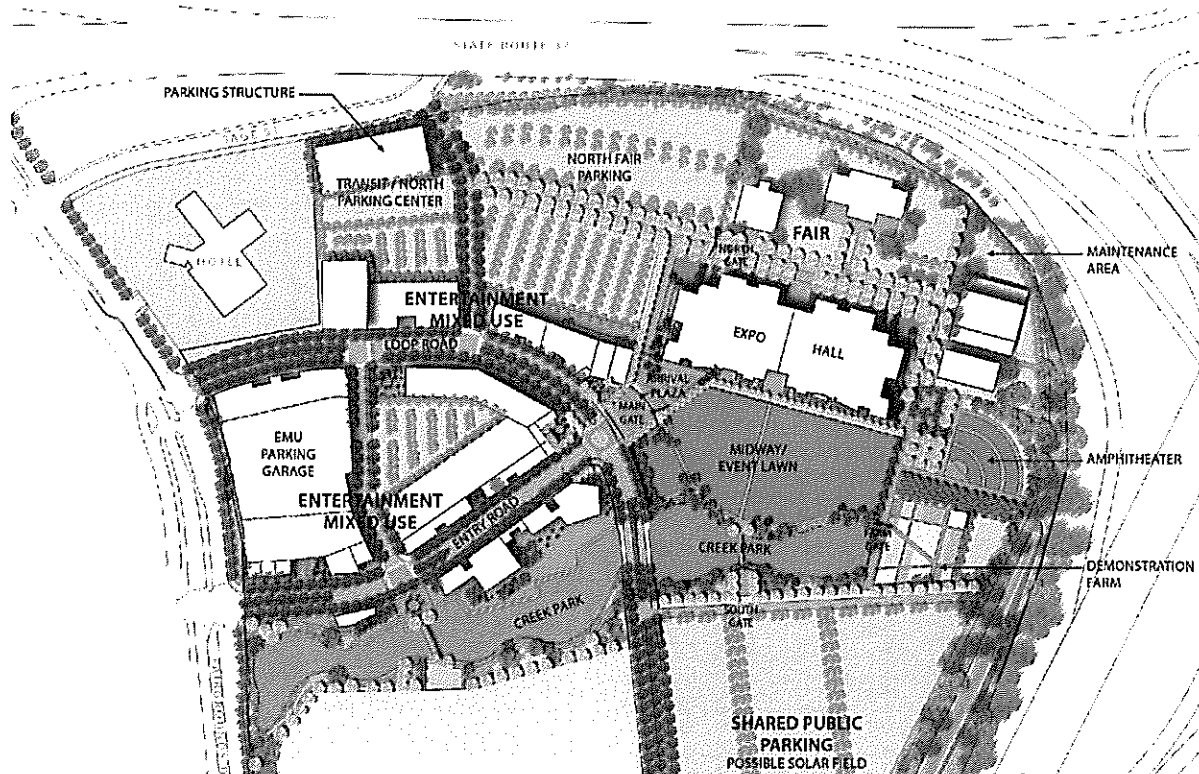


Figure 4.12: Fair Illustrative Plan - Phase 3
Building areas depicted here are conceptual only.



Figure 4.13: Aerial View - Phase 1
Building areas depicted here are conceptual only.



Figure 4.14: Aerial View - Phase 3
Building areas depicted here are conceptual only.

4.3.4 Exposition Hall

As part of Phase 1a, the Plan proposes to replace the existing Expo Hall with a new Exposition Hall that offers 48,600 net square feet of exhibition space in a flexible, highly marketable venue integrated with the existing fair concourse and other facilities. This flexible space can be subdivided in logical increments, as described below, in order to accommodate a wide range of events including conventions, consumer shows, festivals, large parties, and other special events.

In addition to exhibition space, the Exposition Hall provides support space for lobbies, circulation, meeting rooms, kitchen, storage of movable wall panels, and restrooms for a total of 72,000 square feet.

Figure 4.15 to 4.19 illustrate the layout and architectural concepts for this important event building, which is envisioned as follows.

The following descriptions refer to the initial building proposed for construction in Phase 1a and anticipated to serve the Fair through Phase 2. Possible expansion in Phase 3 will approximately double this space and also provide for office space for Fair Administration and Security services.

Building Concept

Conceptual design for the Exposition Hall represents a functional, economical and flexible building design that also provides an architecturally distinct and compelling landmark facility for the Plan Area. In addition to its style and massing, a range of contemporary building materials were selected to reflect a forward-looking vision for the "Fair of the Future". The conceptual design for the Exposition Hall includes the following key elements:

- In addition to serving as interior circulation and gathering spaces, the entry lobby and lounge areas (located on the south side of the building) have been organized to open directly onto a covered exterior terrace and multi-purpose lawn/event space, with views and direct access to the water feature beyond.
- The simple, yet geometrically expressive roof shape of the main Exposition Hall provides an iconic and easily identified building element within the overall site. With its inclined roof surfaces—reminiscent of the hillsides that surround the site—and exposed wall surfaces at both the east and west ends, the building's height and orientation provide a highly visible signage/graphic opportunity when viewed from both SR-37 and I-80.
- The conceptual design embodies a commitment to environmental responsibility, and sustainable goals and practices through proposals for a variety of material selections, features, and elements (see below).

Central Exposition Space

- Nominally, a 270' long by 180' wide (48,600 net square feet), column-free exposition space for each phase, with 30 feet clear to the underside of the structural grid above.
- The space will likely be constructed as a system of steel columns and roof trusses at 15 feet on center, which will clear span the entire (180 feet) width of the hall.
- The interior layout for each phase accommodates the following program functionalities.
 - Up to 235 vendor booths, (at 10' x 10' each)
 - Approximately 1,823 people for banquet-type events, (assuming 20 s.f./person)
 - Approximately 3,645 people for live concerts and shows, (assuming 10 s.f./person)
- Movable, full-height wall panels allow the main space to be subdivided into multiple



- configurations and a broad range of sizes, including: 48,600; 32,400; 16,200; 10,800; 8,100; and 5,400 square foot options.
- Windows provide natural daylight at upper levels of exterior walls, and along east elevation of building, which can be fully blacked out (with movable drapes).
 - The floor finish will be natural concrete, with painted interior gypsum board walls, with painted roof trusses and metal deck ceiling/roof.
 - Electrical power will be provided at: the perimeter of the main space; the upper level grid/catwalk; and distributed locations across the floor (via floor boxes).
 - Provisions will be made to accommodate audio/visual presentations in any of the various room configurations. Room lighting controls will be integrated with the A/V presentation systems.
 - A system of catwalks (accessed by an interior caged ladder) will be provided at the bottom chord of roof trusses, to accommodate special event lighting and rigging systems (by others).
 - HVAC and lighting systems will be separately zoned and controlled to accommodate the various room configurations.
 - Event load-in and load-out will be achieved through on-grade access doors (including standard and high-bay doors) distributed around the perimeter of the building.

Entry Lobby/Café/Lobbies

These areas serve as the primary arrival/entrance point to the facility. The Entry Lobby has been positioned to be easily viewed from the main Entry Road and Arrival Plaza, yet can be easily accessed from secondary entry points. Features include:

- Two exterior walls of the Entry Lobby will be fully glazed to bring natural light into the building interior.
- Interior finishes will include either a carpet tile or quarry tile floor; painted gypsum board or wood paneled accent walls; and a decorative wood slat ceiling below acoustically absorptive materials.
- Secondary Lobbies and Corridors will be finished in a similar manner, and will include glass doors and windows, and a system of movable glass walls to open Lobby spaces directly to the exterior.
- A small café has been located along one wall of the Entry Lobby, to provide snacks and beverages to visitors.

Meeting Rooms

Four break-out meeting rooms have been provided with movable wall partition systems, allowing a variety of room sizes and configurations to serve larger and smaller group needs. Features include:

- Each Meeting Room will be provided with separately controlled lighting and audio/visual presentation systems
- Interior finish materials will include: carpet tile floors; painted gypsum board walls; and suspended acoustical tile ceilings (+12' high), which accommodate fluorescent room and display/accent lighting.
- Natural daylight will be provided through a glazed exterior wall system, (including

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- Natural daylight will be provided through a glazed exterior wall system, (including

provisions for drapes to fully black-out the room during presentations), with doors to access a landscaped exterior patio/garden.

Kitchen

The plan provides space for an approximately 1,800 s.f. commercial grade kitchen in the northeast corner of the building, immediately adjacent to the main Exhibition Hall, (and future Phase III expansion). The Kitchen, as currently sized, will be able to prepare and serve sit down meals to approximately 350-500 diners, in one or more of the exhibition halls or meeting rooms.

To serve larger events, the Kitchen will be optimized to also function as a “catering kitchen” (with food preparation/cooking done off-site, and delivery in warming ovens). For such events, plating and set up will likely need to be provided in temporary exterior space, or utilize a portion of one of the sub-divided exhibition halls.

Features include:

- Interior finishes will be commercial grade, durable and washable and able to meet stringent public health codes and sanitation standards.
- All kitchen appliances will be standard commercial grade.

Administrative Offices

In Phase 3 (or in Phase 1 or 2, if funds are available), the Fair’s administrative offices should be located within the Exposition Hall to optimize operational efficiencies and enhance the market appeal of the new facility. Approximately 5,000 square feet will provide for fair management, security, and parking management, with areas for small staff meetings. Larger groups, such as the Fair Association Board, could make use of the Exposition Hall meeting rooms during non-paid events.

- If incorporated into the building in Phase 1, the administrative offices may be situated as second floor uses over the meeting rooms and hallway; this approach may be the most cost effective as it makes use of building elements (walls and roof) already in place and requires only the addition of stairs, a one-story elevator, and flooring.
- If incorporated into the expanded Phase 3 building, the administrative offices would occupy the portion of the building designated as “Meeting Rooms” in the Phase 1 structure.

Restrooms

Restrooms have been provided in strategic locations around the Exposition Hall.

Positioned on the exterior of the building, restroom entrances have been organized to allow direct access from either interior or exterior events, (and administratively controlled). The new restrooms on the north side of the building will replace the existing restrooms currently located along the concourse.

Exterior Elevations, Materials and Features

- Based on a system of pre-manufactured, insulated metal panels, exterior walls will include a variety of additional finish options (alternate colors, textures, or metal finishes; cement plaster; or stone veneer at select locations).
- Similar to the exterior walls, the main Exposition Hall roof structure will be based on a system of pre-manufactured, insulated metal panels, with a pre-finished standing seam metal roof finish.

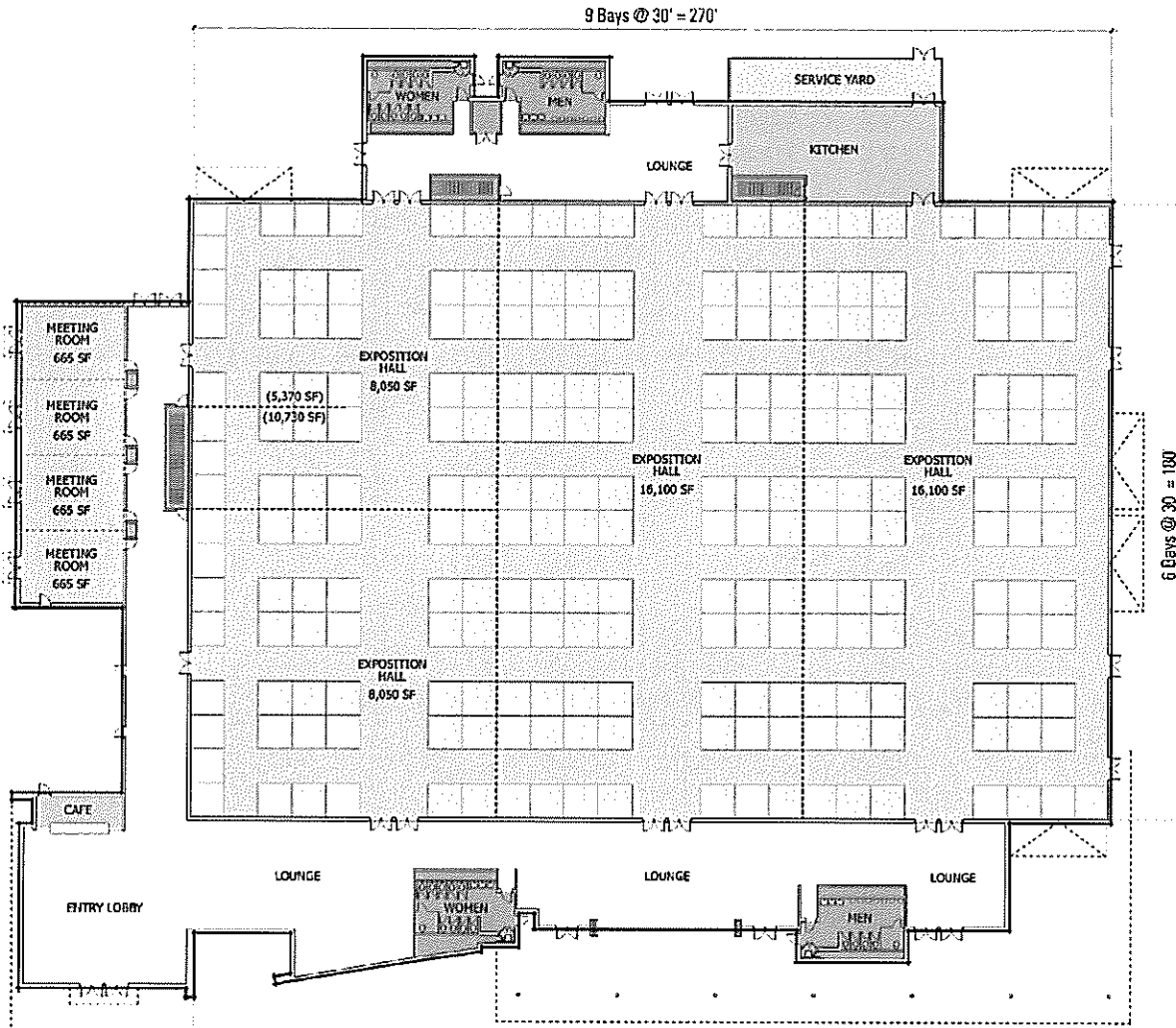
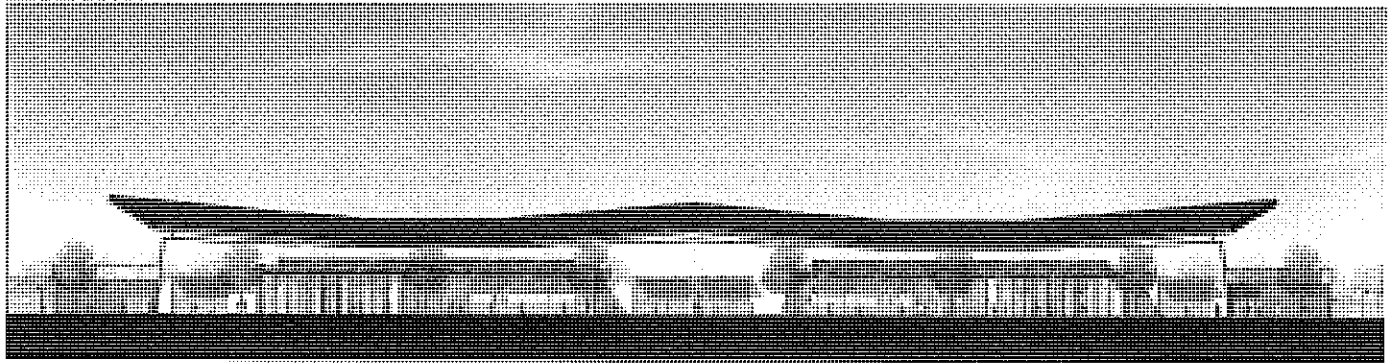
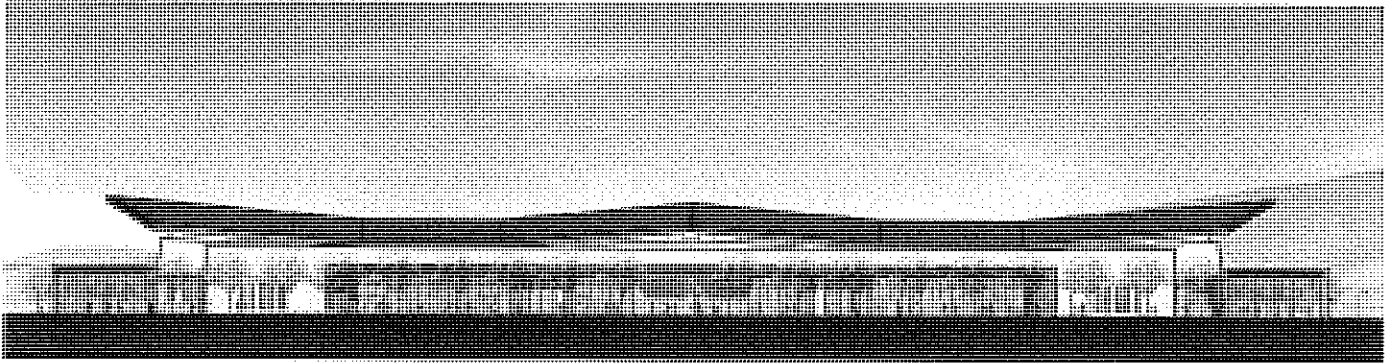


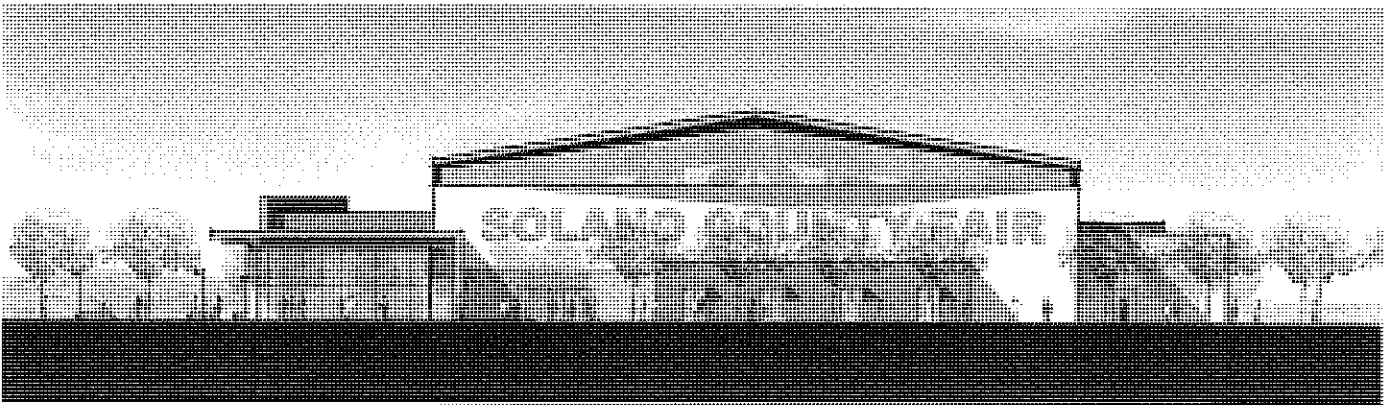
Figure 4.15: Exposition Hall –Schematic Floor Plan



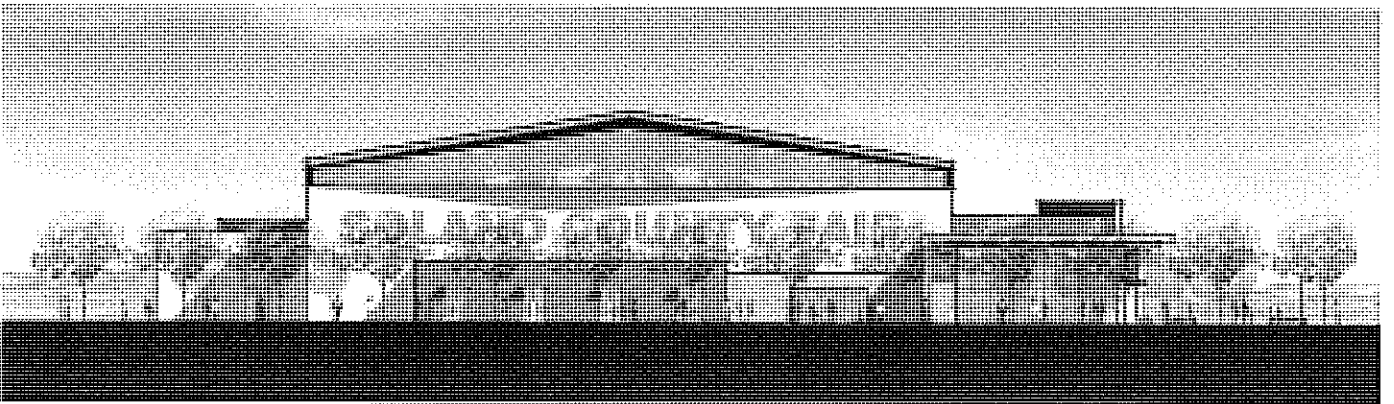
North Elevation



South Elevation



East Elevation



West Elevation

Figure 4.16: Exposition Hall –Elevations



- Lower (single-story) roofs will be designed with open-web roof trusses, metal decking, and a built-up or single-ply roofing system over rigid insulation.
- Glazing at the main and secondary entrance locations will be designed around a pre-finished (either natural or painted), aluminum storefront system. Additionally, large sections of the exterior glazing system will be designed as operable walls, to increase the inter-connection between interior and exterior spaces.
- As conceived, portions of the main Exposition Hall roof will receive photovoltaic and/or solar hot water heating panels.
- Gutters and roof drains will be also be piped to a series of landscaped "rain garden" areas, where rainwater can be collected and filtered before draining to the central water feature.

Sustainable Building Features and Goals

- The south-facing half of the Exposition Hall is proposed for installation of photovoltaic arrays and/or solar water heaters. With a total roof surface of approximately 50,000 square feet, this south-facing portion would provide an area of approximately 25,000 square feet. Additional roof areas over the entry lobby, meeting rooms, and/or south-facing shade canopy could also be utilized, depending on the results of more detailed studies in conjunction with overall energy programs for the Plan Area.
- Pre-manufactured exterior wall and ceiling panels should be selected to provide high insulation values, with metal support framing and finish surface options containing up to 85% recycled material content.
- Concrete slabs and foundations should include reinforcing steel with recycled content (typically ranging between 45% and 70%) and fly-ash, as part of a recycled waste diversion program.
- High efficiency water fixtures should be utilized to conserve water and offset high peak loads within the facility.
- To minimize the use of artificial light, south-facing yet shaded lobby/lounge spaces (as well as small meeting rooms) should have access to natural daylight through operable windows and exterior doors that open directly onto landscape areas. Additionally, skylights or light tubes should be included wherever practical.
- Operable windows should be provided at the upper (clerestory) level of the main Exposition Hall to provide natural daylight, as well as naturally ventilate the space.
- Efficient interior lighting and control systems should be provided, and occupancy sensors utilized wherever practical.

Phase 3 Expansion

Phase 3 assumes a doubling in size of the Exposition Hall from approximately 50,000 net square feet (72,000 gross square feet) to approximately 100,000 net square feet (144,000 gross square feet). If the administrative offices are already accommodated within the Phase 1 building, these uses would be accommodated. At full build out, the Exposition Hall will be a contiguous, column-free space that is sub-dividable into multiple smaller halls, as in Phase 1.

A second Entry Lobby will be "mirrored" at the opposite end of the building, to provide another primary entry point into the expanded facility. Similar in layout to Phase 1, additional lobbies, meeting rooms, restrooms, and an expansion of the Kitchen are also proposed in Phase 3.

4.3.4 Outdoor Venues

Arrival Plaza

- At the eastern terminus of Entry Road, a new Arrival Plaza at the Exposition Hall entry is envisioned for Phase 1a as a location for congregation, ticketing and entry, and a paved outdoor venue for art exhibitions, car shows, or similar events.
- The Arrival Plaza would create a flexible space incorporating movable bollards, planters, or other barriers to accommodate primarily pedestrians, but also occasional vehicles, according to the scheduled event. The width of the plaza should allow for turnaround of passenger vehicles (approximately 80-foot diameter) and drive-through of safety and service vehicles that need to access the west or south sides of the Exposition Hall, with exits to the landscape concourse.
- Portable ticket booths may be integrated into a dramatic entry element. The plaza design and ticket booth location should create spaces for pedestrian gathering and orientation both outside and inside a secured perimeter. Ticket booths may be integrated with signage, banners, and other elements celebrating the Fair of the Future.
- The Arrival Plaza would also be a suitable area for Farmer's Markets or other similar and temporary events.

Exposition Hall Gardens

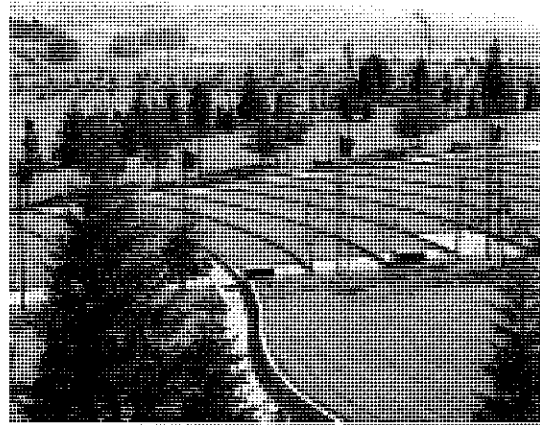
- Rain gardens constructed as part of the Phase 1a and Phase 3 Exposition Hall should surround the building in order to capture, filter, and retain stormwater draining from the large roof surface. The rain gardens should be installed with suitable soil and drainage measures, and planted with species that tolerate rain garden conditions and provide visual appeal.

Midway/Event Lawn and South Concourse

- South of the Exposition Hall, a new Midway/Event Lawn of approximately four acres is proposed for Phase 1a to accommodate the midway during Fair week(s) and other major events throughout the year such as dog shows, festivals, and other activities where a turf surface is desirable. Between events, this area could serve as an extension of the Creek Park, with public access for strolling, picnicking, painting, and other passive recreation.
- The Midway/Event Lawn is intended as a simple grassy area sloping gently toward the water feature, with walks and ramps that provide accessibility. The slope should be approximately two percent in order to provide positive drainage and allow a wide range of activities.
- Mesh-reinforced turf should be used for the Midway in order to accommodate vehicles and temporary structures. A recommended surface material is reinforced turf (such as Grasspave or Advanced Pave Tech Turf) incorporating a root zone mesh or other system that provides a free draining natural grass surface with high load-bearing capability.
- The south-facing edge of the Exposition Hall is intended to include a South Concourse; this pedestrian promenade should be a minimum of 10 feet in width to accommodate service vehicles. The promenade could include terraced steps that lead to the Event Lawn, providing a location of seating and viewing the Midway and water feature.

East Plaza

- In Phase 1, the East Plaza would provide a paved venue for outdoor events adjacent to the expanded portion of the Exposition Hall. It could also serve as a staging area and meeting place near the amphitheater.
- This area would also be suitable for art installations, either permanent or temporary.



Amphitheater

- In Phase 3, with expansion of the Exposition Hall, a new amphitheater is proposed to replace the Fair’s existing 6,000-person concert venue. The new amphitheater is intended as a series of grassy terraces with concrete seat walls and steps for flexibility and visually appeal. A portion of the terraces may be designed to accommodate tables and chairs, so that the amphitheater can accommodate dinner concerts, weddings, and similar events.



- To protect the amphitheater from freeway noise, the upper areas should include berms and/or walls as suggested by Figure 4.20: Amphitheater Section.
- Mesh turf should be considered for amphitheater terraces.

Demonstration Farm

The Demonstration Farm is envisioned for Phase 1a or 1b. Modeled after the popular Centennial Gardens in Orange County, the Demonstration Farm pays homage to Solano County’s rich agricultural heritage and provides an outdoor living classroom for children and families to learn about new techniques in urban agriculture, horticulture, composting, food preparation, healthy living and solar energy or other alternative energy technologies (for example, biofuel production).

Located at the eastern terminus of the Creek Park, the Demonstration Farm celebrates and carry forward the traditions of the Solano County Fair while allowing for exploration and year-round visits from families and school groups.

- The farm should be located close to parking areas to allow easy access for school groups, visitors and service vehicles. The farm should be secured by permanent fencing as needed for security and operations.
- The Demonstration Farm should be planted with rotating crops in all seasons to provide year-round visual interest.

4.3.5 Fairgrounds Fencing, Walls and Gates

Figure 4.21 illustrates the locations of proposed fencing and gates for the Fair of the Future.

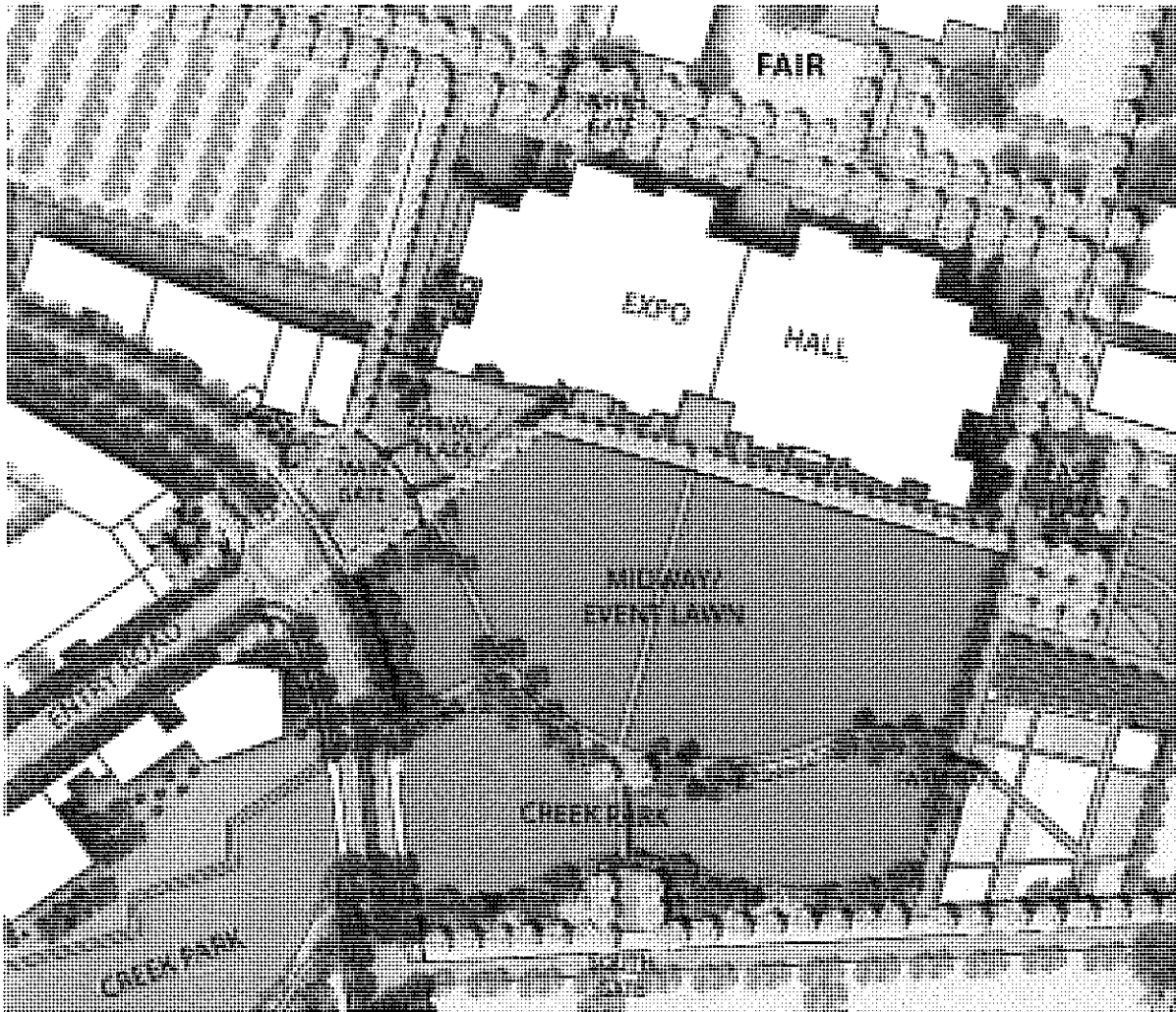


Figure 4.17: Arrival Plaza Illustrative (Phase 3/Buildout Condition)



Figure 4.18: Arrival Plaza Perspective

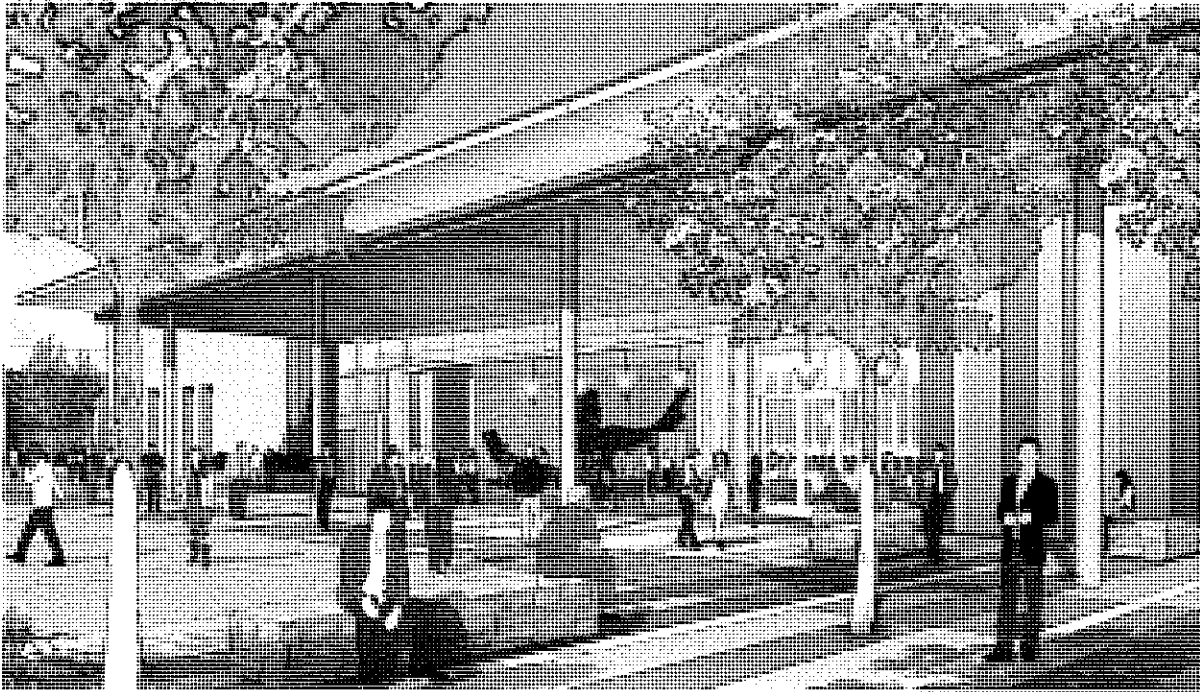
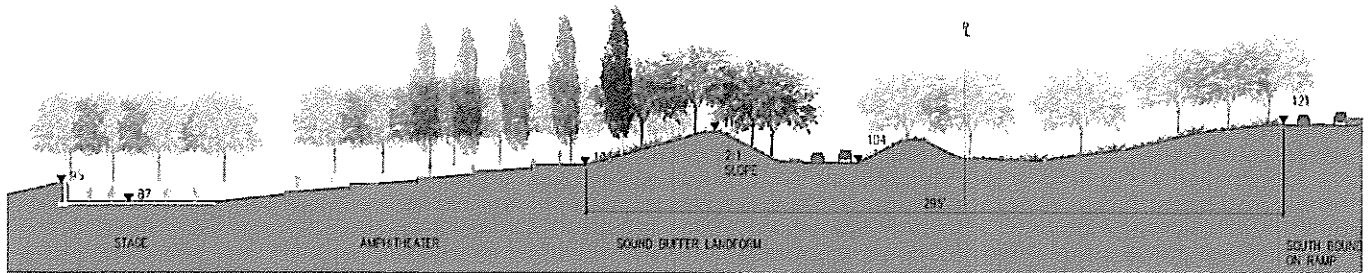
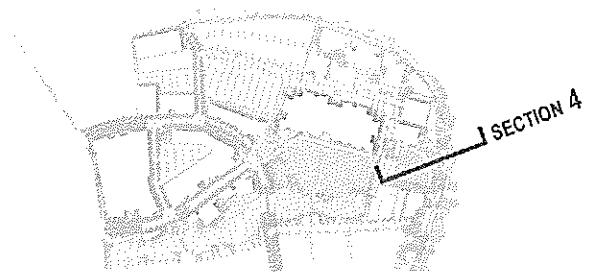


Figure 4.19: South Lobby Perspective



SECTION 4

Figure 4.20: Amphitheater Section



Entries are planned for:

- North Gate at the existing concourse to serve the Exposition Hall and buildings including the satellite wagering facility and McCormack Hall.
- Main Gate at the Arrival Plaza to serve the Exposition Hall, overall Fairgrounds, Creek Park, and pedestrian traffic along the Entry Road.
- South Gate at the Creek Park to link from Shared Public Parking into the Midway and central areas.
- Farm Gate to also link from Shared Public Parking and serve school groups coming to visit the Demonstration Farm.
- Service gates at the north and south ends of the perimeter service road.
- In general, the Fairgrounds should appear open and welcoming to visitors throughout the year. A fortified, "closed for business" appearance should be avoided.

While providing an open, park-like appearance, the Fair's edges and entry points should be designed to provide flexible solutions for safety, security and controlled access to a variety of ticketed venues, with separate gates for concurrent events.

- Attractive, permanent frontage fencing of six to eight feet in height should be used along the more public and visible edges of the Fair, as defined by Figure 4.21. Such fences should be combined with landscape planting and constructed of wrought iron or similar high quality materials. Metal fences may be mounted on a low masonry wall, and/or spanning masonry piers.
- Movable barriers used at the Arrival Plaza for Fair Week and other special events should be designed to create an attractive, festive appearance. Portable ticket booths and other gateways elements should likewise be designed to be compatible with the Exposition Hall architecture and convey an image of quality befitting the Fair of the Future.
- Black, vinyl-clad chain link fencing (with matching posts) may be used to provide security and safety along the north and eastern edges of the Fair and for less visible storage or service areas within the Fair. Evergreen hedges, flowering vines and/or trees should be planted along the base of all security fences. Security fences should be approximately seven feet in height or as needed for security.
- Walls may be used to accommodate grade transitions and provide informal seating areas along the water feature, amphitheater, or other areas. Walls should provide an image of permanence and quality, and may be used as locations for signage and permanent graphics.
- Plywood, un-clad chain link, barbed wire or razor wire fence are prohibited.

4.3.6 Fairgrounds Signage, Lighting and Site Furnishings

- Signage for the Fair of the Future should be designed as a comprehensive "family" of elements to:
 - announce arrival at entry gates,
 - provide schedule of current and upcoming events,
 - direct service vehicles and pedestrians to their destinations, and
 - supply information on the Fair's history and current features.

- Signage may be incorporated into gateway features such as the Arrival Plaza’s turnstile/ security check point.
- Signage should be considered in conjunction with other site furnishings including lighting and seating.
- All site furnishings should be selected to be low-maintenance, durable and attractive elements that harmonize with and complement the Exposition Hall architecture.
- Fairgrounds lighting fixtures should provide attractive, low-level lighting that promotes a safe environment for all users, but remains pedestrian-oriented.
- Lighting should utilize LED or other energy-efficient fixtures that provide pleasing light color.
- Materials for lighting fixtures should be durable and low maintenance. Natural finishes like bronze and nickel steel are recommended.

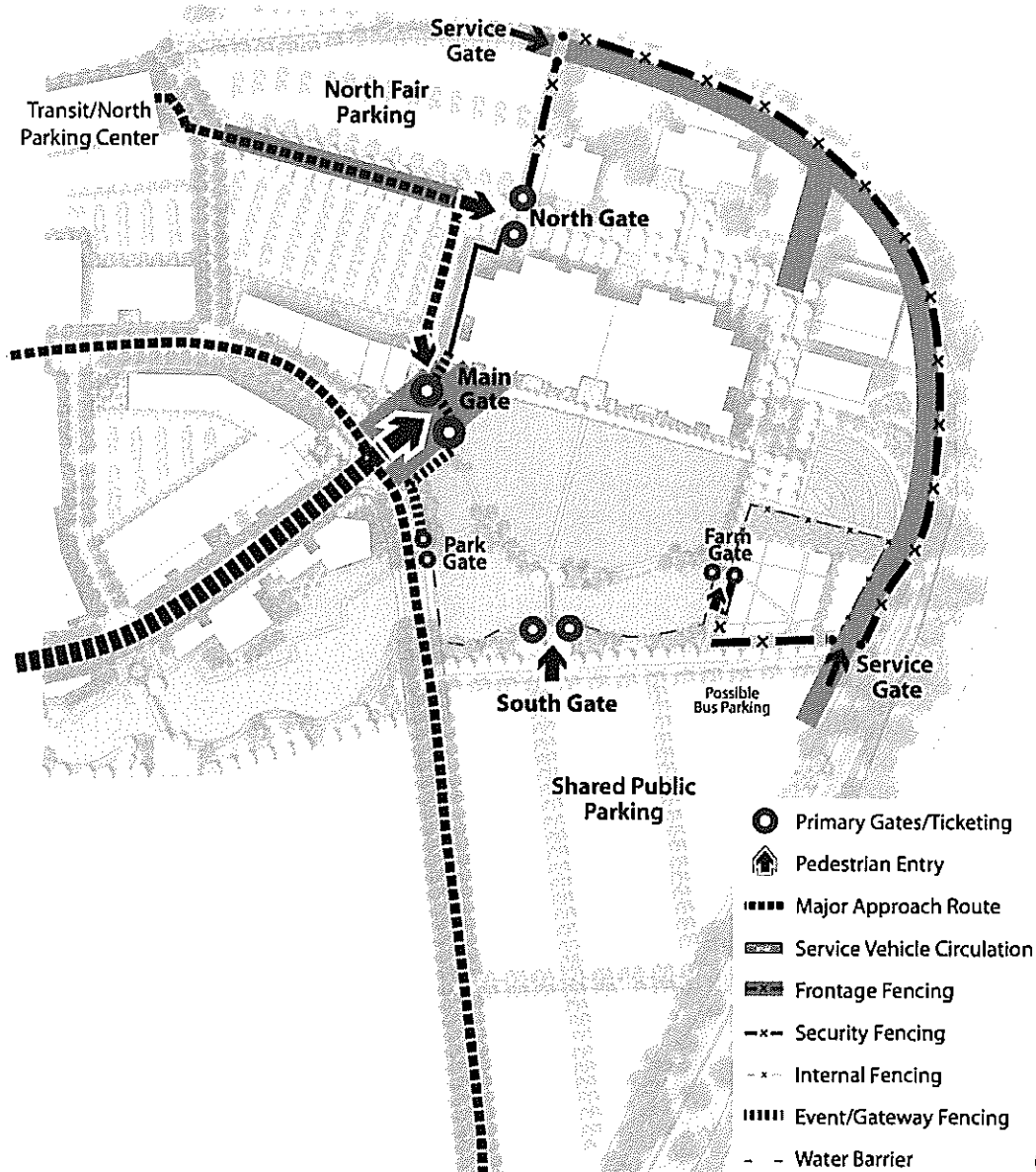


Figure 4.21: Fairgrounds Fencing and Gates
Building areas depicted here are conceptual only.

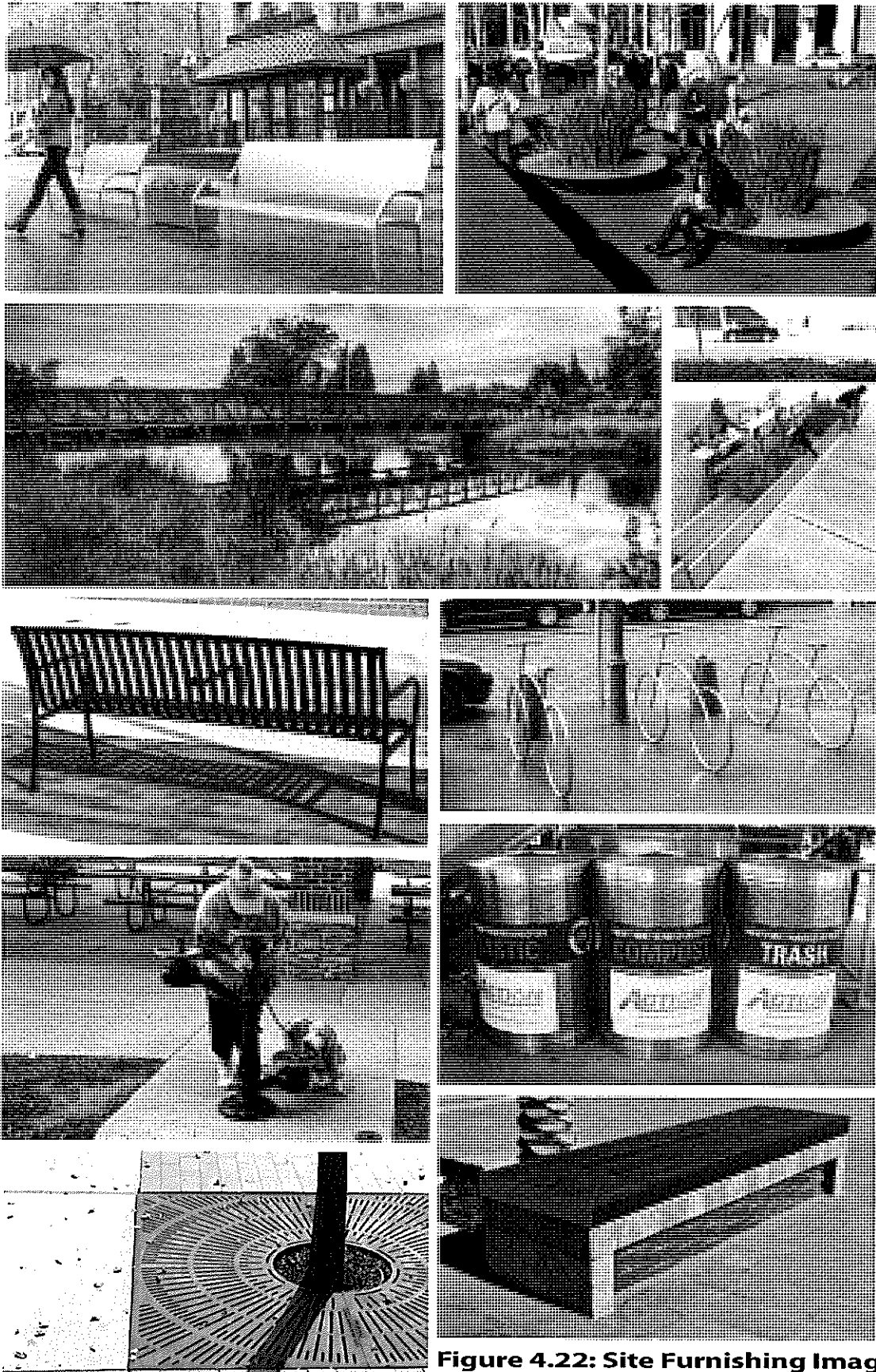


Figure 4.22: Site Furnishing Images

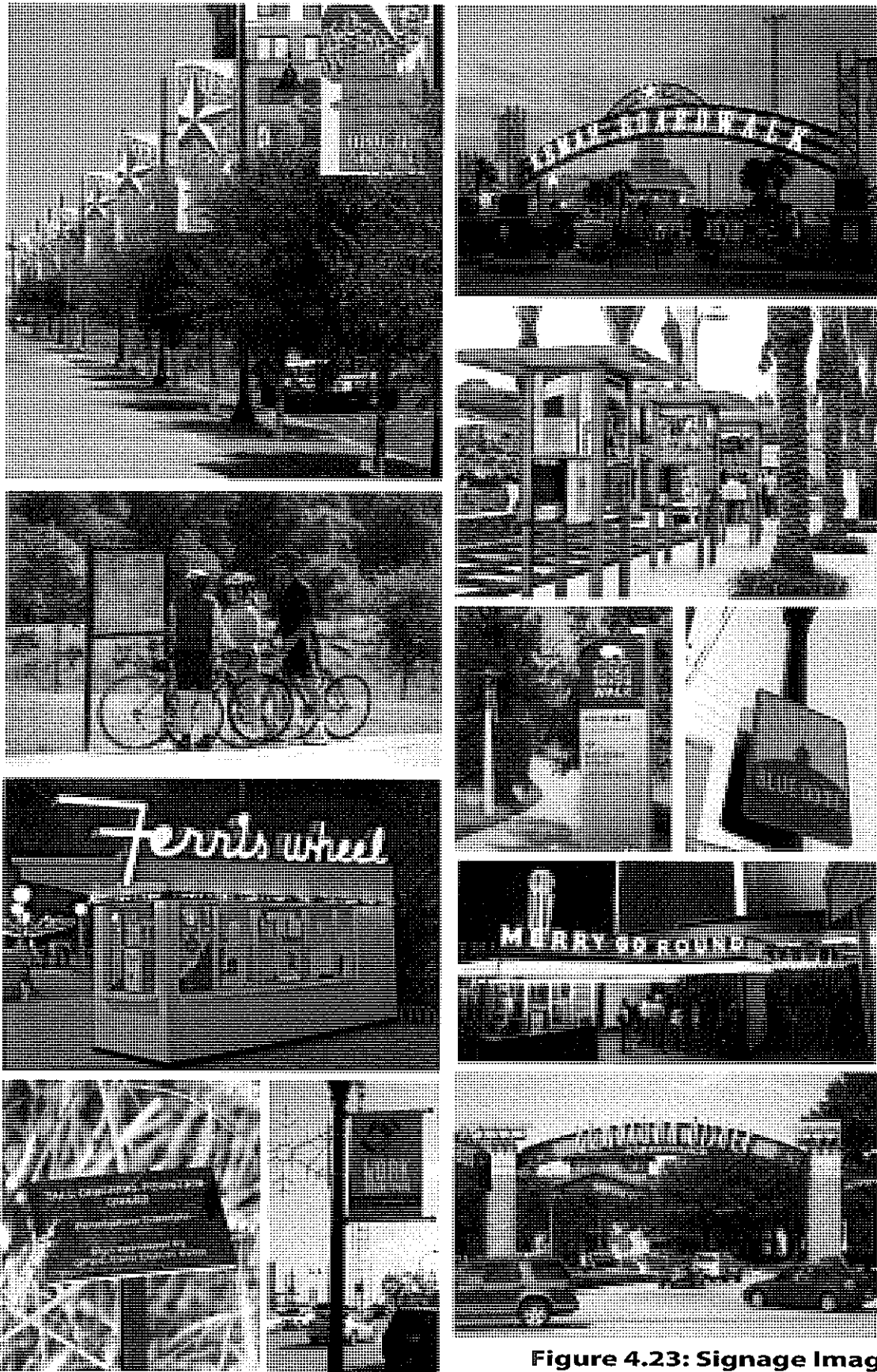


Figure 4.23: Signage Images

4.4 GUIDELINES FOR RIGHT-OF-WAY AND OTHER PUBLIC AREAS

4.4.1 Streetscape and Entries

Streetscape

- Streetscape should conform to the street sections provided in Figures 4.24 to 4.26 and the provisions of Chapter Five.
- Regularly-spaced street trees should be installed as part of roadway construction to along all new roadways to visually unify street edges, establish an identity with the Plan Area, provide a sense of visual enclosure along corridors and perimeters, and generate shade for pedestrian comfort.
- Special street sections include the following:
 - The North Loop Road includes a passenger drop-off lane along Parcel 6, northwest of the Exposition Hall Arrival Plaza. This drop-off serves visitors to the Exposition Hall and also helps to activate a small entry plaza within the Parcel 6 EMU development.
 - The South Loop Road segment between the Entry Road and the bridge includes the same travel lane dimensions as the North Loop Road, with 10-foot wide monolithic sidewalks and no landscape area. Tree wells may be included in the sidewalk, but any additional landscaping would be located within the adjacent Fair or EMU parcels.
 - At the bridge itself, the South Loop Road sidewalks are 12 feet wide to serve bicycles and pedestrians. This segment does not include any street side landscape.



- Streetscapes should reflect the hierarchy and identity of the roadway system. Taller trees should define the Entry Road and Loop Road, with the most impressive tree type marking the Entry Road. Medium-sized trees may articulate the Sage-Loop Connector Road and secondary onsite roads.
- Major streets should be planted with single species of trees to establish gracious and distinctive corridors. Trees should be used to enclose the street, create a comfortable pedestrian scale, and contribute to the identity of the street. Plant selection should consider City of Vallejo guidelines and be limited to hardy species that are drought-tolerant and will thrive in local climate and soil conditions.
- In general, street trees should at maturity be medium or large canopy trees, equal to or greater than the height of adjacent buildings. The planting pattern and species may vary at intersections to provide a flowering or contrasting tree.

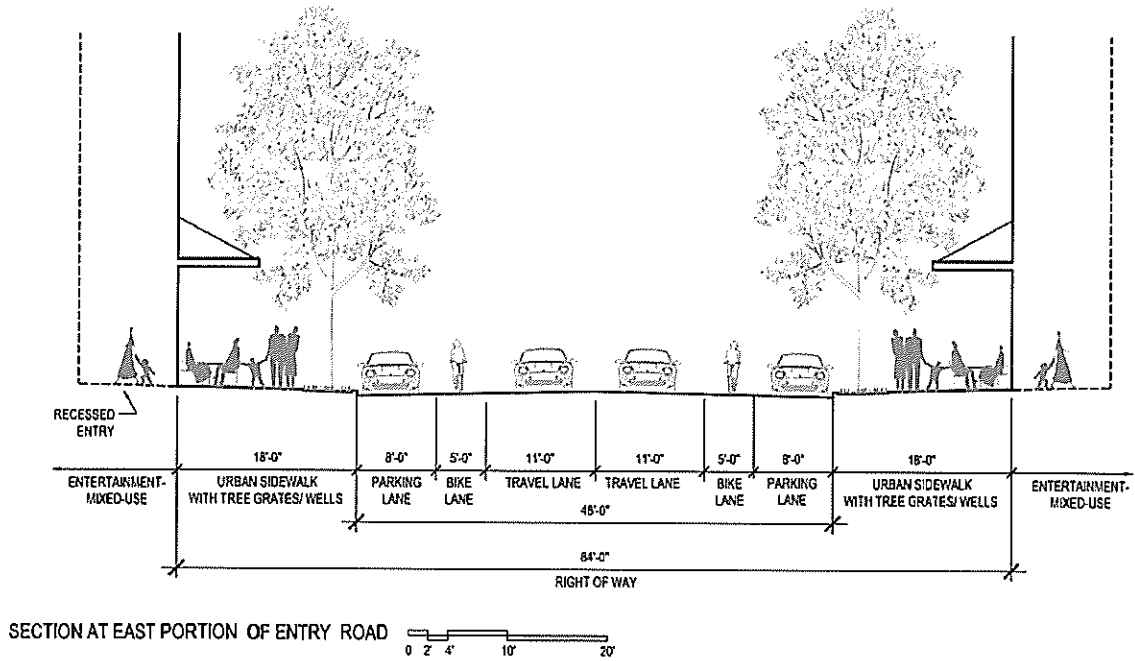
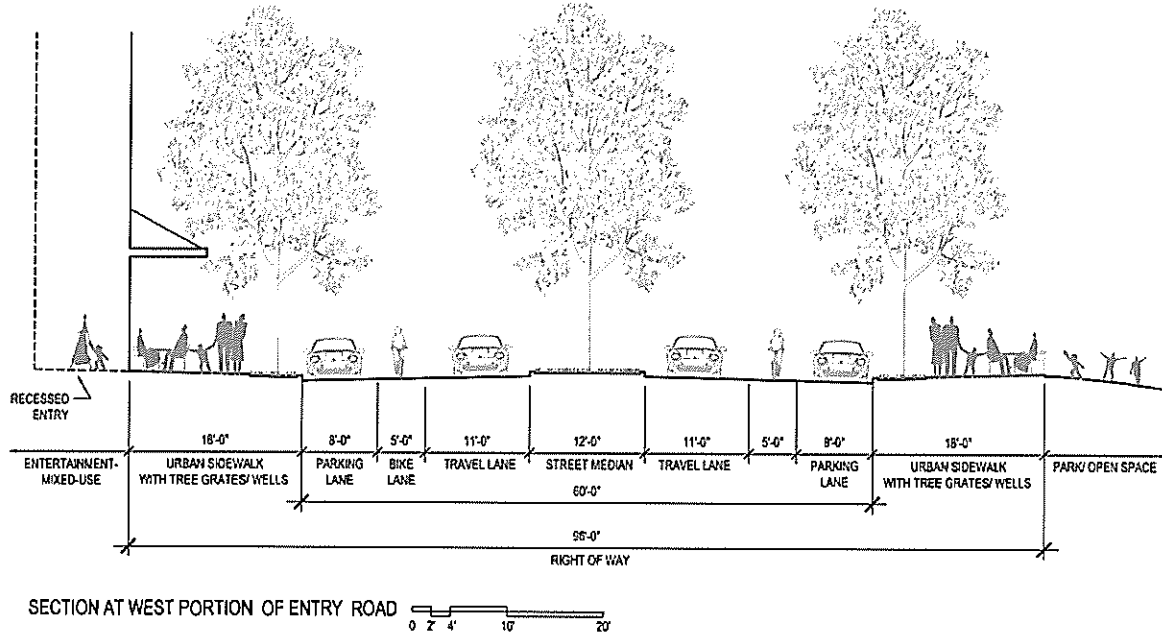
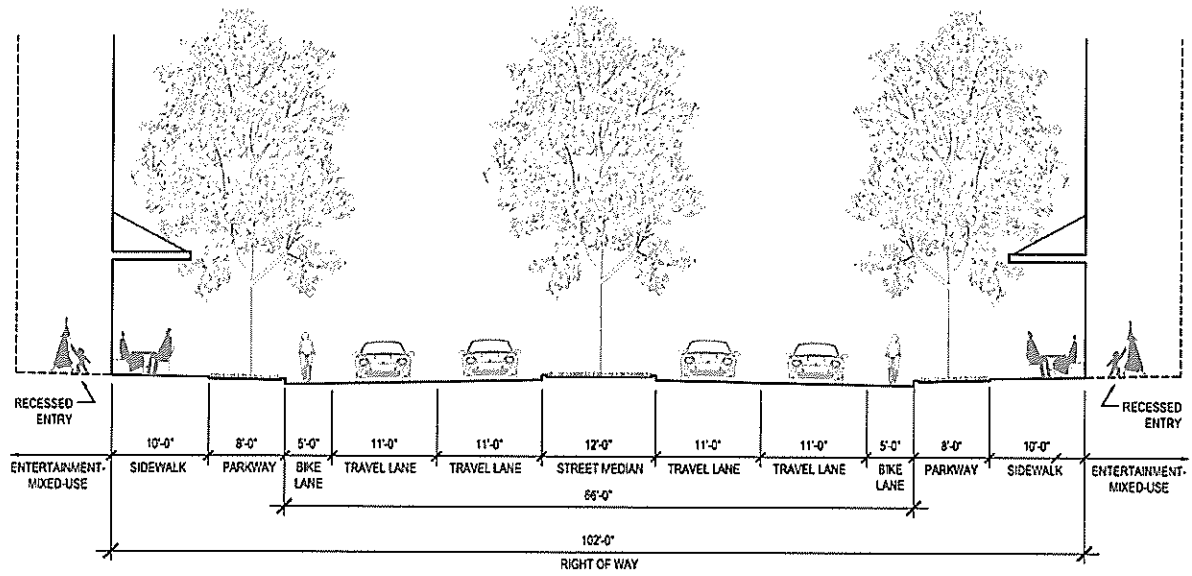


Figure 4.24: Entry Road Sections

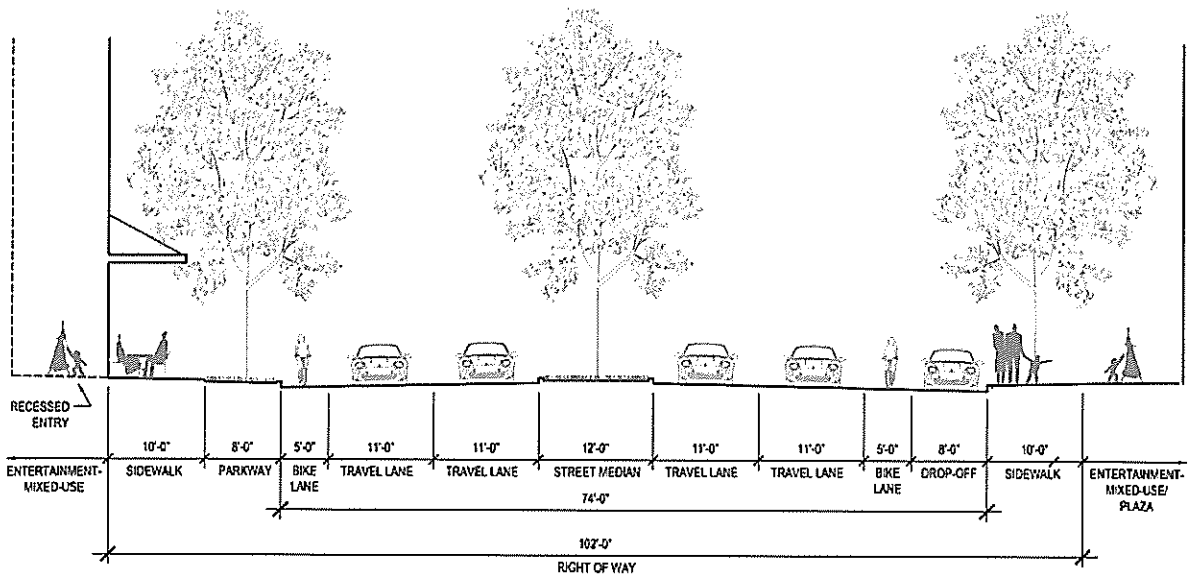
- Trees should be planted between the curb and the sidewalk to protect pedestrians and reduce the scale of the street. Large street trees should be regularly spaced, typically 25 feet on center, but spacing may vary to accommodate street lights, driveways and utility boxes, or other conditions. Smaller scale trees may be spaced more closely.
- For street promenades along the Entry Road and at the pedestrian drop-off near the Arrival Plaza, trees should be provided within minimum five-foot wide tree grates.
- Parkway strips between sidewalks and the curb should be a minimum of seven feet in width, measured from sidewalk to face of curb. Parkway strips should be planted in low maintenance shrubs, groundcovers or lawn, grasses or wild flowers. Plant material should be selected to be well-suited to location; for example, lawn is preferred to shrubs in areas where foot traffic is expected.
- Parkway strips should not be compacted as part of road bed preparation, or if compacted should be properly amended to support healthy root development and plant growth.
- Non-fruiting street trees species are preferred. If fruiting trees or vines are utilized, they should be located so as not to overhang sidewalks or otherwise create maintenance problems.
- Where bump-outs are provided, trees may be shifted into the enlarged planter area provided sight safety distances are maintained.
- Design of the Solano 360 public open space and street areas should create a consistent character and environment conducive to entertainment and urban activities, with a festive and colorful atmosphere.
- Site furnishings (including lighting, seating, wayfinding and waste/recycling receptacles) throughout the Plan Area should be designed and selected to establish a unified vocabulary of related forms and materials to reflect a sense of unity and identity.
- Bike lanes and pedestrian multi-use spaces will characterize the street environment in the Plan Area. As such, lighting, signalization and signage should be pedestrian-scale and should facilitate easy pedestrian and bicycle movement.
- Seating should be provided at frequent areas throughout the Plan Area in the form of benches, movable tables and chairs and seat walls to encourage walking while providing rest opportunities.
- Low road speeds throughout the Plan Area should be defined to foster pedestrian and bicycle-friendly streets (see Section 5.2.1 for traffic calming features).

Entries and Intersections

- Roadway entries into the Solano360 Plan Area should provide a sense of arrival and celebration. The primary pedestrian and "ceremonial" entry at the Entry Road should be designed to welcome pedestrians and orient views toward the water feature. The Loop Road entries should likewise provide a strong sense of place, with clear signage indicated vehicular routes to parking areas.
- The Sage Street entry should emphasize clear signage for service vehicles, buses, and Transit/North Parking Center access.
- Entry plans should be prepared for each project entry prior to development of adjacent improvements. These plans should address landscape, pedestrian access,

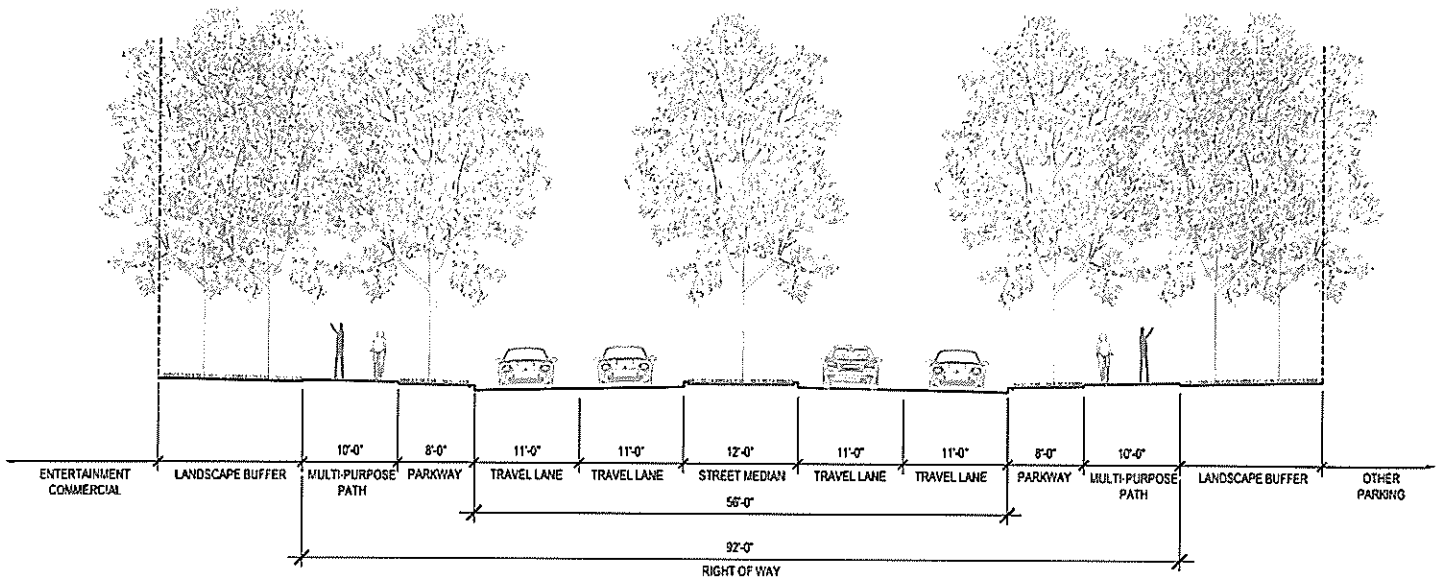


SECTION AT NORTH LOOP ROAD

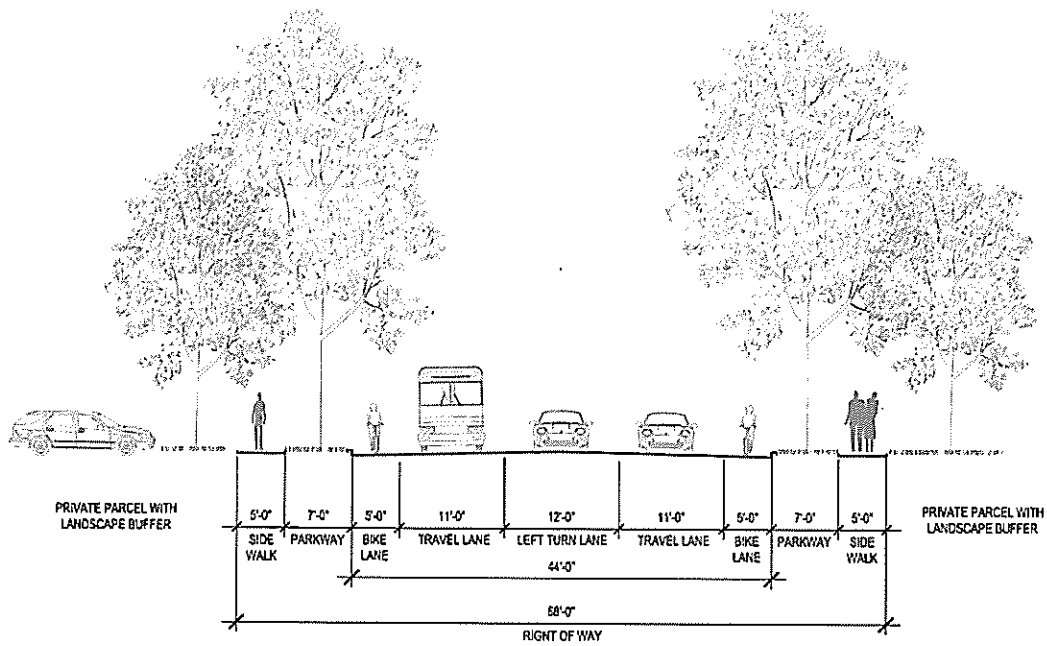
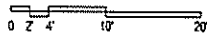


SECTION AT NORTH LOOP ROAD / FAIRGROUNDS DROP-OFF

Figure 4.25: North Loop Road Sections



SECTION AT SOUTH LOOP ROAD



SECTION OF CONNECTOR STREET
(SAGE TO N.LOOP)

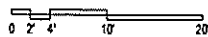


Figure 4.26: South Loop Road and Sage-Loop Connector Road Sections



grading, drainage, monuments, signage, lighting and other public amenities.

- The design of the intersection of the Entry Road and Loop Road should include special features for traffic calming and pedestrian comfort. As envisioned, this stop sign-controlled intersection will be raised six inches to alert vehicles and provide continuous, level crossings for pedestrians from the Entry Road promenade through to the Arrival Plaza.
- Other intersections along the Entry Road and Loop Road should also include traffic calming, bulb-outs to narrow the crossing distances for pedestrians, high-visibility striping, and special paving or textured crosswalks to enhance pedestrian safety. Up lighting may be considered to enhance safety at night and provide a festive atmosphere.

4.4.2 Creek Park and Water Feature

The Creek Park is a critical project component, not only because of its ecologic and hydrologic function, but also because it will provide an important public open space and recreational amenity for visitors and future residents.

The Creek Park forms a new open space corridor through the site with waterfront promenades, picnic areas, lawn terraces, water view plazas, wetlands, and bridges. This example of sustainable design addresses drainage, flooding and water quality issues while providing an iconic feature that visually enhances the project’s entries and activities within the central area.

Appendix F provides additional design criteria addressing water balance, water quality management, creation of wetlands, shoreline conditions, and shoreline safety.

Landscape and Amenity Features

- Creek Park should be a comfortable and beautiful multi-use space.
- The Creek Park should be planted with native and low-water vegetation to minimize irrigation needs.
- Plantings on flat, upland areas should vary from garden-like and decorative to more hardy species conducive to play, but requiring little maintenance.
- Pedestrian amenities within the park, including lighting, seating,



wayfinding and waste/recycling receptacles should be designed and selected to establish a unified character for the park.

- The South Loop Road crossing over the water feature should be designed economically, while creating the appearance of a continuous waterway.
- A variety of edge conditions along the waterfront should be established to provide a safe and visually intriguing waterfront with opportunities for enjoyment of the water.
- Figure 4.27: Water Feature Section describes how the water feature could incorporate a wall or bulkhead in some areas, with riparian vegetation in other areas (see Appendix F for further details).

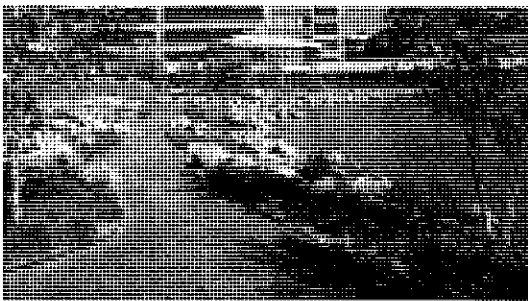
Recreation Opportunities

- The park should accommodate a wide-range of passive and active recreational uses including strolling, jogging, people watching, enjoying views, picnicking, meeting with friends, kite-flying and similar activities.
- Pedal boat rental could be considered as a concession in the Fairgrounds portion of the Creek Park so that visitors can interact with the park via the water feature.

Hydrological Function

Onsite stormwater will be routed through the Creek Park water feature which will discharge into an existing storm drain system and then into Lake Chabot. Offsite stormwater flows from Rindler Creek and/or Blue Rock springs will not be diverted through the onsite water feature but will continue to flow through the Fairgrounds Channel (Chapter Six provides additional detailed information).

- The water feature will capture, treat and store onsite stormwater runoff for water quality improvements and re-use (see Chapter Six).



- The minimum surface area and depth should be based on flood control and water quality requirements. The surface area is planned to be approximately 5.4 acres and the depth will be eight feet with a shallow shelf for wetland planting and safety (see Chapter Six and Appendix F for additional details).
- Sufficient freeboard should be provided between the normal water surface elevation and adjacent development, taking into account the varying types of land uses. Freeboard should be designed to accommodate fluctuations in the water elevation for water quality and flood control purposes.
- The minimum distance between shorelines should provide sufficient space for sides slopes taking into account the varying types of edge conditions. The maximum distance between shorelines should take the bridge designs into consideration. The

maximum bridge span is currently planned to be no greater than 100 feet.

- Side slopes may vary depending on the edge conditions, safety considerations and liner requirements. In general, slopes should not exceed 4:1 in most locations. The bottom surface should be sloped at 2% minimum toward the middle of the water feature.

Access

- Plaza and hardscape areas along the west side of the park are associated with retail, shopping and dining uses along Entry Road and should engage pedestrian activity as follows:
 - A main plaza should be established along the north waterfront, visible from Entry Road.
 - Plaza and hardscape areas along the waterfront should provide ample room for dining and viewing.
 - West Creek Park and all plaza and hardscape areas should be publically accessible, year round.
- The east portion of Creek Park is associated with the Fair of the Future programming. With the exception of facilities operated by private companies, for example a Ferris wheel, these portions of the park should be publically accessible except during major ticketed Fair events and as needed for maintenance and security of Fair facilities.

4.4.3 Fairgrounds Channel

- To the extent possible within the designated Fairgrounds Channel area as shown by Figure 3.1: Land Use Plan, the channel should be defined in a natural-appearing manner, with a meandering horizontal alignment and banks that vary in slope. If meandering or varied side slope angles are not possible within the Fairgrounds Channel area, the channel bottom should be constructed to undulate as much as is feasible, without creating undesirable ponding.
- The final design of the drainage corridor must meet the hydrological requirements for flood control and conform to the space limitations of the designated Fairgrounds Channel area.

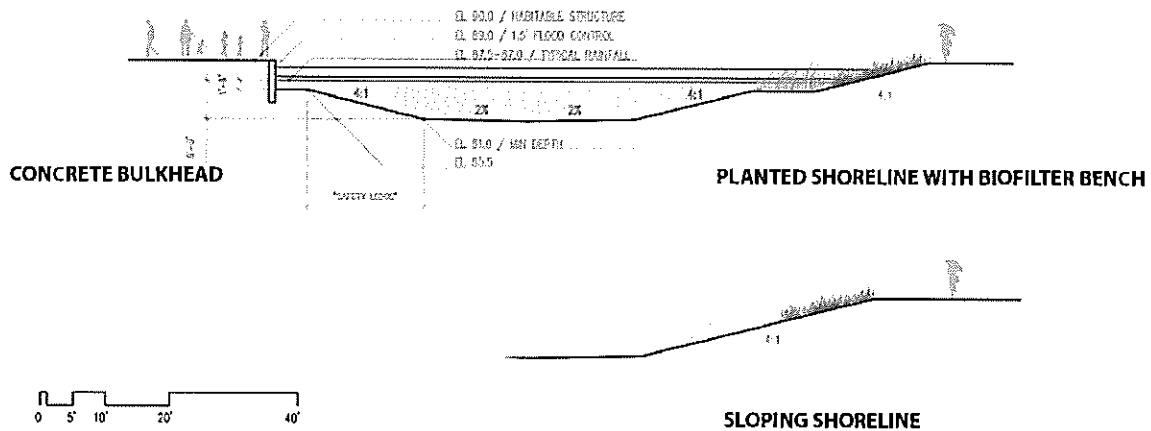


Figure 4.27: Water Feature Section

- To increase the biotic value of the drainage channel, planting benches should be incorporated into the channel design. The banks of the creeks should be stabilized with native vegetation such as willow, and other native riparian plants adapted to the climate of Vallejo.
- Where feasible, the native tule at the bottom of the current channel may be left and will recruit naturally, as will sedges and rushes that could be planted on the channel benches. Side slopes should be planted with a variety of riparian plants adapted to the local climate; these include willows, coyote bush, wild rose, and native grasses. The overstory may be planted with larger, native trees such as sycamore and oak to provide shade and provide a visual buffer from adjacent freeways.
- Invasive species, such as arundo, tamarisk, or star thistle, should be eradicated if present along the drainage corridor.
- Preconstruction surveys should be carried out for special-status species, nesting raptors, nesting song birds and for roosting bats if mature trees will be removed along riparian area. To prevent direct take of a special-status species, under provisions of a Section 7 permit, any special-status species should be moved to a safe location or appropriately mitigated for, according to the requirements of the permitting process.
- Best Management Practices should be used to avoid siltation of the drainage channels from any onsite stormwater runoff.
- A SWPPP should be prepared specifically for the conditions of the site in compliance with the NPDES permit. Examples of BMPs include:



- Conduct all in-channel construction activities during the regional "dry" period as approved by the RWQCB. All efforts should be made to perform all channel work potentially impacting surface waters during periods when surface water flows are at their lowest point.
- No diversion of surface waters should occur during migration periods for special-status species.
- The re-vegetation of banks should follow guidelines and specifications as outlined by environmental review for the Solano 360 project.
- If creek flow is from Rindler Creek and/or Blue Rock Springs Creek is determined to be perennial, work should be conducted during the lowest flow portion of the year. Stream flow should be diverted around the work area using temporary bypass pipes, flumes, or excavated channels that temporarily re-route water around construction area(s). A qualified biologist should be present documenting the conditions and the impact of the construction activity, and assist in relocating stranded wildlife, where necessary.



- Erosion control blankets and/or mats should be used to control erosion of banks and offer bank stabilization.
- Project construction should comply with all terms and conditions of a Streambed Alteration Agreement. Depending on the results of the Phase 1 ESA, and in coordination with the RWCQB, borrow materials should be examined for potential contaminants (e.g., mercury).
- The channel design should incorporate a walking/jogging trail as indicated in Figure 5.10: Pedestrian Circulation. To avoid adding extra width to the channel, this trail should make use of maintenance driveways if possible.

4.4.4 Transit / North Parking Center

The Plan proposes 2.2 acres for a transit/parking facility in the northwest area of site. The Transit/North Parking Center will provide bus access and parking through all phases of the project. In Phase 1, this consists of a bus stop and surface parking. Starting in Phase 2, a three-level parking garage will replace surface parking to serve commuters during the weekdays and parking for the Fair on weekends and at night.

Guidelines are as follows:

- The Transit/North Parking Center access should be from Sage Street and the North Loop Road.
- Buses, shuttles (to/from local hotels, nearby major entertainment uses and the Vallejo Ferry Terminal), taxis, Paratransit (and similar services for disabled individuals), personal electric vehicles and bicycles should be encouraged to use the Transit/North Parking Center.
- Secure bicycle parking should be provided and a bicycle repair and rental facility should also be included.
- Priority parking should be available for disabled persons and car-share services.
- Priority parking should be available for certified pure zero emission vehicles (100% battery electric and hydrogen fuel cell) and compressed natural gas (CNG) vehicles.

4.4.5 Public Parking

Public parking will be provided in parking lots and garages as shown in Figure 5.14: Land Use and Parking, and on the Entry Road.

- Parking facilities should adhere to the guidelines in Section 4.2.4: Parking Areas.
- Parking structures in Public Purpose Areas are not required to incorporate retail uses or other non-parking uses at street level.
- To provide screening from public view, landscape plans for parking structures should include planting, trellises, vegetated walls or other decorative screens, both at the ground level and along vertical walls at street frontages or other public area and open space frontages.

4.4.6 Electronic Reader Boards

Electronic reader boards are planned along the freeway edges, in the locations shown on Figure 4.5: Site Relationships. These signs are intended to provide a revenue source for the Fair and include a new electronic reader board along SR-37, an upgraded electronic reader board along I-80, and two static electronic signs along I-80.

- Design and siting of electronic reader boards should not impede Fair programming or detract from the overall visual and aesthetic character of the Plan Area.
- Electronic reader boards should be oriented away from the Plan Area and toward freeways.
- Electronic reader boards should not contribute to light pollution that would affect nearby residences and should not adversely impact highway travel safety.

4.5 GUIDELINES FOR PRIVATE PURPOSE AREAS

Private Purpose Areas consist of the Entertainment Mixed Use (EMU) parcels, totaling 18.8 acres, and the Entertainment Commercial (EC) parcel of 30 acres. These uses are distinct, as follows:

- EMU development is envisioned to create a connected, walkable area of family entertainment commercial (FEC) businesses and associated restaurants and retail, with buildings oriented to Entry Road, Creek Park, and North Loop Road. As the intensity of this area increases through Phases 2 and 3, development will include vertically mixed uses that contribute to a vibrant, pedestrian-oriented Public Entertainment Core.
- EC development is envisioned to be a single destination theme park or amusement park with outdoor rides and venues visible from adjacent freeways and public roads, contributing to the visibility and identity of Solano360 as an entertainment district. Should the EC area be developed as a multi-parcel, mixed-use commercial center, the land use and design provisions for EMU areas will apply.

Section 3.6 provides land use policies for these areas.

Section 4.2 establishes guidelines applicable to all portions of the Plan Area, including the Private Purpose Areas. The following guidelines address additional site and architectural standards for EMU and EC development.



4.5.1 Entertainment Mixed Use (EMU) Guidelines

Urban Design

- Primary intersections, particularly those along Entry Road and Creek Park, should be reinforced with high quality landmark buildings or gateway elements to support the identity of the Plan Area. Such buildings should exhibit thoughtful, imaginative architectural design to welcome visitors and promote a pedestrian-oriented character.
- The Entry Road should provide an urban, pedestrian-oriented corridor of specialty shops and services, restaurants, tree-shaded sidewalks, and art illustrating the history of Vallejo and Solano County, all developed at an appealing pedestrian scale.

- Design of buildings and outdoor spaces along Entry Road should utilize complementary color, special materials, signage, furnishings and landscaping to promote a unique identity and active commercial heart for the Plan Area.
- Buildings and entries should be located primarily at the back of road rights-of-way. Where building entries are set back in courtyards, paseos, or arcades, landscape features such as vertical planting treatments, trellises, or decorative walls should define and clearly mark such openings at the street edge.
- To create a “restaurant row” and active pedestrian promenade along the Entry Road, blocks that include FEC’s or large retail stores are envisioned to include smaller footprint storefronts along the primary road right-of-way (see Figure 4.28: Entertainment-Mixed Use Building Prototype).
- Entries to large footprint buildings, such as FEC’s or large retail stores, may be recessed, emphasized with architectural elements, or otherwise articulated to identify entry points to primary FEC uses.
- Development along North Loop Road in Phase 3 may also include large footprint buildings, but should also incorporate smaller, street-oriented retail shops with recessed entries or entries off of an interior courtyard or arcade.
- All buildings should provide a clearly articulated pedestrian entrance, either via storefront, recessed storefront, arcade or courtyard, with direct pedestrian access to either North Loop Road or Entry Road.
- Parking should be located to the rear of parcels. By Phase 3, no surface parking lots should front on either Entry Road or North Loop Road.
- Open spaces for recreation, gathering and visual relief should be designed to appear deliberate and not as “left over” space between buildings.
- Outdoor dining should be encouraged along sidewalks and promenades to promote street activity.

Architectural Design

Buildings should reflect the vibrant, urban mixed-use nature of the Solano360 Plan Area, supporting the pedestrian character of streets and contributing to an overall identity for the project.

Massing and Articulation

- Buildings should establish continuous storefronts and courtyard openings along Entry Road and, in Phase 3, North Loop Road. Buildings should maintain a distinctive urban character with storefronts oriented to streets.
- Building frontages should contribute to an active street life by providing ample seating, gathering places, and exterior protection from sun and rain in the form of recessed walkways, awnings, canopies, or trellises along primary pedestrian traffic areas.
- Building façades longer than 200 feet should be designed to appear as more than one building, aggregated on the block with variation in massing, eave/parapet, color, material and balcony depth.
- Buildings should incorporate vertical height variety to break the monotony of long un-interrupted building facades of matching height.
- Building floor plans should be designed with flexibility to accommodate changes in

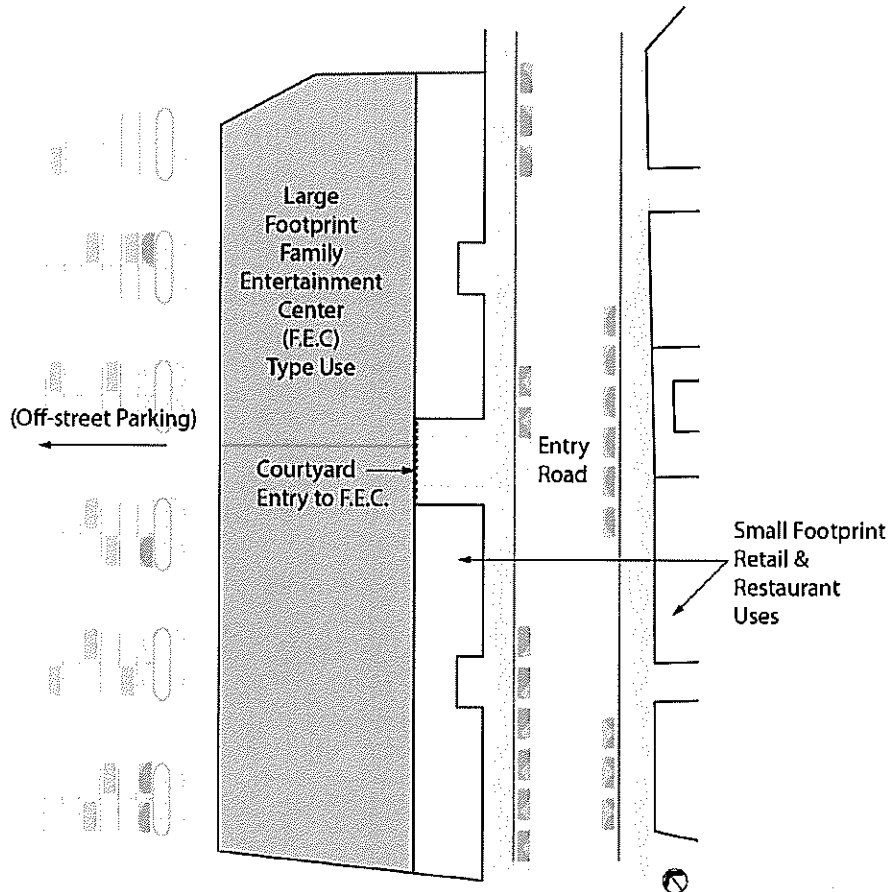
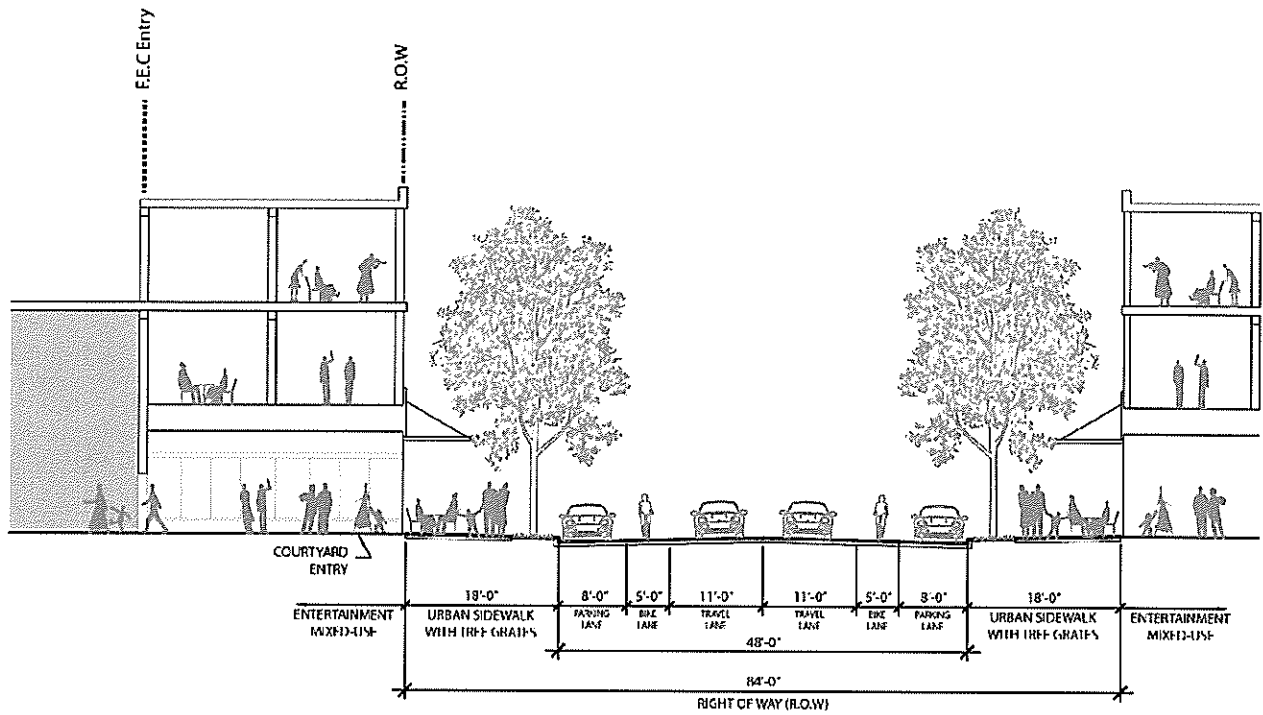


Figure 4.28: Entertainment-Mixed Use Building Prototype
Building areas depicted here are conceptual only.

commercial tenants over time.

- Sun angles should be considered in the design and placement of structures to allow sunlight into deep spaces and provide for both shaded and sunlit public spaces.
- Mechanical equipment should be hidden or screened by architectural elements that match the architecture of the rest of the building.

Windows and Doors

- Wall openings should show depth of the wall, without use of flat or tacked-on window trims.
- Windows and doors should be simple in both design and placement. Use of mullions that divide window into panes of glass is encouraged.
- Building doors and windows facing street frontages should be fully functional.

Porches and Patios

- Upper level patios (either recessed or extended) or French balconies are encouraged, but should be usable and not merely decorative.

Colors and Materials

- Rich materials such as stone, brick, and wood are encouraged. Material mixture must be in accord with the simplicity of building massing.
- Brick and stone should be detailed in proper corner-turning and load-bearing proportions.
- Local materials and vendors are preferred.

Lighting and Signage for Buildings

- Materials for lighting and signage fixtures should be durable and weather well.
- Natural finishes like bronze, nickel steel and sustainably-treated wood are recommended.
- Lighting and signage should be integrated into building design.
- Lighting, where appropriate for convenience and safety, should not cause light pollution or glare into adjacent properties.
- Energy-efficient LED lighting is highly encouraged.
- In addition to wall signs, pedestrian scale signage such as blade signs, awning signs, and window decal signs are encouraged throughout the project to contribute to an active, vibrant pedestrian experience. Signage that clutters pedestrian environments is discouraged.

4.5.2 Entertainment Commercial (EC) Guidelines

In addition to the general guidelines provided in Section 4.2, the following guidelines are included to address the Entertainment Commercial (EC) area.

- Design of the northern portion of the EC parcel should address the Creek Park by incorporating a pedestrian gateway connected to trails and promenades along Entry Road and Creek Park. Design of venues and structures along this northern edge should create appealing, festive views for visitors traveling southbound on Fairgrounds Drive.
- EC entries should be reinforced with high quality, highly visible landmark structures

or gateway elements to support the identity of the Plan Area as an entertainment hub for Vallejo and the greater Solano County. Such elements should exhibit thoughtful, imaginative architectural design to welcome visitors.

- Any security barriers along Creek Park should consist of high quality, ornamental fencing with low vegetation that allows filtered views. Visually impermeable barriers along the Creek Park should be avoided.
- Taller rides and venues, up to 250 feet in height, should be concentrated within the central and eastern portions of the EC parcel in order to maximize visibility from I-80 and provide transitions to Fairgrounds Drive and the Creek Park. Along the EC parcel's northern, western, and southern boundaries, maximum heights should be limited to approximately 150 feet.
- Parking areas should be concentrated in the southern portion of the EC parcel, with active venues concentrated to the north along the Creek Park and the west along Fairgrounds Drive (see Section 3.6: Land Use Policies). Design of venues should consider creation of exciting views from freeways.
- EC development should incorporate locations for shuttle stops along the Loop Road.

4.6 SUSTAINABILITY AND RESOURCE MANAGEMENT

4.6.1 Solano360 Sustainable Design Attributes

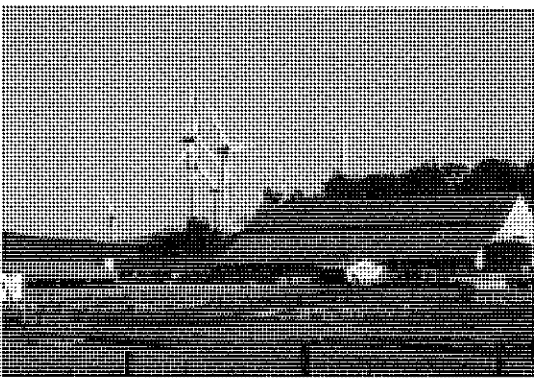
The Plan incorporates sustainable design and development within the land use, transportation, infrastructure, and design provisions described in this document. The following section summarizes those measures and provides cross-references to relevant sections. In addition, this section provides "next step" measures for sustainability that can be incorporated into subsequent design proposals and project implementation.



The following measures incorporate aspects of national guidelines and standards for sustainability, including the United States Green Building Council (USGBC) Leadership in Energy & Environmental Design – Neighborhood Development (LEED-ND) rating system and the Guidelines and Performance Benchmarks identified under the Sustainable Sites Initiative (SSI).

Sustainable Site and Building Design

- **Location and Facility Reuse:** The Plan makes use of areas that have been previously developed, including significant portions of the existing Fairgrounds facilities. Approximately 87,000 square feet of existing Fair building area will be retained as well as the concourse itself (approximately 83,300 square feet.) and associated outdoor (paved and lawn) venue areas totaling over 30,000 square feet. This approach recycles previously disturbed land and reduces the need for construction of buildings and infrastructure. Reusing buildings, materials



and existing paved surfaces also reduces waste, debris, and air quality impacts that would be generated during demolition.

- **Compact Development:** The Plan land use mix emphasizes the phased development of themed entertainment park and family entertainment uses, with flexibility to accommodate office and residential uses. Higher density development helps to conserve land and preserve open space and, when provided alongside a mix of uses, promotes livability, transportation efficiency and walkability.
- **Diversity of Uses:** The housing allowed in the Private Purpose Areas would be located within a quarter-mile (five minute) walk of onsite uses including shops, restaurant, entertainment and offices. As mentioned in Section 3.6.2, establishing a small grocery store onsite would deter some vehicle trips for residents and workers.
- **Open Space:** Open space areas can provide habitat, reduce urban heat island effects and allow for enhanced stormwater management. The Plan establishes a variety of open spaces that encourage walking, physical activity and time spent outdoors. New open space uses include six acres of Creek Park within Private Development Area and three acres within the Fair, two acres of Demonstration Farm, four acres of Midway/Event Lawn, one and a half acres of concert amphitheater, three acres of paved plazas and promenades, and one acre of other gardens and courtyards around the new Exposition Hall (acreages are approximate).
- **Sustainable Building Design:** The proposed conceptual design for the Exposition Hall incorporates sustainable features, such as natural ventilation and photovoltaic roof panels, that will partially enable the building to obtain LEED Silver certification or meet equivalent performance standards, as required by County General Plan policy. The Plan will comply with the Solano County General Plan requirement relative to energy efficiency and green construction policies.



Health and Well-Being

- **Bicycle and Pedestrian System:** In addition to the open space described above, the Plan proposes pedestrian and bicycle routes as illustrated by Figures 5.10 and 5.11. In addition, a jogging circuit is proposed along the Fairgrounds Channel. These public trails, promenades, bike lanes and paths encourage residents and visitors to get out of their cars and walk, bike or jog from

destinations within and near the Plan Area.

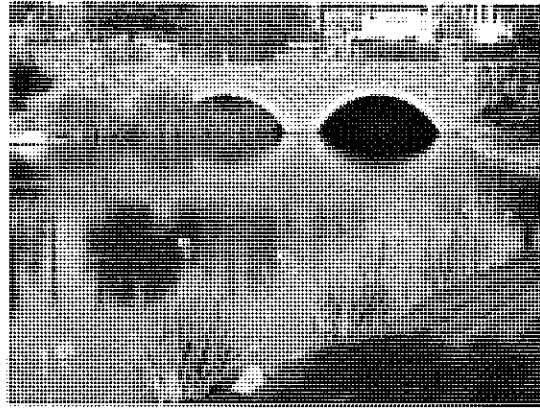
- **Walkable Streets:** Walking is key to providing healthy and sustainable communities. The major roads (Entry Road and Loop Road) provide a minimum of 10-foot wide, tree-shaded sidewalks or multi-purpose paths on each side. Controlled intersections, bulb-outs, and high-visibility crosswalks are provided at onsite intersections to enhance pedestrian safety; this includes the raised intersection at the Fairgrounds Arrival Plaza (see Figure 4.17).
- **Bicycle Facilities:** The Plan proposes bicycle facilities along the Entry Road and Loop Road, connecting to proposed bike lanes on Fairgrounds Drive between SR 37 and Redwood Parkway and allowing easy bike connections to onsite destinations. These facilities consist of bike lanes on Entry Road and North Loop Road, multi-purpose paths along South Loop Road, and secure bicycle parking at key activity nodes including the Fairgrounds and private purpose development (EMU and EC) parcels. The Transit/North Parking Center will also provide a secure bicycle parking area and may include other bicycle amenities such as a bicycle repair facility (see Figure 5.11: Bicycle Circulation).
- **Noise:** To the extent possible, the Plan provides buffers and provisions for onsite uses that may be particularly sensitive to noise impacts. The amphitheater, located in the eastern portion of the Fairgrounds near the I-80 freeway, is buffered by an earthen berm as shown by Figure 4.20: Amphitheater Section. Within the Fairgrounds, the amphitheater is separated from the future midway to avoid noise impacts during multiple events or Fair Week. Possible housing is restricted to the western portions of the Plan Area in order to avoid impacts from noise and air quality. Impacts by the project on offsite uses are mitigated by the distance between noise-generating uses, such as the amphitheater or midway, and sensitive offsite areas such as residential neighborhoods.
- **Equitable Site Use:** Site uses will provide economic or social benefits to the local community, with public access to recreational and civic facilities such as the Creek Park, renovated Fair of the Future and outdoor spaces, and Demonstration Farm.
- **Sustainability Awareness and Education:** The proposed Demonstration Farm provides opportunities to celebrate the historic agricultural character of the area and provide educational programming. Other environmental education programs may be provided through the Fair. Educational and interpretive signs describing restored habitat and water conveyance systems will be located throughout the Creek Park.

Water Quality and Management

- **Flood Control:** The Plan proposes removing the western and southern portions of the Plan Area from the floodplain, alleviating flooding in the offsite mobile home park to the extent possible, and improving the quality of onsite storm runoff. As described in Chapter Six, these improvements involve enlarging the Fairgrounds Channel and adding improving the existing crossing under Fairgrounds Drive.
- **Stormwater Collection and Re-use:** The new multi-purpose water feature within Creek Park will retain and improve runoff from the Plan Area, which can then be re-used onsite for irrigation. It also functions as a recreational amenity and water quality BMP (see Chapter Six). Capture and reuse is consistent with Low Impact Development practices and the San Francisco Bay Area NPDES stormwater quality permit. As described in Chapter Six, a majority of the Plan Area will be designed to drain to the Creek Park water feature for water quality treatment. Portions of the southern Plan Area may

drain to the Fairgrounds Channel depending on the storm drain system hydraulic limitations.

- Potable Water Demand: Capture and reuse of stormwater for irrigation within the water feature will reduce potable water demand. Use of drought-tolerant and local plant species will further reduce potable water demand (see Section 4.2.3: Landscape Plan and Guidelines). In addition, a “purple-pipe” (recycled water) system is planned within each backbone roadway (see Figure 6.3: Non-Potable Water Exhibit). The “purple-pipe” system will be installed in accordance with Title 22 standards for recycled water use in the event recycled water becomes available on a municipal scale.
- Low Impact Design (LID): Structural LIDs proposed by the Plan include the water feature bioswales and rain gardens to collect water from the Exposition Hall roof. Non-structure LID’s include minimization of paved parking areas through creation of shared parking strategies and multi-purpose turf areas, such as the midway, that can accommodate overflow parking.
- Wastewater: The Plan’s water reduction and conservation measures also result in reduced generation of wastewater due to recycling and reduced flows.



Chapter Six provides additional measures (see Sections 6.2.4, 6.3.4, and 6.4.4).

Transportation

- Transit: The Plan provides a multi-modal Transit/North Parking Center where commuters can park their vehicles and board buses bound for job centers or other destinations such as the Vallejo Ferry Terminal. Frequent local bus service will provide a better option for bringing people to the project, reducing the overall traffic impact. The Transit/North Parking Center can also be used for parking during weekend events.
- Linked Trips: The project is designed to include a variety of complementary venues and attractions within easy walking distance of each other, resulting in a 33% rate of linked vehicular trips and a corresponding reduction of transportation impacts.
- Parking: The Plan designates paved parking areas to serve development uses as the project builds out, but minimizes the extent of parking through phased and shared parking strategies and multi-purpose turf areas, such as the midway, that can accommodate overflow parking when it is not in use for outdoor events. Within the Entertainment Mixed Use areas, parking is allocated to the side and/or rear of blocks, creating more pedestrian-oriented streets. Larger surface lots will have landscape buffers at the street and channels edges and will incorporate shade trees or, as

described below, solar arrays for an onsite source of renewable energy.

Energy

- Solar Arrays at Exposition Hall: As described in Section 4.3.3, the main Exposition Hall roof is proposed for a photovoltaic array and/or solar hot water heating panel installation of approximately 24,300 or more square feet. Other buildings and parking facilities are also available for installation of photovoltaics.
- Natural Cooling: The Exposition Hall incorporates a shade canopy to mitigate the effects of solar glare along the south-facing facade.

4.6.2 Next Step Sustainability Measures

In addition to the sustainable provisions embodied in the Plan as described above, additional “next step” measures are proposed for consideration during implementation of projects within the Plan Area.

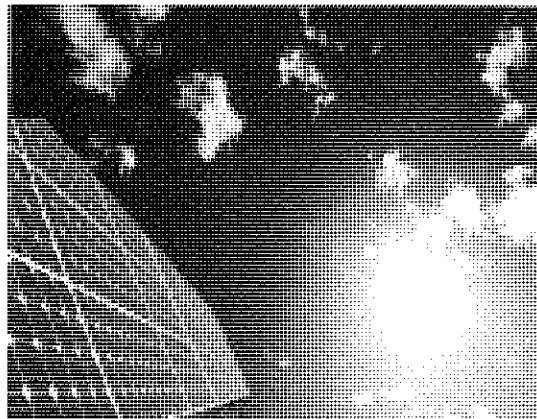
Green Building

- Other green building and low impact design (LID) measures should be considered for more detailed stages of building and site design. These may include:
 - cisterns to capture rain water,
 - recycled water facilities for flushing toilets and other uses where potable water is not required,
 - high efficiency fixtures and appliances within buildings,
 - vegetated roofs and photovoltaic arrays on roofs,
 - use of recycled and locally available materials,
 - maximizing opportunities for natural shading and ventilation,
 - orientation of buildings to maximize energy efficiency and provide natural cooling and ventilation,
 - deciduous trees next to buildings and along streets to reduce ambient temperature, reduce heat gain, allow for cooler natural ventilation, and provide a more pleasant pedestrian environment,
 - deciduous trees and vines in front of south-facing walls and windows to further cool buildings by intercepting sunlight during summer months, yet allow direct sunlight during the winter,
 - green screens (metal lattices planted with vines and/or climbing flowers) to shade south- and west-facing walls to reduce interior heat gain and beautify buildings,
 - trees of appropriate heights and spreads to provide ample shade in the summer months for outdoor spaces such as patios and plazas, pedestrian walkways, roadways, and parking lots,
 - structures such as trellises and porticoes incorporated into the building/landscape edge, especially on south- and west-facing exposures, to provide shade in the summer and allow solar penetration when the sun is at a low angle in the winter,
 - landscape buffers, screens, and windrows to permit facilitate cooling by prevailing breezes in summer months and to reduce interior heat gain, and
 - site lighting minimized to reduce light pollution and minimize energy usage,



using full cutoff luminaires, low-reflectance surfaces, and low-angle spotlights.

- Non-structural LID measures should be established where practical. These may include, but are not limited to, programs to monitor pavement cleaning (street sweeping), illicit discharge elimination, and parking lot design and management.
- Developer of projects within the Plan Area should be encouraged to pursue LEED certification and other green building credits and awards, as such recognition will physically and symbolically represent the sustainability values of Solano360.



Energy

The following measures are in addition to the photovoltaic arrays / solar hot water heating panels planned for the Exposition Hall roof, as described previously. All proposals should be developed in coordination with the County Operations Manager.

- A Public Private Partnership (PPP) with a solar partner may be pursued to provide some of the infrastructure costs associated with the site development. The Plan allocates extensive areas for parking, including approximately 24.7 acres for Shared Public Parking. These large-scale facilities could include photovoltaic arrays to provide onsite energy, shade for cars, cost savings and a possible revenue source (as excess energy could be sold).
- A district energy system, or cogeneration, could be evaluated to provide on-site energy and reduce building water heating and cooling requirements. The water feature in the Creek Park could be utilized to provide cooling via a heat transfer/cooling tower device for adjacent buildings.
- Photovoltaic arrays should be considered for all new and retrofitted buildings, including structures within the EMU and EC areas.
- Wind turbine and other alternative energy technologies could be incorporated into the Demonstration Farm to test and provide educational examples for families and visiting school groups.

Waste Management

- A construction waste management plan could be developed that would identify salvage, recycling or donation of construction materials.

Materials, Operations and Maintenance

- No wood from threatened tree species should be used in construction or finishing.

Certified wood should be used wherever practical.

- Building and landscape materials should contain recycled content wherever practical.
- Materials that are produced and sold locally, including soils, should be used wherever practical.
- Any adhesives, sealants, paints and coatings used should be those with reduced VOC emissions.

CHAPTER FIVE: TRANSPORTATION

5.1 INTRODUCTION

This chapter addresses circulation and transportation within the Plan Area, including off-site and on-site roadways. It describes the existing roadway system, identifies likely improvements needed to support Plan Area development, and establishes policies for transportation, parking and circulation systems within the Plan Area.

The information in this chapter is informed by the Plan's conceptual site plans and may be subject to change as more detailed plans and specifications are developed as part of the design and development process.

5.1 OFF-SITE HIGHWAYS AND ROADS

The Plan Area is bounded by Fairgrounds Drive to the west, Sage Street and SR-37 to the north, I-80 to the east, and Coach Lane to the south. The site is currently directly accessed via Fairgrounds Drive and Sage Street.

- SR-37 is an east-west four-lane freeway that connects I-80 to US 101 in Marin County. A diamond interchange is provided at SR-37/Fairgrounds Drive. East of I-80, SR-37 connects to Columbus Parkway, providing a route through eastern Vallejo to Benicia.
- I-80 is an interstate freeway that runs in a north-south direction directly east of the site. Between three and five travel lanes are provided in the northbound (eastbound) and southbound (westbound) direction, as lanes are added and dropped between interchanges adjacent to the project site.
- Fairgrounds Drive is a north-south arterial road that provides four lanes along most of the project frontage, transitioning to two lanes south of the Six Flags Discovery Kingdom exit driveway.
- Sage Street is an east-west two-lane roadway that connects Fairgrounds Drive to the neighborhood north of SR-37, via an underpass.
- The Solano Transportation Authority (STA) is planning the Redwood Parkway/Fairgrounds Drive Improvement Project that will improve Fairgrounds Drive to a continuous four lanes between SR-37 and Redwood Parkway, plus a third northbound through lane between the Six Flags Discovery Kingdom Exit Driveway/Fairgrounds Drive intersection and SR-37, and improve the configuration and capacity of the SR-37/Fairgrounds Drive interchange and the Redwood Parkway/I-80 interchange. The project is in the Project Approval/Environmental Document (PA/ED) phase.

5.2 ON-SITE CIRCULATION

5.2.1 Vehicular Circulation

The Plan Area will be accessed via:

- Two higher-capacity intersections along Fairgrounds Drive at the North and South Loop Road,
- A lower-capacity but highly visible secondary access from Fairgrounds Drive at the Entry Road, and
- Another secondary intersection on Sage Street (see Figure 5.1) serving primarily service and transit vehicles.

Figures 5.2 and 5.3 show the roadway and intersection configurations in the northern and southern parts of the Plan Area, respectively. Figure 5.4 highlights the turning movements for intersections along Fairgrounds Drive. Figures 5.5 to 5.8 show street sections for the primary roadways.

Major Roads

Entry Road – The Entry Road aligns with the current Fairgrounds entry road, opposite the Six Flags Discovery Kingdom exit. The intersection is currently signalized.

The Fairgrounds Drive/Redwood Parkway Improvement Project is currently designed to provide a second southbound left-turn lane and a third northbound through lane at the intersection. However, in order to create a more pedestrian-oriented character for the Entry Road, the Plan proposes a narrower Entry Road street section of one 11-foot lane and a parking lane in each direction, with a wide urban sidewalk on the north and south sides. Therefore, the intersection with Fairgrounds Drive will only require a single southbound left-turn lane to feed into the single inbound travel lane onto the Entry Road. A center median will extend approximately 250 feet east from Fairgrounds Drive, opening to provide space for a 150-foot left-turn lane into the EMU Parcels 7 and 8.

The Entry Road will intersect with the Loop Road at the Exposition Hall Arrival Plaza, with a raised intersection to provide traffic calming and pedestrian safety. East-bound traffic will turn right or left onto the Loop Road, with a drop-off area located north of the Arrival Plaza where only limited vehicular access will be possible for special purposes such as emergencies or access for the disabled.

Loop Road – The Loop Road is intended to provide the primary circulation through the site, connecting to Fairgrounds Drive at two locations. The Plan proposes that the northerly connection include two left-turn lanes for southbound traffic on Fairgrounds Drive; this would require a revision to the Fairgrounds Drive/Redwood Parkway Improvement Project design.

North Loop Road is planned to provide two right turn lanes northbound onto Fairgrounds Drive, with no southbound left turning movement allowed. The intersection would be signal-controlled for all movements except for the southbound through traffic along Fairgrounds Drive, which would be a free-flow movement, with no change to access into Six Flags Discovery Kingdom. Additionally, the direct access from the Courtyard by Marriott Hotel onto Fairgrounds Drive would need to be modified to prohibit outbound left turns. All other turning movements would remain.

The South Loop Road intersection connection is located between the Entertainment Commercial (EC) parcel and the southerly parking area. This intersection would be signalized and would serve as the main access for the EC uses (although all trips to the site would have the option of using other intersections). At the planned intersection location, Fairgrounds Drive currently includes two travel lanes; in the future, the STA project is expected to provide a four-lane cross-section that includes two southbound lanes and two northbound lanes.

The Loop Road has a different design to the north and the south of the Entry Road. To the north, the configuration would consist of four 11-foot through lanes with a center median/left-turn lane and bike lane on both sides. This design would serve as the “urban street” function desired for the adjacent Entertainment Mixed Use (EMU) and Fair uses. Along the EMU Parcel 6 frontage, north of the Exposition Hall Arrival Plaza, the north side of the street would include a passenger drop-off lane.

To the south of the Entry Road, the proposed South Loop Road configuration consists of four 11-foot lanes, a center median/left turn lane, and a multi-purpose path on both sides. This section is expected to serve higher traffic volumes generated by a combination of Fair and EMU trips, along with most or all of the EC trips. The multi-purpose paths would provide pedestrian promenades connecting from the southern parking facilities to destinations located at the Fair and Public Entertainment Core.

Sage-Loop Connector Road – This short north-south roadway will provide access to the site from Sage Street, connect to the North Loop Road, and provide access to the Transit/North Parking Center, adjacent EMU parking areas, and North Fair parking lot. The street section is planned to be three 11-foot lanes (one in each direction and a center two-way left-turn lane), with bike lanes. As this roadway will provide the most direct route between points north and the northerly fair parking lots, it will be a desirable route, potentially overloading the single southbound left-turn lane on Fairgrounds Drive at Sage Street. Therefore, on peak-attendance days, such as the County Fair weekend, access management may be needed on Fairgrounds Drive to distribute traffic appropriately to the Sage-Loop Connector Road entrance; the North Loop Road entrance, at which the southbound left-turn capacity is twice that at Sage Street; and the Main Entry Road entrance.¹

In-Tract Minor Roads and Driveways

Other vehicular roadways would provide circulation within individual land use parcels. These routes will be determined at a later stage of the site development process, but are expected to include:

- **Loop-Main Entry Connector**– The Phase 1 Illustrative Concept (Figure 4.11) indicates an in-tract (within the parcel) driveway from the Entry Road into the two large EMU parcels #6 and #7, connecting to the North Loop Road. This minor connector may also function as a storm drainage easement (see Chapter Six).
- **Perimeter Road** – The northerly and southerly parking lots would be connected via a perimeter road to allow maximum parking and circulation efficiency. The perimeter roadway would likely have a minimal two-lane cross-section between the lots, and would run along the northern and eastern edges of the Fair parcels, providing intra-lot circulation and travel between designated parking areas. The roadway will be gated at both ends of the Fairgrounds to provide for security.

Traffic Calming Features

The Plan Area roadways and intersections would be designed for slow speeds to enhance the pedestrian environment and promote safety. Proposed traffic calming features include:

- Narrow (11-foot) lane widths
- Curved roadway alignments and short blocks (northern portion)
- Traffic control at primary intersections (all-way stops)
- Curb extensions at intersections
- High-visibility crosswalks
- Raised intersection design at certain intersections, including the intersection of the Entry Road and the Loop Road

Speed humps, while not currently envisioned, may be considered for the long straight section of South Loop Road, should speeds become a problem in this section.

5.2.2 Pedestrian Circulation

Figure 5.10 shows the pedestrian circulation plan. The Plan provides a comprehensive network of pedestrian facilities, including sidewalks, multi-use paths, and controlled crossings to promote walking to the site and within the site. All of the primary and secondary roadways on the site

¹ Note that Sage Street/Fairgrounds Drive is not currently signalized, but is programmed to be signalized by the City of Vallejo.

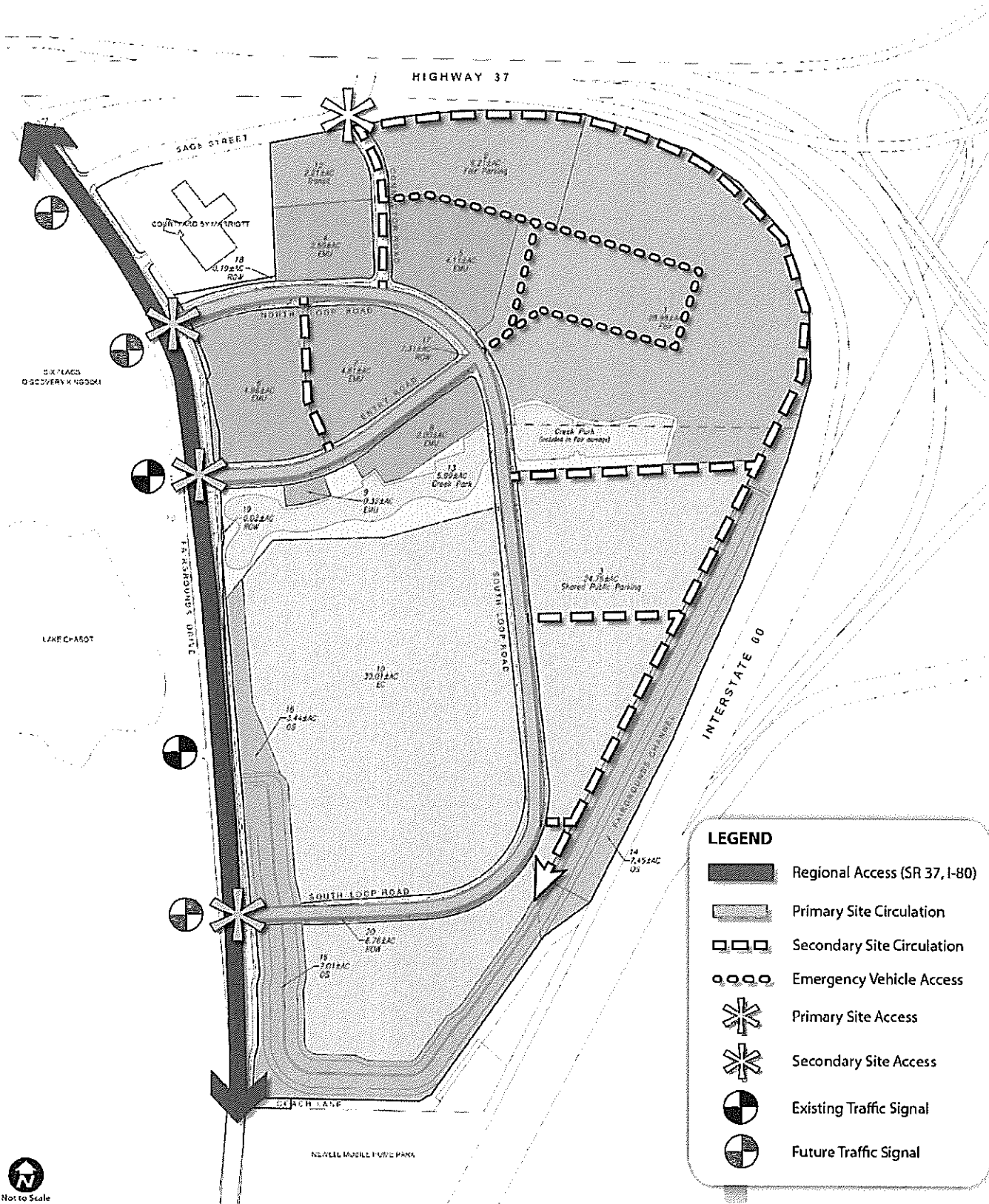


Figure 5.1: Vehicular Circulation

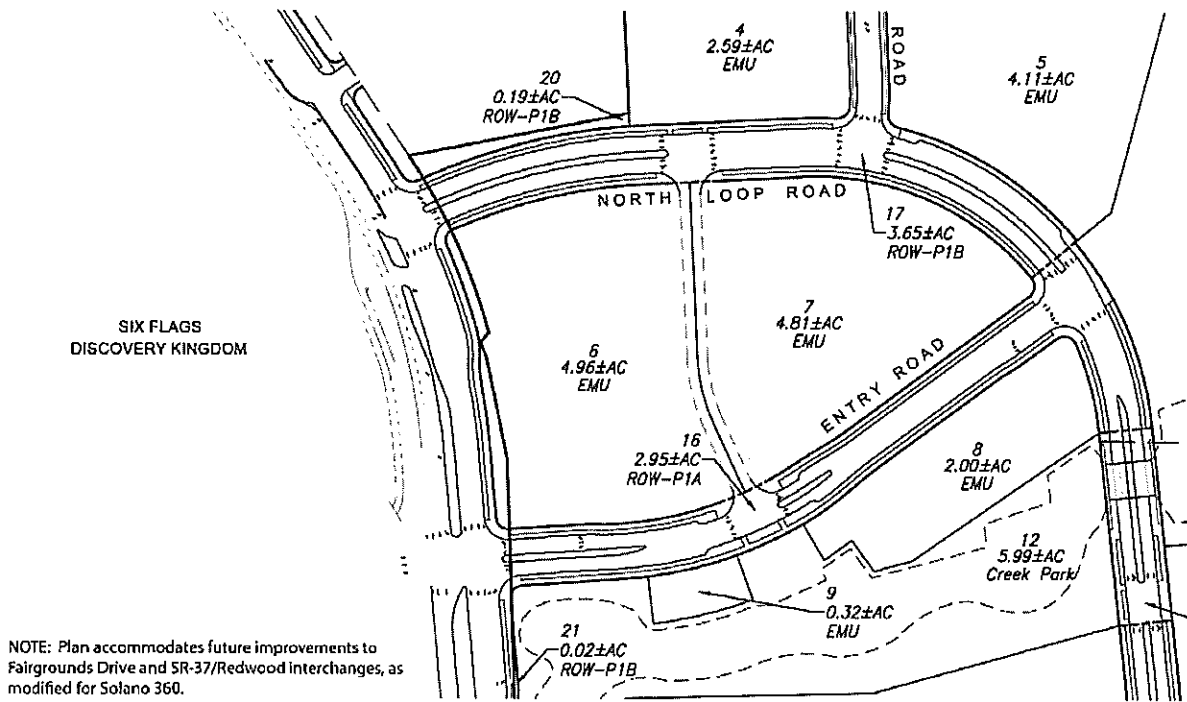
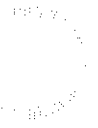


Figure 5.2: North Area Circulation

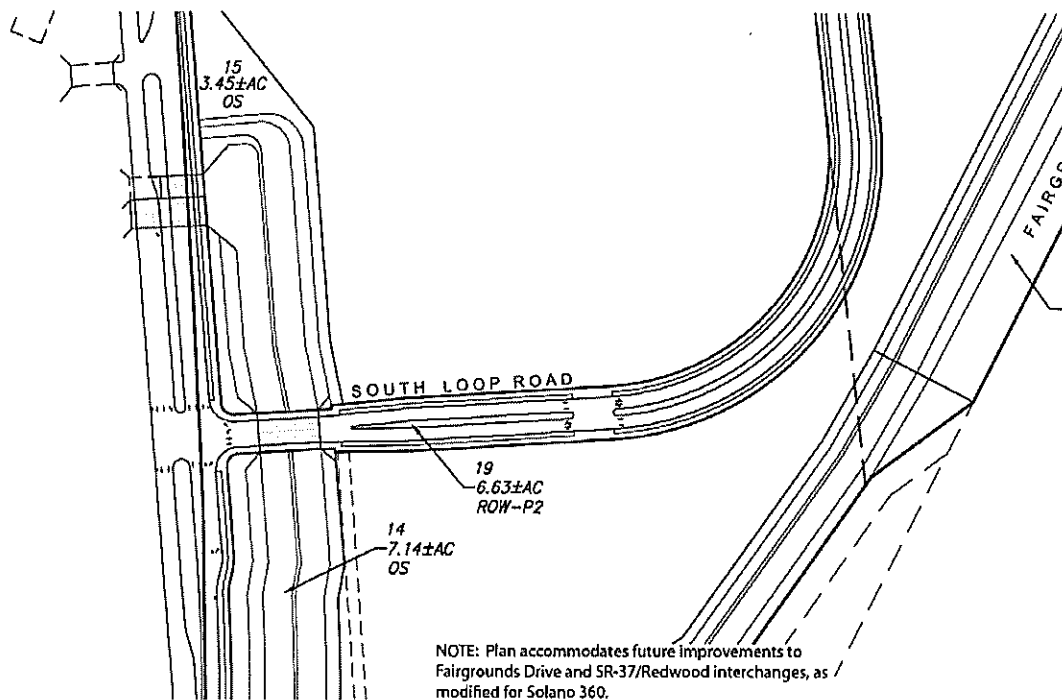


Figure 5.3: South Area Circulation

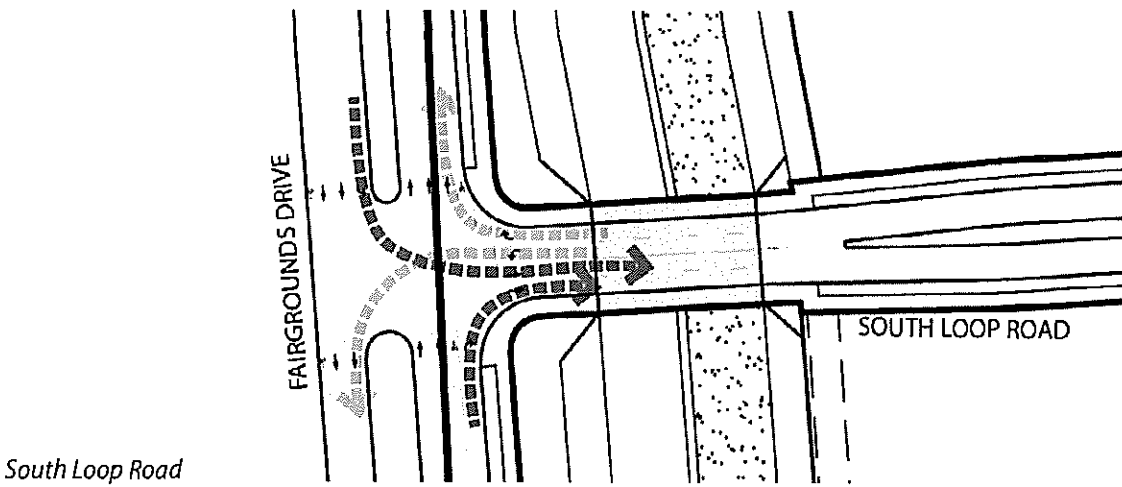
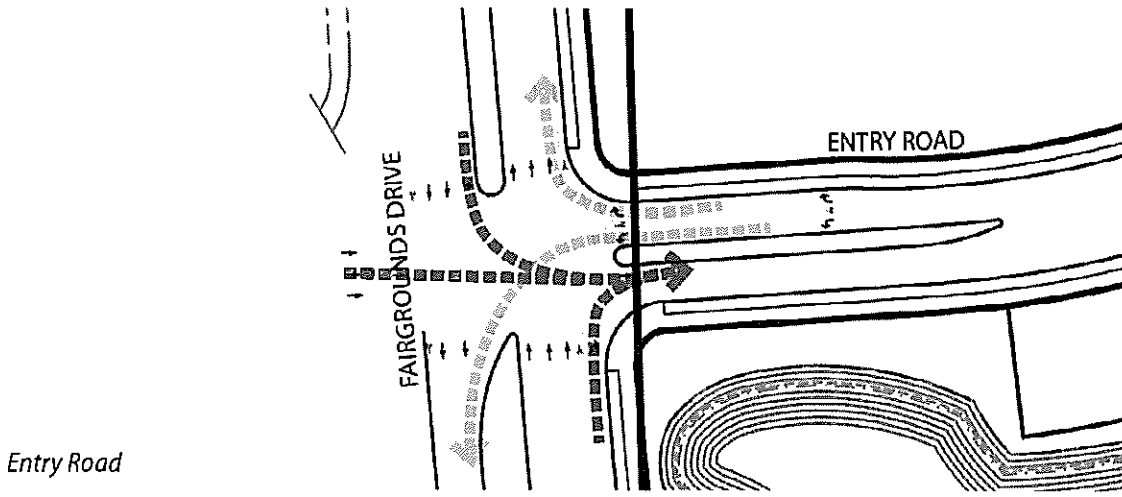
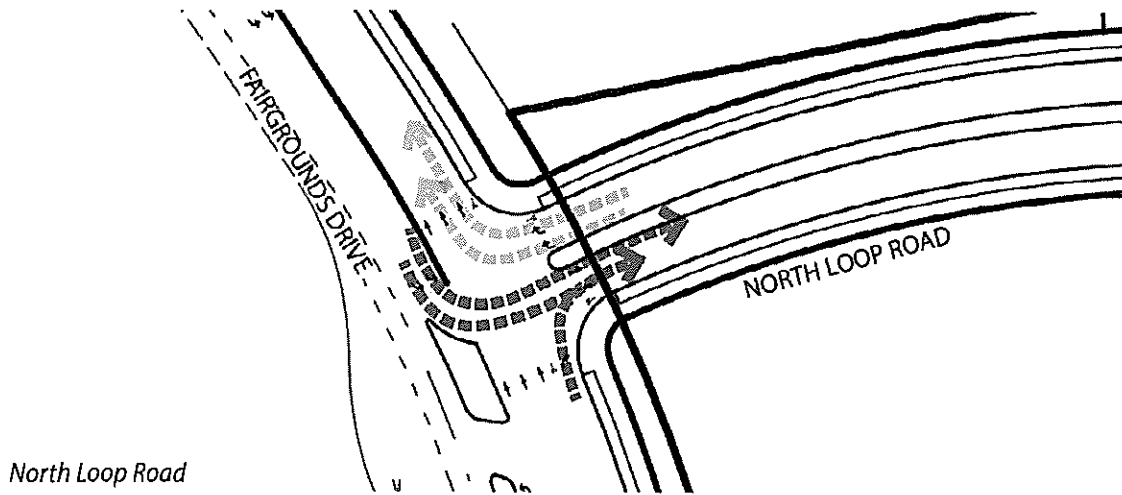
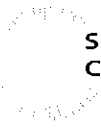


Figure 5.4: Turning Movements at Fairgrounds Drive Intersections

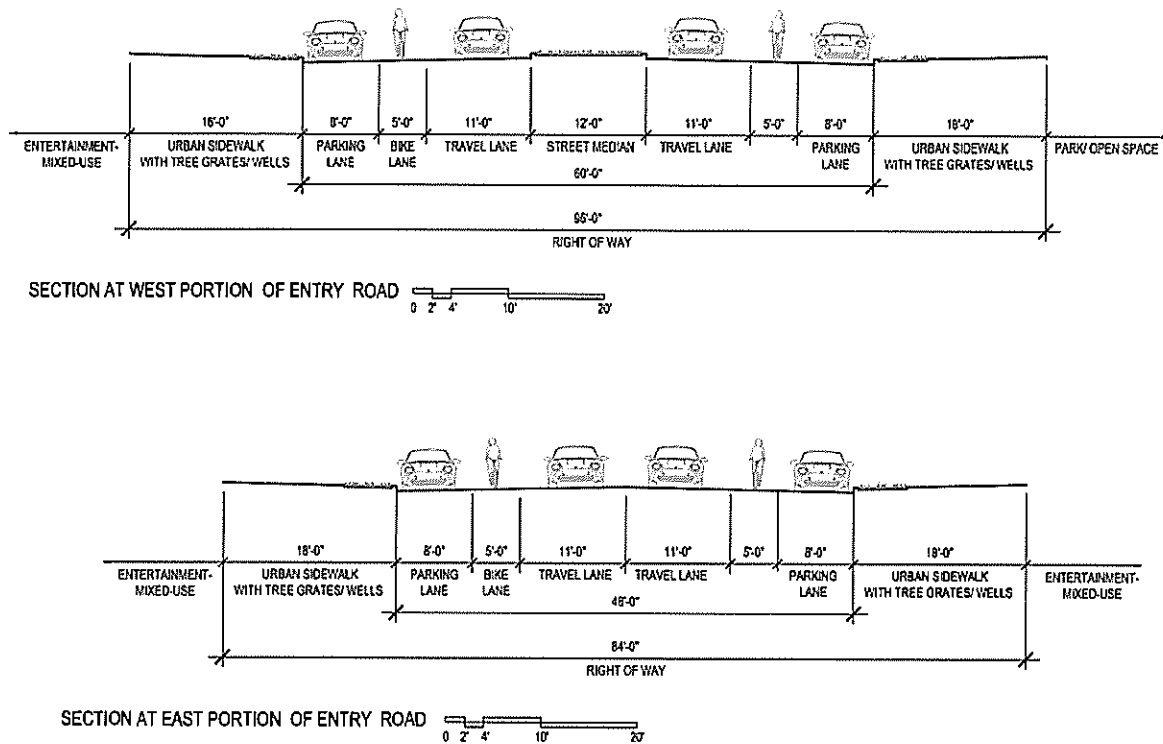


Figure 5.5: Entry Road Sections

have 10-foot minimum wide sidewalks or multi-use paths on both sides.

Controlled crossings (i.e. regulated by a traffic signal) will be provided across Fairgrounds Drive at the Main Entry Road intersection and at the Sage Street intersection (when signalized). High visibility crosswalks will be provided on all approaches at the on-site intersections, including Main Entry Road/Loop Road, Loop Road/Sage-Loop Connector, and Main Entry Road/Main Entry-Loop Connector.

The Plan proposes a continuous trail loop around the southern area including a along the west and southern boundaries of the Plan Area along Fairgrounds Channel, along the Fairground Drive buffer, and through Creek Park. Along with other subsequent permits, the trail along Fairgrounds Channel will need to be confirmed by relevant agencies.

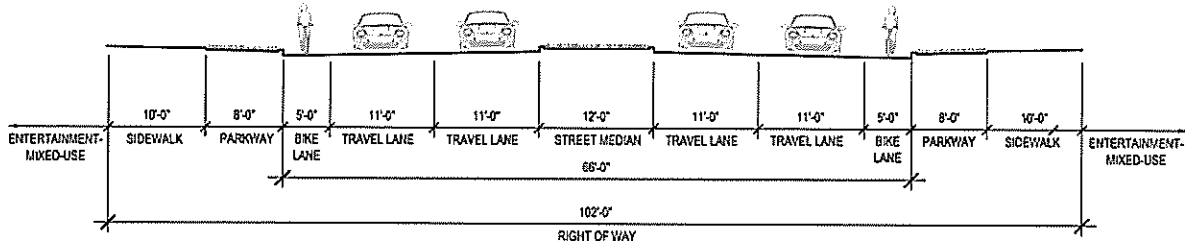
5.2.3 Bicycle Circulation

Figure 5.11 shows the bicycle circulation plan. Fairgrounds Drive currently has bicycle lanes along most of the project frontage, although there is a gap in the lanes as shown on Figure 5.3. The STA project will provide continuous bike lanes on Fairgrounds Drive between SR-37 and Redwood Parkway. The Plan provides bike lanes or a multi-purpose path on all the primary roadways. Secure bicycle parking areas will be provided on the Fairgrounds site and on all EMU parcels as they develop, and on the EC site. The Transit/North Parking Center will also provide a secure bicycle parking area, and may include other bicycle amenities such as a bicycle repair facility.

5.3 PARKING

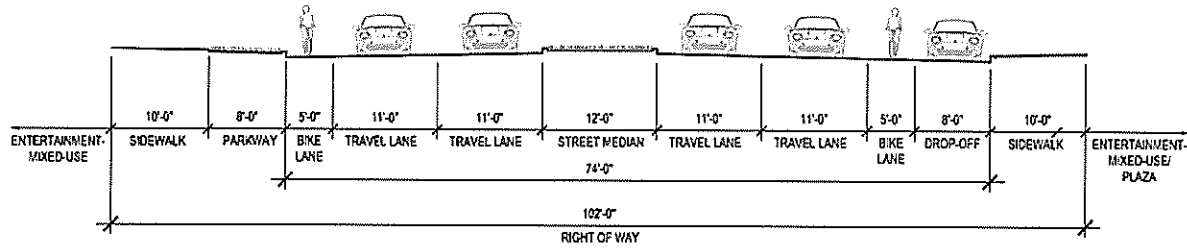
5.3.1 Plan Area Parking

Figures 5.12 to 14 illustrate the distribution and phasing of parking within the Plan Area, and



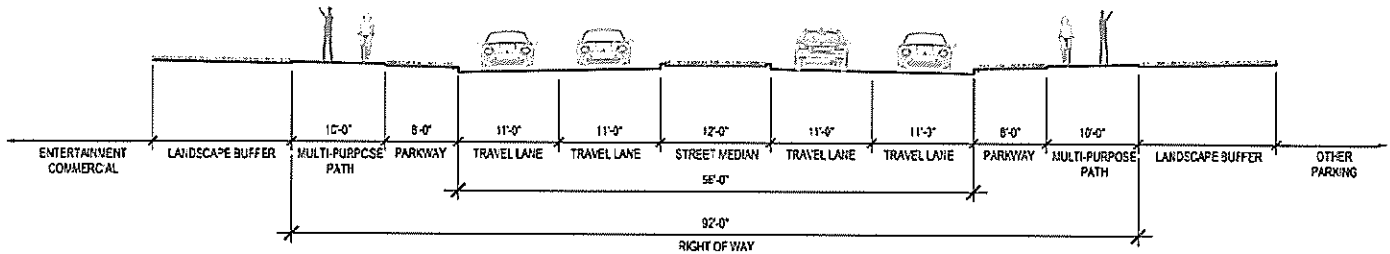
SECTION AT NORTH LOOP ROAD

Figure 5.6: North Loop Road Section



SECTION AT NORTH LOOP ROAD / FAIRGROUNDS DROP-OFF

Figure 5.7: North Loop Road Section at Drop-Off



SECTION AT SOUTH LOOP ROAD

Figure 5.8: South Loop Road Section

Table 5.1 shows the parking totals by phase and land use type.

In Phase 1, parking facilities would include the existing surface lots and interim parking within graded pads and other areas that are not yet fully developed with buildings or other uses. Phase 2 parking would be provided by surface lots and the Transit/North Parking Center's parking structure. In Phase 3, additional structured parking is proposed to meet parking demand for the increased development density (see discussion below).

The parking supply is calculated based on the projected demand for the various uses at each phase of buildout. These parking provisions are subject to a Parking Operations Management Plan to be prepared by the County and parking agreements between the County and Six Flags Discovery Kingdom.

The Plan provides parking as follows.

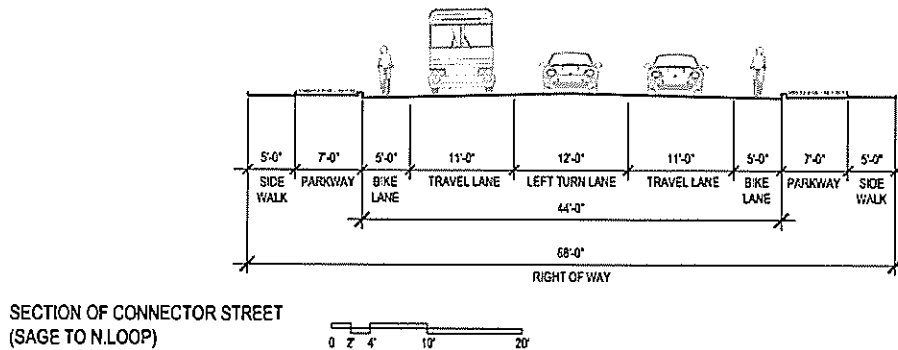


Figure 5.9: Sage-Loop Connector Section

Public Development Areas Parking

- Solano County Fairgrounds currently requires parking at levels that vary through the year, peaking during the County Fair week at about 3,500 spaces accommodated within on-site lots, most of which are in unpaved parking areas.
- The Plan proposes Fairgrounds parking within the North Fair Parking lots and additional parking, service and loading areas along the northern and eastern perimeter of the Fair area. In Phase 1, the Fair would also utilize an interim parking area of approximately seven acres located at the northern portion of the existing golf course. In Phases 2 and 3, the Fair would also utilize Shared Public Parking (see below).
- Shared Public Parking would be used by the Fair and others under the terms of the County’s Parking Operations Management Plan and parking agreements between the County and Six Flags Discovery Kingdom. This 24.7-acre area would be developed as surface parking in Phase 2, replacing the existing golf course and Phase 1 interim parking. In Phase 3, approximately five acres of the surface parking lot is intended to be converted to a multi-level parking structure.
- The Transit/North Parking Center would provide interim surface parking in Phase 1 and, starting in Phase 2, a parking structure.
- On-street parking would be available along both sides of the Entry Road; these parking lanes can be converted to travel lanes to accommodate heavier traffic during peak events.

Private Development Areas Parking

- Entertainment Mixed Use Development should be supplied with parking at five spaces per thousand square feet for each phase as shown in Table 5.1. Parking would consist of surface parking within parcel areas. In Phase 3, a parking structure is planned to allow intensification of development. The parking structure may be located in any EMU parcel that is not adjacent to the Creek Park.
- This phased increase in parking is intended to provide for buildout and intensification of uses within the EMU area, with the goal of providing retail-type parking supplies.
- Entertainment Commercial Development would include surface parking within the EC parcel for Phase 2, transitioning to a combination of on-site surface parking within the EC parcel and joint use of Shared Public Parking facilities in Phase 3 (either within the proposed parking structure or in surface lots) as shown in Table 5.1. These parking

requirements are based on surveys of similar theme park uses indicating that such parking typically accounts for approximately 40 percent of the site's acreage.

5.3.2 Structured Parking

Structured parking is proposed as follows:

- A three-level parking structure at the Transit/North Parking Center, accommodating approximately 380 parking stalls (assume 300' x 135' footprint, three levels, 320 square feet/stall).
- A four-level parking structure in the southern end of the Plan Area within the Shared Public Parking area, accommodating approximately 2,500 parking stalls (assume 400' x 500' footprint, four levels, 320 square feet/stall).
- A three-level parking structure integrated into the Entertainment-Mixed Use area, accommodating approximately 1,000 parking stalls (assume 300' x 360' footprint, three levels, 320 square feet /stall). Figure 5.14 Land Use and Parking indicates this parking structure located at EMU Parcel 6 near the intersection of Fairgrounds Drive and the North Loop Road; however, it could be located within any EMU parcel(s) located along North Loop Road (Parcels 4, 5, 6 or 7) but should not be located in Parcels 8 or 9 or adjacent to the Creek Park. The parking structure would allow for intensification of EMU development from 0.2 to 0.4 FAR and could contain ground-level development along North Loop Road.

5.3.3 Parking for Nearby Major Entertainment Areas

Parking facilities proposed for the Plan Area are designed to support the viability of the overall entertainment district, including the sustained operations and potential growth of Six Flags Discovery Kingdom. The usage and financial terms for parking will be subject to a Parking Operations Management Plan to be prepared by the County and by parking agreements between the County and Six Flags Discovery Kingdom.

In addition to parking within Six Flag Discovery Kingdom's own property, parking is available, subject to a Parking Operations Management Plan and parking agreements between the County and Six Flags Discovery Kingdom, within the Solano360 Plan Area through each phase, as follows:

- Phases 1a and 1b: Existing surface parking and additional overflow parking within undeveloped portions of the site.
- Phase 2: Shared Public Parking (surface parking)
- Phase 3: Shared Public Parking (surface parking and South Parking Garage)

In addition to these parking facilities, the project proposes shuttle connections linking parking facilities, Six Flags Discovery Kingdom, and Plan Area destinations (see Figure 5.15).

5.3.4 Parking Operations Management Plan

In order to maximize the flexibility of parking throughout the build-out of the project, the County should develop a Parking Operations Management Plan to provide a mechanism for coordinating events, facilitating joint-use parking, and addressing offset of scheduling to make full and efficient use of the planned onsite parking facilities.

A parking management plan would include the following elements:

- A cooperative use agreement outlining the parking requirements for each use/operator, including an annual schedule with days and times of day for minimum parking requirements.

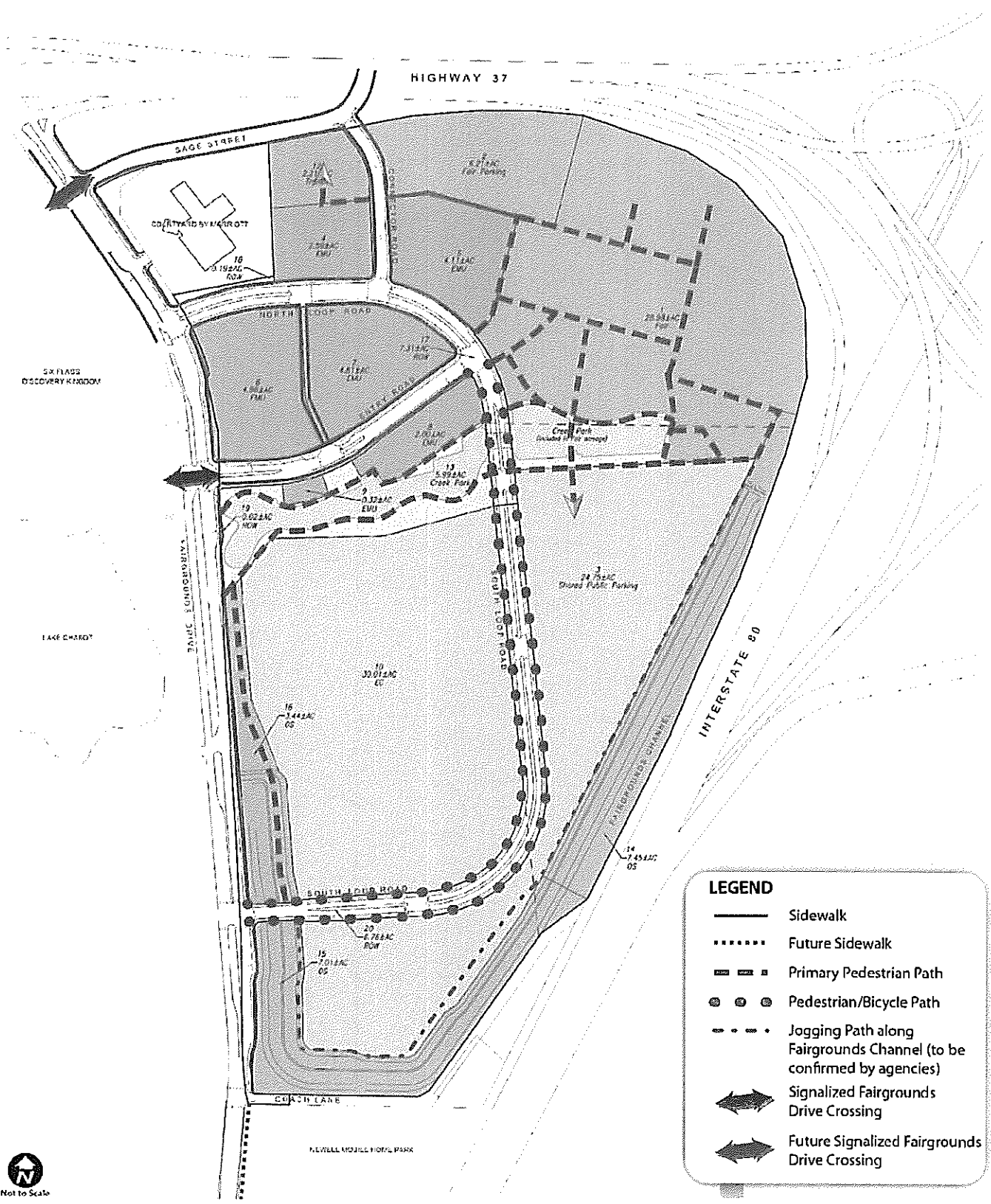


Figure 5.10: Pedestrian Circulation

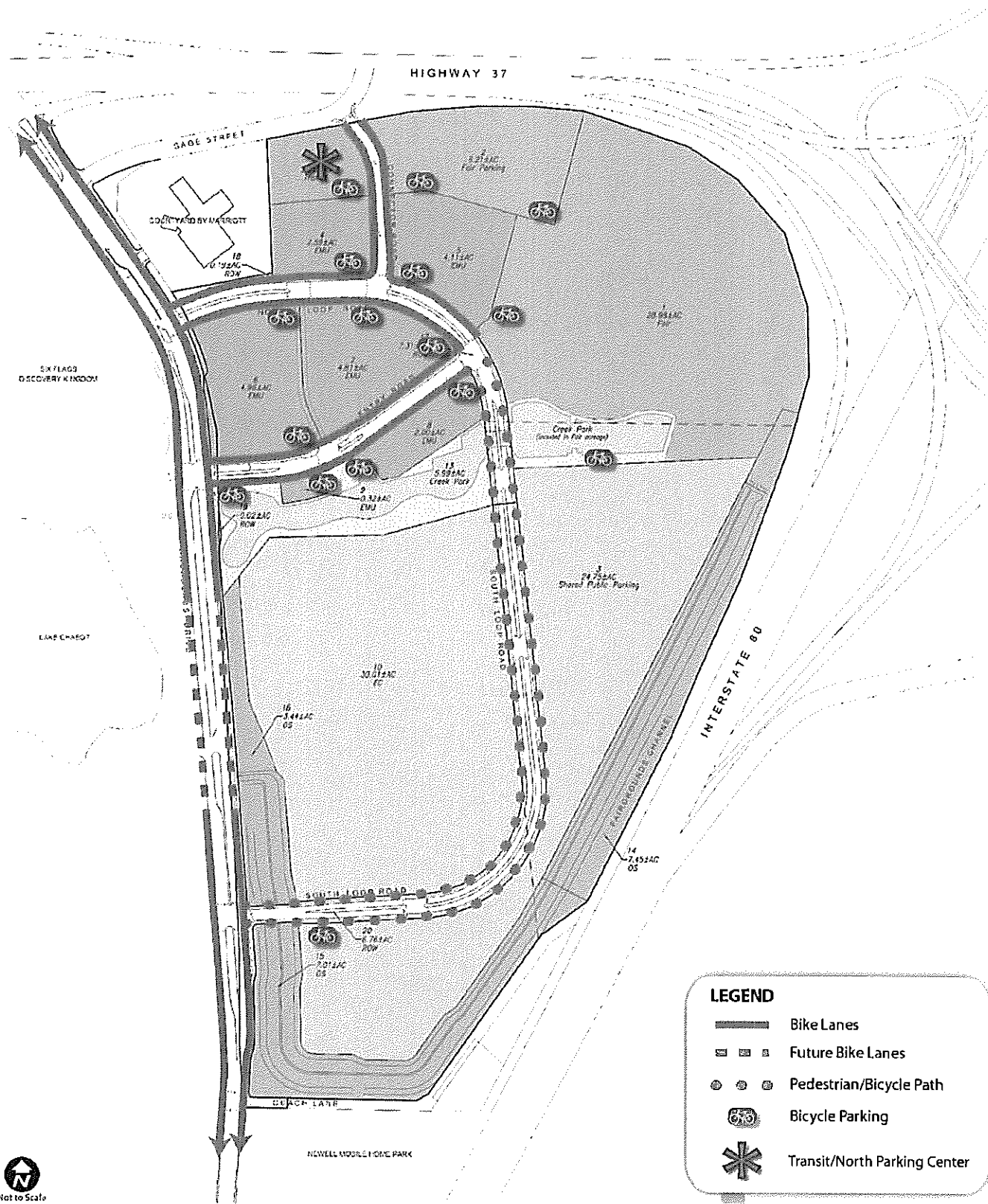


Figure 5.11: Bicycle Circulation



- Designation of parking management staff for each user/operator, along with County and City liaisons.
- A process by which the parking management staff and liaisons would meet and update the parking management plan on a regular basis, including assessing past performance of the plan and adjusting the plan to improve operation going forward.
- Provision of changeable message signs on-site and at the project entrances to direct drivers to the appropriate parking area.
- Regular monitoring of parking usage and traffic approach and departure patterns on peak days, to allow fine-tuning of site wayfinding, traffic management, and parking management strategies.

Table 5.1: Parking by Phase

LAND USES	Summary Description of Phases															
	EXISTING		PHASE 1A			PHASE 1B			PHASE 2			PHASE 3				
	Parking Spaces ^{1,10}	New Acres	New Parking Spaces ¹	New Acres	Cumulative Parking Count	New Acres	New Parking Spaces ¹	New Acres	Cumulative Parking Count	New Acres	New Parking Spaces ¹	New Acres	Cumulative Parking Count	New Acres	New Parking Spaces ¹	Cumulative Parking Count
Public Development Areas																
Public Parking	1,300		1,300		1,300											
South Fair Interim Parking (north portion of golf course)		7.0	875		875											
North Fair Interim Parking (undeveloped transit/road/EMU parcels & existing admin pkg)			1,750		1,375											
North Fair Parking ^{9,11}						6.2	775		775							775
Shared Public Parking Structure ⁵																
Shared Public Parking Surface ⁶																
Transit Center-Bus Docking																
Transit Center - Parking Structure ³																
Entry Road onstreet parking ⁸		2.3	73		73											380
Subtotal - Public Areas¹⁰			3,998		3,523				3,755				3,828			5,708
Private Development Areas^{2,3}																
Entertainment Mixed Use (0.2 FAR)		9.8	427		427											
Entertainment Mixed Use (0.4 FAR)							2.0	174								
Entertainment Mixed Use Parking Structure ⁷																
Entertainment Commercial - venue area																
Entertainment Commercial - parking area ⁴																
Entertainment Commercial - Shared Public Parking use ⁶																
Subtotal Private Development			427		601				1,805				2,406			2,554
TOTALS			4,425		4,124				5,560				6,234			8,262

Table Notes:

- Surface parking assumes 125 cars/acre.
- Parking demand for EMU uses assumed to be 5 spaces/1000 sf.
- Parking demand for EC uses assumed to be similar to typical theme park configurations.
- In Phase 2, 40% of Entertainment Commercial (EC) parcel is used for parking (13 acres); in Phase 3, EC parking is provided by 6 acres within the EC parcel and by the South Parking Garage located in Shared Public Parking area.
- In Phase 2, Transit Center provides approx. 380 parking spaces (800x135' footprint; 3 levels; 320 sf/stall).
- In Phase 2, "Shared Public Parking" assumes development of 27.4 acres of surface parking (2,600 spaces). In Phase 3, five acres are used for South Parking Garage (approx. 2,500 parking spaces with 400x500' footprint; 4 levels; 320 sf/stall), with 1,975 remaining surface spaces. Shared Public Parking is shared by Fair, expanded EC (in Phase 3) and others according to County's Parking Operations Management Plan and other parking agreements.
- In Phase 3, EMU parking includes parking structure (approx. 1,000 parking spaces with 300x360' footprint; 3 levels; 320 sf/stall); 630 surface spaces; and onstreet spaces for total of approx. 1,650 spaces (5 spaces/1000 sf).
- Parking lanes will be available along the Entry Road except when needed as travel lanes for peak events.
- In Phase 2, added North Fair Parking requires the demolition of Admin and County buildings; admin space would be in a portable at rear of fair site.
- In Phase 2, added North Fair Parking requires the demolition of Admin and County buildings; admin space would be in a portable at rear of fair site.
- Parking totals do not include an estimated 4 acres of service/ employee parking space at the back of the fairgrounds.

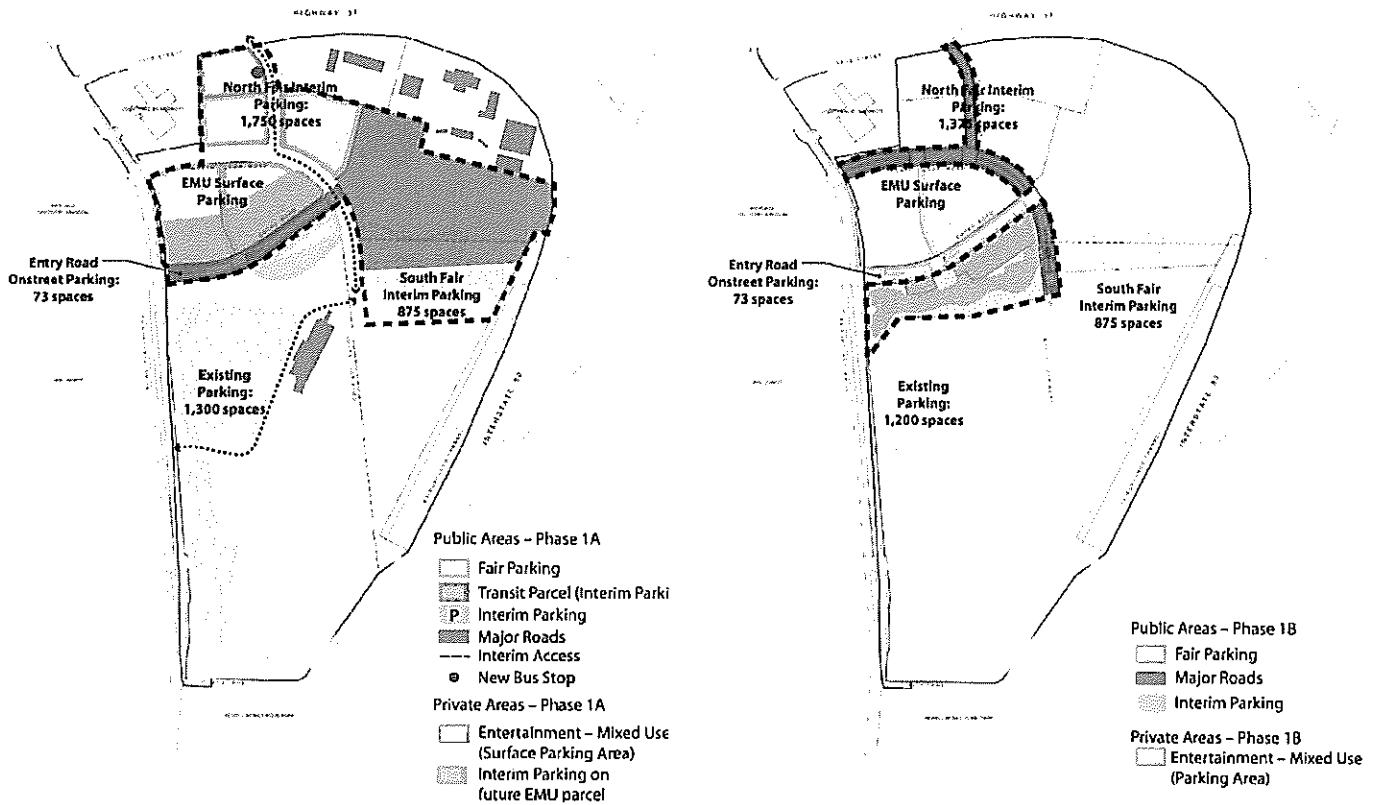


Figure 5.12: Phase 1 Parking Facilities

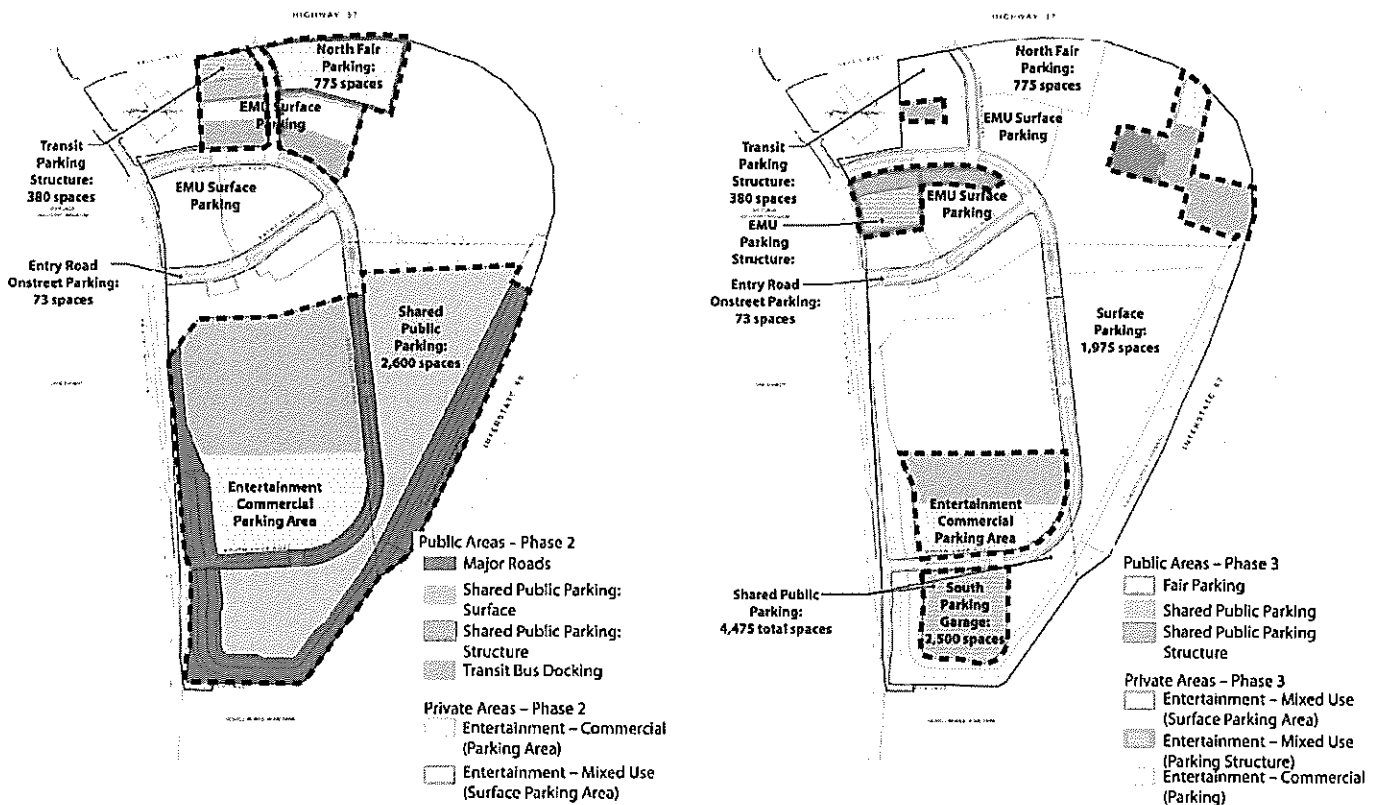


Figure 5.13: Phase 2 & 3 Parking Facilities

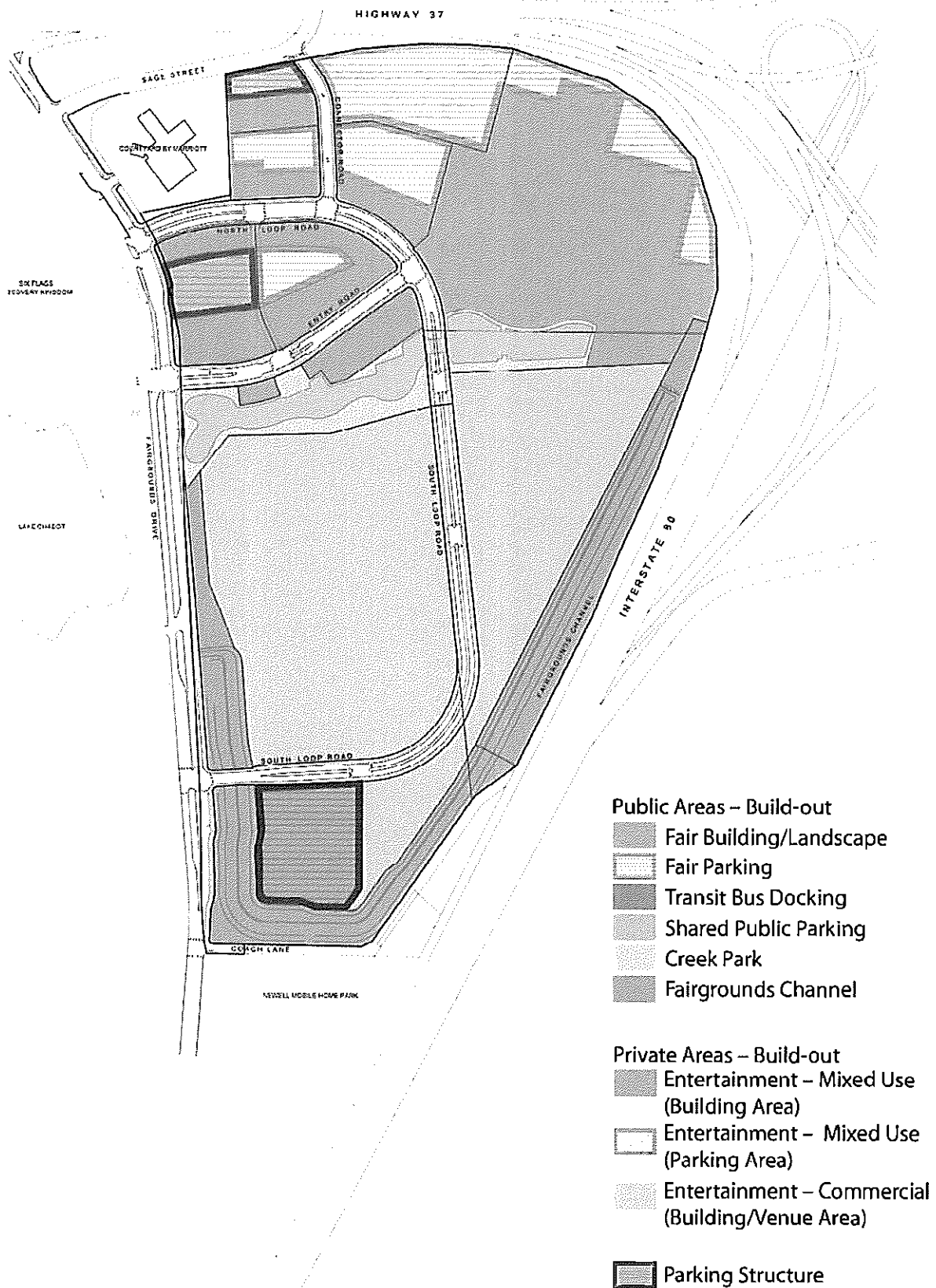


Figure 5.14: Land Use and Parking



5.4 TRANSIT AND ONSITE SHUTTLE

The Solano360 Vision Report included a 2.5-acre Transit/North Parking Center that incorporates a structured parking facility (scheduled for Phase 2). The Plan locates this use in the northern portion of the site, with access from Sage Street and North Loop Road.

Public transit is promoted as a viable transportation mode choice for those traveling to the Plan site for employment or entertainment, for the following reasons:

- The Plan land uses, along with Six Flags Discovery Kingdom across the street, will generate high traffic volumes on summer weekends, as well as on summer weekdays and to a lesser extent on non-summer weekends and weekdays. Transit use can reduce passenger vehicular traffic approaching and departing the site;
- Several transit routes operate in the Plan vicinity and in greater Vallejo, making transit a reasonable option for a good portion of the potential employment base and visitor market;
- As regional travel demand and corresponding congestion grows over time, transit may become a more desirable choice for travelers, with the growing system of High-Occupancy Vehicle (HOV) lanes and higher funding levels for sustainable transportation systems at the state and federal levels.

The Transit/North Parking Center would serve as a bus hub, with pedestrian and bicycle connections to the rest of the site. The Transit/North Parking Center is located south of Sage Street and would be accessed directly from the Sage – Loop Connector Road. Potential bus circulation routes are shown in Figure 5.15: Transit and Shuttle Routes. Prior to the construction of the Transit Center (expected in Phase 2), bus service can be accommodated with a stop on the Entry Road or the North Loop Road.

The Transit Center could also serve an on-site shuttle that could potentially be coordinated with Six Flags Discovery Kingdom operations to serve both sites. Figure 5.15 indicates possible routes for onsite shuttles that could pick up passengers at parking facilities and deliver them to destinations within the Plan Area and Six Flags Discovery Kingdom.

5.5 OFFSITE TRANSPORTATION IMPROVEMENTS

The off-site roadway and intersection improvements, by phase, are summarized below.

Phase 1

- Contribute funding, based on the Plan's proportional share of total future traffic, toward the provision of an exclusive right-turn lane at the intersection of Redwood Street/I-80 Westbound Ramps/Fairgrounds Drive; alternatively, contribute the same funds toward the provision of the ultimate improvements planned at this location as part of the Redwood Parkway/Fairgrounds Drive Improvement Project currently being planned by the STA.

Phase 2

- Contribute funding, based on the Plan's proportional share of total future traffic, toward the provision of improvements at the Fairgrounds Drive/SR 37 Ramps intersections, which are part of the Redwood Parkway/Fairgrounds Drive Improvement Project, including:
 - Widening the SR 37 Westbound Off-ramp to a four-lane cross section
 - Widening Fairgrounds Drive between the two ramp intersections to allow two

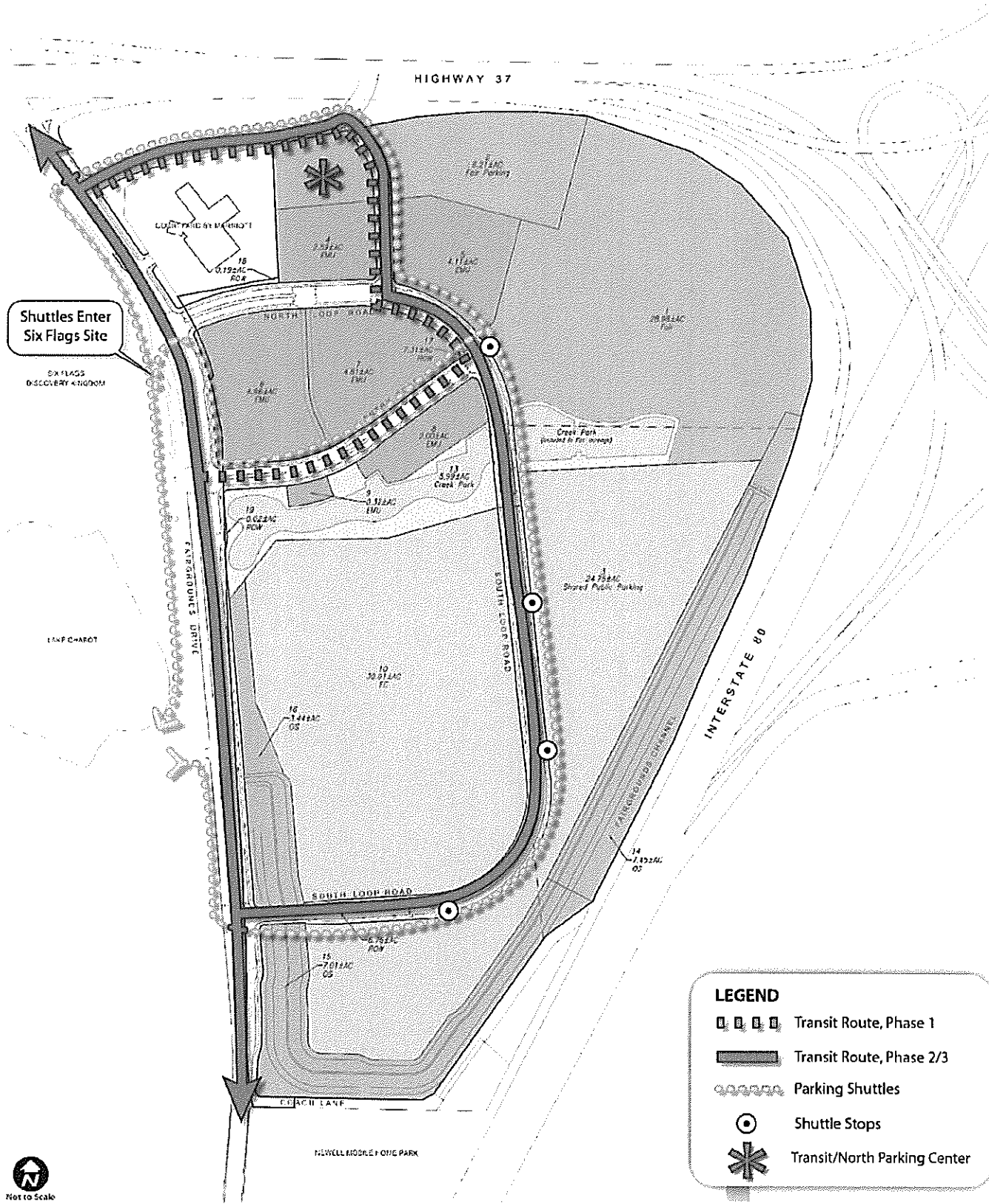


Figure 5.15: Transit and Shuttle Routes





full-length southbound left turns at the eastbound ramps intersection and one full-length northbound left turn at the westbound ramps intersection (including a transition on southbound Fairgrounds Drive north of the westbound ramps intersection)

- Widening the northbound approach to the eastbound ramps intersection to provide a third northbound lane that feeds directly into a right-turn lane onto the eastbound on-ramp
- Contribute funding, based on the Plan's proportional share of total future traffic, toward the construction of the re-configured I-80/Redwood Parkway interchange, which is part of the Redwood Parkway/Fairgrounds Drive Improvement Project.

Phase 3

- Contribute funding, based on the Plan's proportional share of total future traffic, toward the widening of Fairgrounds Drive along the Plan frontage, which is part of the Redwood Parkway/Fairgrounds Drive Improvement Project. Specifically, the improvements include widening the northbound direction to two lanes, with a third northbound through lane added just south of the South Loop Road intersection, widening the southbound direction to two lanes; bicycle lanes on both sides, and sidewalk along the east side of the roadway.
- Modify the Fairgrounds Drive/SR 37 Eastbound Ramps intersection to add a second northbound right-turn lane feeding the eastbound on-ramp.

5.6 TRAVEL DEMAND MANAGEMENT

Travel Demand Management refers to strategies to reduce single-occupant vehicle use for trips to/from the site, which in turn reduces traffic congestion and parking demand. Both of these effects would significantly benefit the site. Traffic congestion on peak days could be a deterrent to visitation at all the uses on the site – the Fairgrounds, the Entertainment Mixed Use uses, and the Entertainment Commercial uses. Similarly, inability to find a parking space, or back-ups resulting from the parking being close to fully occupied, would also deter visitation, as well as exacerbate traffic congestion on Fairgrounds Drive. The following travel demand management strategies are therefore included in the Plan to minimize the traffic and parking generated by the site:

- Provision of a commuter check benefit to all employees of the Fair, EMU and EC uses, allowing them to use pre-tax dollars to purchase discounted bus passes;
- Designation of separate employee and visitor parking areas, and limitation of the number of parking spaces available to employees;
- Advertisement of transit options on the County Fair website and the websites of all users/operators in the Plan Area;
- Provision of brochures detailing transit options, bicycle routes, and on-site bicycle parking facilities, at all venues in the Plan Area;
- Provision of a use agreement for an off-site parking supply, with shuttle service, for use by site employees during the peak-use days, for example during the County Fair and other high-attendance events.
- Discounted tickets to events and venues for off-peak hour admission and/or parking.
- Advertised incentives for weekend use of the downtown Vallejo Ferry and Parking Garage, with direct shuttle service to the Plan Area.

5.7 EVENTS MANAGEMENT PROGRAM

A Fairgrounds Events Management Program is proposed as part of this Plan to allow an increased amount of Entertainment Mixed Use development in Phase 1 of the project, without incurring the need for offsite transportation improvements. The Events Management Program pertains to the Fair only and does not restrict private development uses within the project. The program will be developed and implemented by the Solano County Fair Association.

The Events Management Program is designed to reduce automobile trips at the summer weekend late morning peak hour, when the capacity of the existing offsite transportation infrastructure (especially the SR-37/Fairgrounds Drive interchange) would otherwise be exceeded, resulting in undesirable traffic congestion. The objective is to insure that automobile trips do not exceed 498 peak hour trips.

Under the Events Management Program, the Fair would stagger starting and ending times for activities held during the peak summer weekend hours, spreading them out over time in order to keep traffic impacts at a baseline condition. For example, a major summertime weekend event at the Exposition Hall could not be scheduled at the same time as an event at other Fair facilities.

The following measures apply to summer weekends, from May to October.

- When Banquet Seating, Assembly Seating, or Trade Show events with estimated attendance at 75% or higher occupancy are scheduled on weekend days starting by 1 PM, all other events on-site should have start times staggered by a minimum of two hours (later than the Exposition Hall event start time). End times for those events should also be staggered by at least two hours.
- When Banquet, Assembly or Trade Show events with estimated attendance from 50% - 75% occupancy are scheduled on weekend days starting by 1 PM, all other events on-site should have start times staggered by at least one hour (later than the Exposition Hall event start time). End times should also be staggered by at least one hour.
- Non-seated concert events with estimated attendance at 50% or higher occupancy should not be scheduled to start before 1 PM on weekend days.
- When non-seated concert events with estimated attendance below 50% are scheduled for weekend days starting by 1 PM, all other events should have start times staggered by at least two hours (later than the concert). End times should also be staggered by two hours.
- In addition to the above guidelines, when multiple venues including the Exposition Hall are scheduled on summer Saturdays and Sundays, all events should be staggered by a minimum of one hour.

hp color LaserJet 5550 printers



usage page (S/N JPFC62W07P)

1

Product Name: hp color LaserJet 5550
 Printer Name: hp color LaserJet 5550

Usage Totals (equivalent)

PRINTER Page Size	SIMPLEX		Units	DUPLEX			Mono	Color	TOTAL Total
	Mono	Color		Mono	Color	Units			
LETTER	159,705	285,478	1.0	1,380	3,404	2.0	162,465.0	292,286.0	454,751.0
LEGAL	448	864	1.3	0	0	2.6	582.4	1,123.2	1,705.6
A4	4,577	6,579	1.0	41	32	2.0	4,659.0	6,643.0	11,302.0
EXECUTIVE	0	0	0.8	0	0	1.6	0.0	0.0	0.0
11X17	216	1,190	2.0	0	0	4.0	432.0	2,380.0	2,812.0
A3	0	1	2.0	0	0	4.0	0.0	2.0	2.0
ENVELOPE #10	934	10	0.4	**	**	**	373.6	4.0	377.6
ENVELOPE MONARCH	0	0	0.3	**	**	**	0.0	0.0	0.0
ENVELOPE C5	0	1	0.6	**	**	**	0.0	0.6	0.6
ENVELOPE DL	0	0	0.4	**	**	**	0.0	0.0	0.0
B4(JIS)	0	23	1.5	0	0	3.0	0.0	34.5	34.5
B5(JIS)	0	0	0.7	0	0	1.4	0.0	0.0	0.0
ENVELOPE B5	0	2	0.7	**	**	**	0.0	1.4	1.4
CUSTOM	2	1	1.0	**	**	**	2.0	1.0	3.0
DPOSTCARD(JIS)	0	8	1.0	**	**	**	0.0	8.0	8.0
A5	0	0	0.5	**	**	**	0.0	0.0	0.0
8K	0	0	1.7	**	**	**	0.0	0.0	0.0
16K	0	0	0.8	**	**	**	0.0	0.0	0.0
EXECUTIVE(JIS)	0	0	1.1	**	**	**	0.0	0.0	0.0
LETTER ROTATED	7	0	1.0	0	0	2.0	7.0	0.0	7.0
A4 ROTATED	0	0	1.0	0	0	2.0	0.0	0.0	0.0
A6	0	0	1.0	**	**	**	0.0	0.0	0.0
B6(JIS)	0	0	1.0	**	**	**	0.0	0.0	0.0
8.5X13	0	3	1.1	**	**	**	0.0	3.3	3.3
STATEMENT	0	3	0.5	**	**	**	0.0	1.5	1.5
12X18	0	0	2.3	**	**	**	0.0	0.0	0.0
RA3	0	0	2.1	**	**	**	0.0	0.0	0.0
TOTAL PRINTER USAGE							168,521.0	302,488.5	471,009.5

Print Modes & Paper Path Usage (actual)

PRINT MODES USAGE Print Mode	Mono	Color	Total
AUTO SENSE MODE	169358	300260	469618
HEAVY MODE	0	0	0
TRANSPARENCY MODE	0	0	0
CARDSTOCK MODE	6	3	9
GLOSSY MODE	0	2	2
TOUGH PAPER MODE	0	0	0
ENVELOPE MODE	0	0	0
HEAVYGLOSSY MODE	0	0	0
LIGHT MODE	0	0	0
LABEL MODE	24	103	127
IMAGE MODE	0	0	0
EXTRA HEAVY MODE	0	0	0
HUMID/OHT MODE	0	0	0
HUMID/TOUGH MODE	0	0	0
TOTAL			469756

PAPER PATH USAGE Source	Count	Destination	Count
Envelope Feeder	0	Face Up	0
Manual Feed Tray	544	Face Down	469771
Tray 1	4633	External Bin	0
Tray 2	209411	Other	0
Tray 3	255183	TOTAL	469771
External Tray	0		
Other	0		
TOTAL	469771		

Historical Printer Coverage

■ Black	3.7%	■ Magenta	2.3%
■ Cyan	2.9%	■ Yellow	2.6%



CHAPTER SIX: PUBLIC INFRASTRUCTURE AND SERVICES

6.1 INTRODUCTION

The construction of onsite and offsite infrastructure improvements will be required to accommodate proposed development within the Plan Area. The Plan is intended to provide infrastructure and services that meet City standards and integrate with existing and planned facilities and connections, without diminishing services to existing residents or businesses within the City.

This chapter provides an overview of the major utility infrastructure improvements and the public services needed to serve full build-out of the Plan Area. Utilities addressed include storm drainage (including grading), potable and non-potable water, wastewater, electricity, natural gas, telecommunications, wireless communications, and waste management. Services include police and fire protection. (Transportation and parking infrastructure requirements are addressed in Chapter 5.)

The major public "backbone" infrastructure improvements are planned to provide services to the entire Plan Area including both public purpose and private purpose development.

The existing utilities within the fair concourse area will remain in-place, but will be connected to new "backbone" infrastructure along Sage-Loop Connector Road (water, sewer, storm, electric, gas, phone, cable). In addition, new utility stubs to the fair parcels will be provided along North and South Loop Road. The new Exposition Hall, for example, can connect to new utility stubs at the intersection of North Loop Road and Entry Road and/or to existing utilities within the concourse. Improvements to existing utilities within the concourse area are not included with this plan.

Major objectives for infrastructure include:

- Develop practical cost effective solutions that can be constructed in phases.
- Provide flexible options that can adapt to market conditions.
- Implement solutions that minimize impacts to the environment and maximize sustainability.
- Details relating to phasing and financing are included in Chapter Seven: Implementation.

The information in this chapter is informed by the Plan's conceptual site plans and may be subject to change as more detailed plans and specifications are developed as part of the design and development process.

6.2 STORM DRAINAGE AND GRADING

6.2.1 Background and Existing Conditions

Vallejo Sanitation and Flood Control District (VSFCD) provides public stormwater and flood control protection services for the Plan Area. The City of Vallejo administers stormwater quality protection through the San Francisco Bay Region Municipal Regional Stormwater National Pollution Discharge Elimination System (NPDES) permit (the permit is generally referred to as the MRP).

The Plan Area is located within a 4,600+ acre watershed identified in the VSFCD master plan as the Lake Chabot watershed. Drainage systems from approximately 3,300 acres of the watershed converge on the Fairgrounds property and discharge into the "Fairgrounds Channel". The channel

wraps around the eastern, southern and western periphery of the Plan Area as shown in Figure 6.1.

Approximately 62 of the 149± acres within the Plan Area are currently developed with impervious surfaces including buildings and paved parking lots. Approximately 33 additional acres are developed with a golf course and equestrian racetrack. The remaining 57 acres are generally undeveloped. Portions of the undeveloped land are utilized as unpaved parking facilities for fair events and for overflow parking from Six Flags Discovery Kingdom.

There are several components related to the existing drainage system within the Plan Area. Major components include four creeks (North, Central and South Rindler Creek and Blue Rock Springs), a manmade open channel (Fairgrounds Channel) and Lake Chabot. Other components include both public and private underground pipe systems. Public facilities are owned and maintained by the VSFC and are located both onsite and offsite. Private facilities are owned and maintained by the County / Fairgrounds Association and are generally located onsite. The private facilities are not well documented, but appear to discharge into public facilities.

An understanding of the historical creek systems is important as it relates to the drainage system and causes of flooding in the Plan Area. The existing public drainage facilities within the Plan Area revolve around the creek systems that collect water from the much larger watershed to the north, east and south of the site. The creeks are known as North Rindler Creek, Central Rindler Creek, South Rindler Creek and Blue Rock Springs. The creeks converge near the Plan Area and discharge into Fairgrounds Channel prior to discharging into Lake Chabot (see Figure 2.4: Existing Drainage Pattern and Figure 6.1: Stormwater Exhibit).

- North Rindler Creek has been diverted into an underground pipe system that crosses under SR-37 and discharges into Lake Chabot.
- Central Rindler Creek has been diverted into a combination of underground pipes, box culverts and manmade open channels. A pipe/culvert system crosses under I-80 near the north end of the existing racetrack and discharges into the Fairgrounds Channel (the channel flows south along the east property line; then flows west along the south property line; then flows north along the west property line and eventually crosses under Fairgrounds Drive and discharges into Lake Chabot).
- South Rindler Creek has been diverted into a combination of underground pipes, box culverts and manmade open channels. A pipe system crosses under I-80 near the south end of the racetrack and discharges into the Fairgrounds channel that connects to Lake Chabot as described above.
- Blue Rock Springs has been diverted into a combination of pipes, box culverts and manmade open channels. South of the Fairgrounds property it is an open channel that flows north through the Newell Mobile Home Park and discharges into the Fairgrounds Channel (near the middle of the southern Plan Area).

The Fairgrounds Channel is not capable of containing 100-year flood flows from the offsite creeks as identified in the VSFC Storm Drain Master Plan and illustrated on the FEMA flood map (Panel Number 06095C0440E). Flows overtop the banks and spill onto portions of the Fairgrounds property, Fairgrounds Drive, Coach Lane and into the Newell Mobile Home Park. Proposed improvements to alleviate flooding impacts within the Plan Area are described in Section 6.2.2 of this chapter.

6.2.2 Proposed Stormwater Collection and Conveyance

In conjunction with the preparation of this Plan, VSFC was consulted to determine existing system operation, capacity and future infrastructure needs. VSFC reported that:



- Drainage improvements within the Plan Area should be based on hydrology and hydraulic calculations documented in the VSFC Master Plan. The Master Plan contains an analysis of the upstream watershed including estimates of the peak 100-year flow rates for North, Central and South Rindler Creek and Blue Rock Springs.
- Drainage improvements within the Plan Area should lower the maximum hydraulic grade line (HGL) to elevation 86.0 (NAVD88) at the confluence of Blue Rock Springs and the Fairgrounds Channel. Lowering the HGL to elevation 86.0 would be consistent with recommendations in the VSFC Master Plan and would help alleviate flooding along Coach Lane and within the Newell Mobile Home Park area.

The main drainage infrastructure improvements for the project are designed to remove the Plan Area from the flood plain. The site is currently in the flood plain due to high offsite flows from the east and south as described in Section 6.2.1. Newell Mobile Home Park to the south of the Plan Area also has flooding problems due in part to the existing channel conditions. It is therefore desirable to also improve the flood conditions for the mobile home park. Proposed floodplain improvements include placing fill material at the northern end of the Plan Area to raise the ground elevation and enlarging the existing Fairgrounds Channel to contain flood flows at the southern end of the Plan Area.

Fairgrounds Channel

In order to remove the southern portion of the Plan Area (Phase 2) from the flood plain the existing Fairgrounds Channel will be widened and deepened, and the existing crossing will be improved under Fairgrounds Drive. The channel improvements will improve the flooding conditions for the mobile home park although additional improvements within the park may be required that will not be a part of this project (a separate VSFC Capital Improvement Project has been identified in the VSFC Master Plan for that work).

The proposed cross-section of Fairgrounds Channel includes an eight-foot wide, one-foot deep meandering low flow "notch"; a 40 to 50-foot wide, two-foot deep low flow channel section; and a 20 to 50-foot wide, five to seven-foot deep upper level bench. Side slopes are planned at a minimum of 3:1, but may be flattened to 4:1 where possible. Some slopes may be constructed at 2:1 if approved by VSFC. The overall depth of the channel varies from 5 to 15 feet. The channel ranges in overall width from 100± to 180± feet at the top of bank as shown on Figure 6.1. Refer to Chapter Four for design guidelines associated with the channel improvements.

As an option, particularly as it relates to Phase 1 development, the existing levee along the channel and adjacent to the racetrack may be utilized to protect portions of the Plan Area from flooding. In order for the levee to be taken into account in its "as-is" condition it would need to be able to be accredited / certified through the Army Corps of Engineers. As of the preparation of this Plan it is not known if the levee in its current "as-is" condition would meet the design standards. If the levee cannot meet the design standards it would need to be reconstructed. In any event, the levee would not be sufficient to remove the entire Plan Area from the floodplain and the above described improvements to Fairgrounds Channel would still be required. The levee in combination with channel improvements may also be considered as a viable solution, which could potentially avoid some jurisdictional wetland impacts, but at the same time would make the Plan Area a "levee protected community".

The design of the Fairgrounds Channel should be closely coordinated with VSFC and other permitting agencies.

Creek Park and Water Feature

Aside from the proposed channel improvements, the multi-purpose water feature within the Creek Park would be constructed onsite to manage and reduce peak discharges from the Plan

Area. It would effectively function as a recreational amenity providing a visual focus for the Creek Park and associated trails, a water quality Best Management Practice feature, a detention basin, and an irrigation source. The water feature would connect to an existing 84" underground pipe near the northwest corner of the Plan Area.

A primary objective of the water feature is to provide water quality benefits for the project and improve the water quality of site runoff before that water leaves the Plan Area and enters Lake Chabot. Some of the potential water quality measures that may be implemented include:

- Provide sufficient depth and volume of water to control temperature
- Construct a flow and depth control device where water leaves the onsite water feature and connects to a pipe that connects to Lake Chabot
- Line the water feature to avoid impacts associated with the existing high water table condition. The water feature should be designed to maintain a high quality of water and the liner should minimize any existing poor groundwater quality water from mixing with the water feature. In addition, the water feature should be designed to maintain a minimum depth of water and the liner should minimize fluctuations in water elevation due to changes in groundwater elevation.
- Construct sediment control systems
- Construct trash/debris collection systems
- Install aeration system in water feature
- Connect Plan Area irrigation systems to the water feature to circulate water
- Provide a make-up water system to maintain water volume due to evaporation losses

Other Improvements

Additional water quality improvements would be constructed throughout the Plan Area such as biotreatment facilities in order to meet the MRP requirements.

Onsite drainage systems within the streets would be designed in accordance with City and VFSCD standards. Underground pipes would be designed to accommodate 15-year storm events. Surface flow in the streets would be designed to accommodate 100-year storm events by directing runoff toward the water feature or Fairgrounds Channel.

New stormwater pipelines would be constructed in each backbone roadway providing service to each parcel. Existing public pipelines that traverse the Plan Area would be relocated as necessary to avoid conflicts with development. Existing pipes with the Fair concourse will remain in use.

Refer to Figure 6.1: Stormwater Exhibit for drainage calculations.

6.2.3 Grading and Soil Conditions

Preliminary geotechnical studies have determined that undocumented fills and soft compressible materials exist within the Plan Area. The "fill" is associated with the partial filling of Lake Chabot, which previously extended from its existing eastern bank to the west side of the existing Fairgrounds racetrack. The undocumented fill may require remedial grading and/or deep foundations. Additional geotechnical studies to determine the extent and condition of the fill and the required remediation should be required with future design phases of development.

Groundwater within the area is shallow; therefore, dewatering should be included in the construction process to allow for deep excavations.

The onsite water feature as described in Section 6.2.2 should be lined to avoid impacts associated

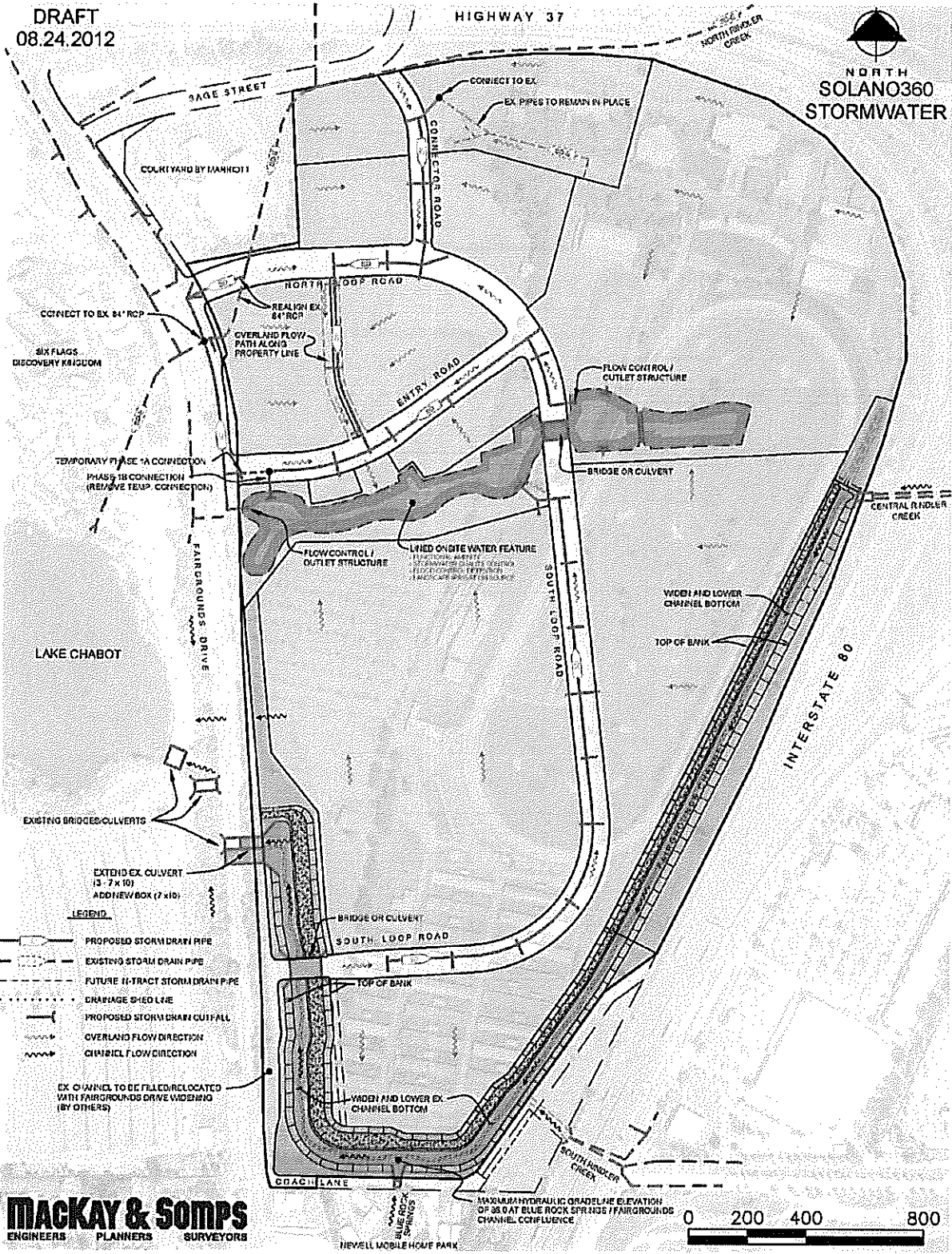


Figure 6.1: Stormwater Exhibit

with shallow groundwater.

6.2.4 Sustainable Practices for Storm Drainage

The onsite water feature would serve to “harvest runoff” to be used onsite for irrigation. Harvesting and reuse is consistent with Low Impact Development (LID) practices as specified in the San Francisco Bay Region Municipal Regional Stormwater National Pollution Discharge Elimination System permit (MRP).

The MRP requires “regulated projects”, of which Solano360 qualifies, to implement at least one “site design and stormwater treatment requirement” from a specific list of options (MRP Provision C.3.c(i)(2)(a)). As an example, option (v), as specified is to “minimize stormwater runoff by implementing one or more of the following site design measures”:

- Direct roof runoff into cisterns or rain barrels for reuse.
- Direct roof runoff onto vegetated areas.
- Direct runoff from sidewalks, walkways, and/or patios onto vegetated areas.
- Direct runoff from driveways and/or uncovered parking lots onto vegetated areas.
- Construct sidewalks, walkways, and/or patios with permeable surfaces.
- Construct driveways, bike lanes, and/or uncovered parking lots with permeable surfaces.

The Solano360 project would implement these measures to the extent practical and as required by the MRP. In particular the Solano360 onsite water feature would effectively function to harvest stormwater runoff for reuse similar to a cistern or rain barrel.

MRP Provision C.3.c(i)(1) requires “regulated projects” to implement “source control measures” as follows:

(a) Minimization of stormwater pollutants of concern in urban runoff through measures that may include plumbing of the following discharges to the sanitary sewer, subject to the local sanitary sewer agency’s authority and standards:

- Discharges from indoor floor mat/equipment/hood filter wash racks or covered outdoor wash racks for restaurants.
- Dumpster drips from covered trash, food waste and compactor enclosures.
- Discharges from covered outdoor wash areas for vehicles, equipment, and accessories.
- Swimming pool water, if discharge to onsite vegetated areas is not a feasible option.
- Fire sprinkler test water, if discharge to onsite vegetated areas is not a feasible option.

(b) Properly designed covers, drains, and storage precautions for outdoor material storage areas, loading docks, repair/maintenance bays, and fueling areas;

(c) Properly designed trash storage areas;

(d) Landscaping that minimizes irrigation and runoff, promotes surface infiltration, minimizes the use of pesticides and fertilizers, and incorporates other appropriate sustainable landscaping practices and programs such as Bay-Friendly Landscaping;

(e) Efficient irrigation systems; and

(f) Storm drain system stenciling or signage.



6.3 POTABLE AND NON-POTABLE WATER

6.3.1 Background and Existing Conditions

Public water service for the Plan Area is provided by the City of Vallejo and managed by the City of Vallejo Public Works Department – Water Division. Private systems located within the Fairgrounds property are owned, operated and maintained by the County. Under current conditions, adequate water service is provided. New construction in the Plan Area will result in increased demand for water service.

Existing public pipelines are located underneath Fairgrounds Drive, Sage Street and Coach Lane. There are two public water connections and meters to the site. The first is located at the north end of the site off of Sage Street. The second is located at the south end of the site off of Coach Lane. Private pipelines exist throughout the Fairgrounds property.

Two non-potable (raw) water systems exist within the vicinity of the Plan Area. One system is public and is owned, operated and maintained by the City of Vallejo. The pipeline is referred to by the City as the “Cal-Pac” line. The other system is privately operated and maintained by the County / Fairgrounds Association. The pipeline is referred to by the Fairgrounds Association as the “Lake Chabot” line.

The supply source for the Cal-Pac system is the North Bay Aqueduct (NBA). The NBA is also one of the City’s potable water supply sources. The Cal-Pac pipeline runs along the northern property line of the Fairgrounds site and currently delivers irrigation water to Blue Rock Springs golf course. The system has been utilized in the past to provide irrigation water to the Fairgrounds Joe Motara golf course and racetrack. A turnout in the system exists near the north end of the racetrack which could be utilized for future development on the Fairgrounds property.

The supply source for the Lake Chabot system is Lake Chabot itself and the tributary watershed area that drains to it. The Lake Chabot system currently provides water to the Joe Motara golf course through a lease agreement between the City of Vallejo and the Vallejo Golf Club.

The Lake Chabot pipeline extends from Lake Chabot to the Joe Motara golf course near the middle of the Fairgrounds property. A pump station is located on the southeast shore of Lake Chabot that delivers water into the pipeline.

6.3.2 Potable and Non-Potable Water Supply and Demand

The City of Vallejo will provide water to the site for domestic use, fire protection, irrigation purposes and make-up water for the onsite water feature. The total average water demand estimate for the Plan Area is approximately 97.7 million gallons per year including potable use, irrigation and evaporation losses from the water feature.

6.3.3 Water Treatment, Storage and Distribution

The City of Vallejo will provide treated water to the Plan Area. Non-potable sources may be available for irrigation and make-up water purposes. Depending on the non-potable water quality, it is possible that a chlorination and/or filtration system may be required.

A 12” public water pipe currently exists within Fairgrounds Drive. Public water connections also exist at the north end of the project off of Sage Street and at the southern end of the project off of Coach Lane.

Based on information provided by the City Water Division, development of the Plan Area may trigger the need for a new 24” pipeline in Fairgrounds Drive from Sage Street to Coach Lane. Water modeling should be required with future design stages of the development to determine if the new 24” pipe is required and when.

Water storage tanks exist throughout the City of Vallejo. Based on information provided by the City Water Division, development of the Plan Area is not expected to trigger the need for any additional storage.

New potable water and non-potable water pipelines should be constructed in each backbone roadway providing service to each parcel. The non-potable pipelines should be constructed in conformance with Title 22 recycled water standards (also sometimes referred to as "purple" pipe). Existing potable water pipelines with the Fair concourse area will remain in use.

6.3.4 Sustainable Practices for Potable and Non-Potable Water

Measures for water reduction, efficiency and conservation are recommended for development in the Plan Area as required by the California Green Building Code and recommended in the City of Vallejo's Urban Water Management Plan. This Plan includes guidelines that are intended to incorporate water-conserving measures in the design of new development and infrastructure (see Chapter Four).

Harvesting and reusing stormwater for irrigation along with non-potable water sources are intended to be used throughout the Plan Area. A non-potable water system is planned within each backbone roadway. The non-potable system should be installed in accordance with Title 22 standards for recycled water use in the event recycled water becomes available.

The VSFCDC prepared a wastewater treatment and reclaimed water feasibility study in 2003. The study concluded that constructing a city-wide reclaimed water system was not economically feasible. However, the VSFCDC has long term goals of implementing such a system sometime in the future. Benefits of the system include a reduction in potable water demand as well as reducing water quality impacts on the Bay associated with wastewater discharge.

6.4 WASTEWATER

6.4.1 Background and Existing Conditions

The wastewater conveyance and treatment system for the Plan Area is owned, operated and maintained by the VSFCDC. Public conveyance pipelines are located within public right-of-way or easements. Private systems are located within the Fairgrounds property and are operated and maintained by the County / Fairgrounds Association.

The VSFCDC treatment plant is permitted to treat up to 15 million gallons per day (mgd), but is currently only treating approximately 9 mgd. Based on discussions with the District Engineer there is sufficient capacity to treat wastewater generated by future development within the Plan Area.

Based on discussions with the District Engineer it is not likely that any offsite improvements will be required to convey wastewater to the treatment plant. The VSFCDC system model should be updated to verify pipeline capacity is sufficient during the design stage of the infrastructure.

6.4.2 Wastewater Generation & Treatment

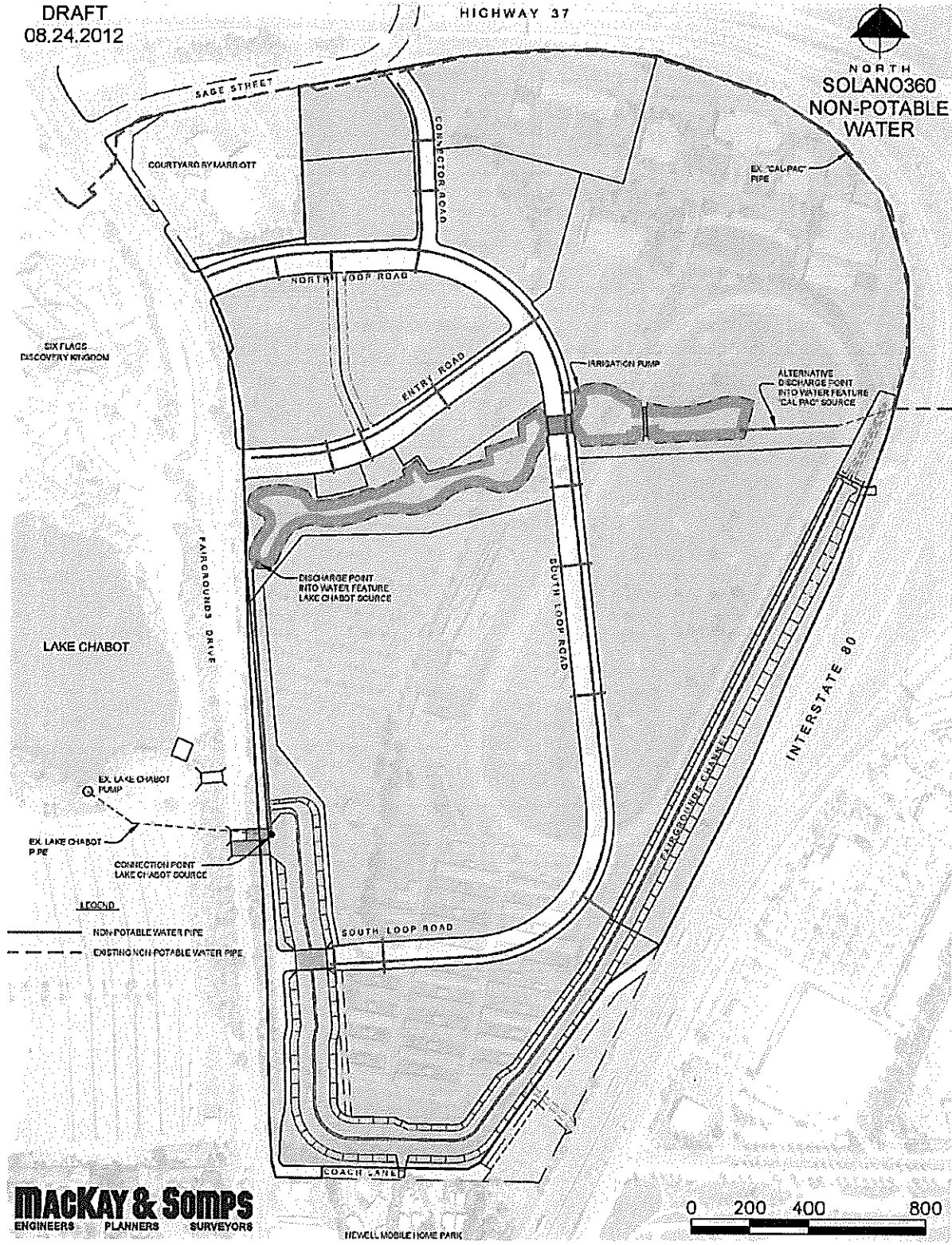
The total average wastewater generation estimate for the Plan Area is approximately 53.0 million gallons per year (0.15 mgd).

VSFCDC has adequate treatment capacity to accommodate development of the Plan Area.

6.4.3 Wastewater Collection and Conveyance

New wastewater pipelines should be constructed under each backbone roadway providing service to each parcel. New facilities located within the public right-of-way or within public easements should be owned and operated by VSFCDC. Existing public pipelines that traverse the Plan Area should be relocated as necessary to avoid conflicts with development.

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MACKAY & SOMPS
ENGINEERS PLANNERS SURVEYORS

Figure 6.3: Non-Potable Water Exhibit

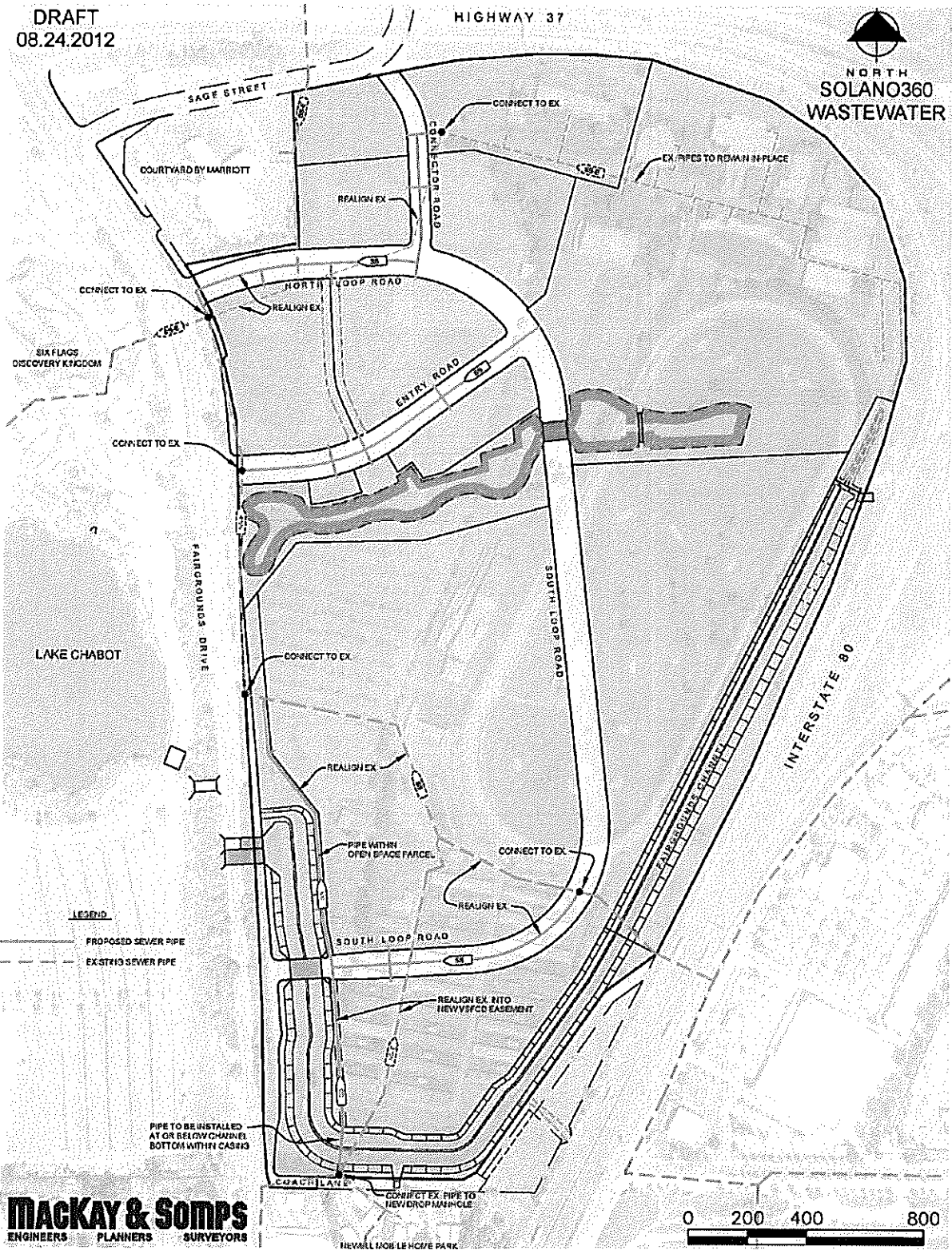


Figure 6.4: Wastewater Exhibit

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HIGHWAY 37

NORTH
SOLANO360
JOINT TRENCH

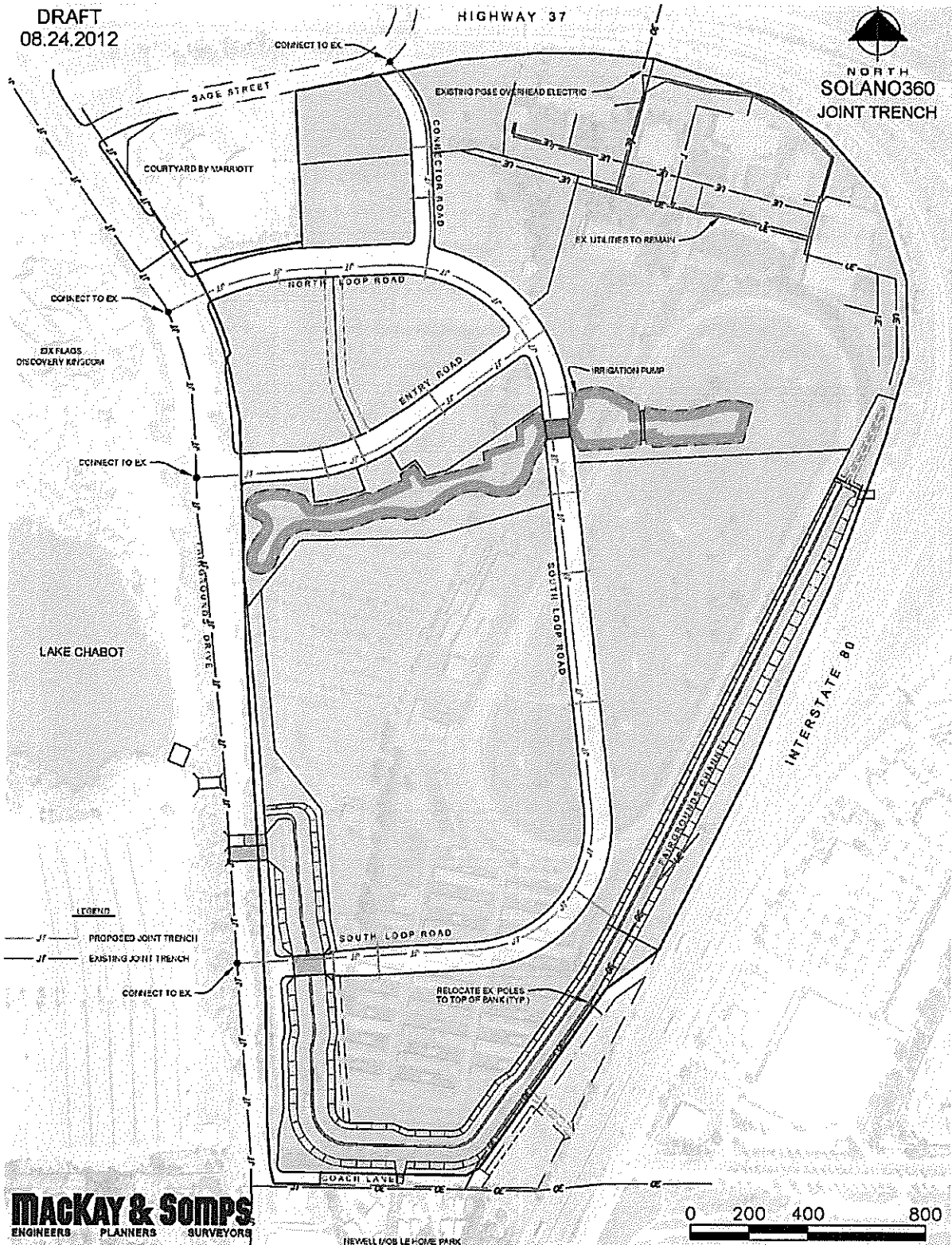


Figure 6.5: Joint Trench Exhibit



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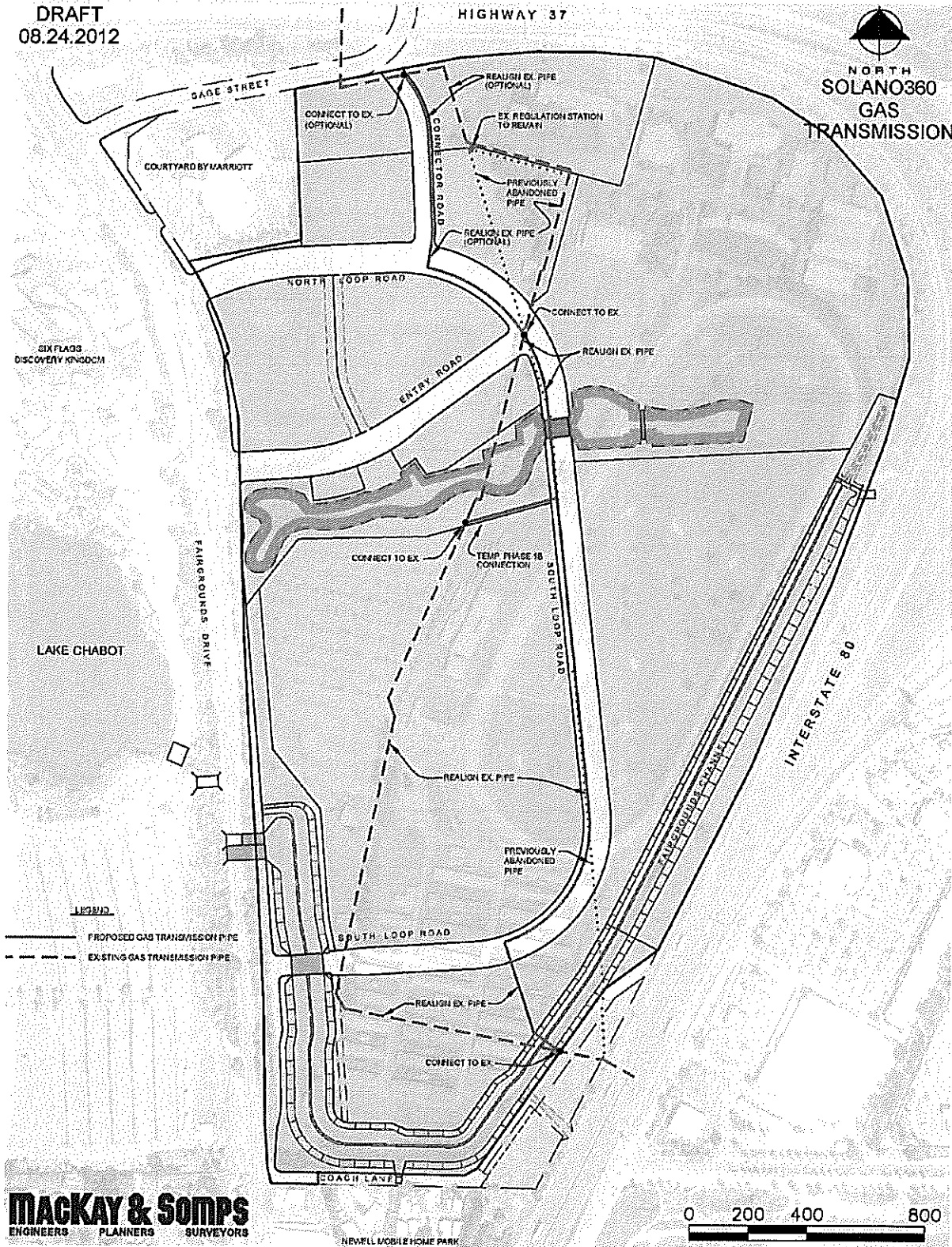


Figure 6.6: Gas Transmission Exhibit

6.4.4 Sustainable Practices for Wastewater

Water reduction, efficiency and conservation measures should be implemented throughout the Plan Area in order to save potable water and reduce wastewater generation. Reducing wastewater flows improves water quality in the Bay by reducing the amount of discharge into the Bay and helps reduce energy requirements associated with treatment and pumped conveyance.

Reclaimed wastewater facilities may be installed under surface parking areas within the Plan Area; this use is included as a permitted use in Section 3.5.

6.5 ENERGY AND TELECOMMUNICATIONS

6.5.1 Background and Existing Conditions

Primary power to the Plan Area is currently provided by Pacific Gas & Electric (PG&E) at the north end of the site. An overhead PG&E power line crosses Highway 37 and connects to PG&E transformers behind the "County Building". The power system leaving the transformers is a private system maintained by the County / Fairgrounds Association. The system consists of both overhead and underground facilities.

An additional power source is located at the south end of the site that provides power to the highway signs along I- 80.

Underground joint trench facilities exist in Fairgrounds Drive. It is anticipated that power will be provided to the proposed development from these facilities.

AT&T telephone facilities exist both underground and overhead throughout the site.

A 12" steel gas transmission pipe runs through the site. A gas regulation station is located near the existing Administration Building, which reduces the transmission pressure down to distribution pressure. Distribution pipes are located throughout the site. It is anticipated that a majority of the transmission pipe will need to be relocated as part of the proposed development.

6.5.2 Electricity and Natural Gas

Natural gas and electricity will be provided to the Plan Area by PG &E. Additional development and build-out of the project as envisioned in this Plan will increase the demand for natural gas and electricity. As noted above it is anticipated that a majority of the existing gas transmission pipe that traverses the Plan Area will need to be relocated.

6.5.3 Telecommunications

Telecommunication and cable service for the Plan Area will be provided by AT&T and CableCom. Additional development and build-out as envisioned in this Plan will increase the demand for telecommunication and cable service.

6.6 PUBLIC SAFETY

Police and fire protection will be provided by the City of Vallejo. A separate Fiscal Impact Analysis has been prepared by Goodwin Consulting Group for the Solano360 Plan. The Fiscal Impact Analysis indicates that the Plan Area will generate sufficient revenue in each Phase to cover Police and Fire Protection costs.



CHAPTER SEVEN: IMPLEMENTATION AND ADMINISTRATION

7.1 INTRODUCTION

This chapter sets forth the planned strategies and actions to be undertaken by the County and City in order to achieve the proposed high quality Private Purpose Area and Public Purpose Area development envisioned in the Plan.

7.1.1 Purpose of the Plan

As described in Chapter One, this document serves a dual purpose:

- For the County, this document serves as a master plan to guide improvements to the Public Purpose Areas, as shown on Figure 1.2. These areas, consisting of the Fair, Major Roads, Shared Public Parking, Creek Park, Fairgrounds Channel, and Transit/North Parking Center, will be exempt from the City's land use authority as long as they are utilized for a public purposes. These areas will be subject to the provisions of the County's process for development review, as well as approvals required from other agencies as described below.
- For the City, this document serves as a Specific Plan and Planned Development Master Plan within the meaning of Vallejo Municipal Code (VMC) Chapters 16.104 and 16.116 to guide development of the Plan Area. Private Purpose Areas, consisting of the Entertainment Mixed Use and Entertainment Commercial parcels (as shown on Figure 1.2: Public & Private Purpose Areas), are subject to the provisions of this Plan and must be consistent with all City codes, regulations, policies and guidelines.

7.1.2 Definition of Public Purpose Areas

Public Purpose Areas, which are owned by the County and utilized for a public purpose, are exempt from City land use authority. Upon adoption of this Plan, the City and County shall enter into an agreement that will establish a process to categorize future uses not contemplated in this Plan into Public Purpose or Private Purpose Areas.

The County intends to construct the Major Roads according to City standards and dedicate them to the City as public rights-of-way when complete.

Requirements and procedures for development of public and private areas, as well as coordination between the agencies, are outlined below.

7.2 DEVELOPMENT STRATEGIES FOR PUBLIC AND PRIVATE AREAS

The County and City intend to take actions to create an environment that is conducive to private investment through by:

- (1) Establishing land use regulations through adoption of the Specific Plan and Master Plan and amendments to the General Plan and City of Vallejo Municipal Code, including specific development standards;
- (2) Certifying environmental review for the proposed project; Approving a financing plan for public infrastructure, and financing / implementing initial public facilities and infrastructure in order to create the high quality character of the area and a sense of place; and
- (3) Vesting of development rights and entitlements through Development Agreement and land use process;

- (4) Establishing a simple and expeditious project approval process for proposed private development consistent with the adopted Plan.

The County and City will also undertake actions outlined in this section to assure that there is an implementation process in place that provides for certainty and consistency related to approval of proposed public and private development actions consistent with the adopted Plan.

In summary, the Solano360 development strategy assumes that the County will have the following Property Owner responsibilities in addition to any set forth in the Conditions of Approval.

- The County and City will enter into a Development Agreement/Implementation MOU.
- The County may issue an RFP for a single Developer or multiple Developers for the site. Such agreement(s) may include a ground lease of land.
- The County, or its Developer(s), will have responsibility for constructing all "horizontal development" (including grading, roads, and utilities) necessary to serve the Plan Area. Major roads will be built by the County and dedicated to the City of Vallejo once constructed to City standards.
- The County, or its Developer(s), will have responsibility for the preparation of finished pads for the EMU and EC parcels.
- The County, or its Developer(s), may seek others to develop the vertical buildings on the EMU and/or EC parcels, or may "build to suit" (develop, maintain and manage).
- The County, or its Developer(s), will sub-lease the EC and EMU parcels.
- EC and EMU end-users will build vertical improvements, or the County, on its own or through its Developer(s), will build-to-suit.

7.3 COUNTY-CITY AGREEMENTS

Subsequent to or concurrent with the certification of the EIR and adoption of the Plan, the County/Fair Association and the City will enter into agreements necessary for successful implementation of the Solano360 Project, including the respective authorities, responsibilities and coordination among the parties regarding proposed public facilities and infrastructure, proposed public and private development and project management responsibilities. It is the intent of the County and City to have these agreements finalized prior to proactively seeking interest from prospective private development interests.

These agreements are expected to include, but are not necessarily limited to the following, which may be combined into one master agreement between the City and the County.

7.3.1 Implementation Memorandum of Understanding (MOU)

The Implementation MOU will address:

- Planning, design, financing, installation, and maintenance of public facilities and infrastructure.
- Public and private development approval processes, including applicable design review.
- Provision and financing of public services to serve the proposed public and private development.
- Process for solicitation and selection of a private developer (Master Developer) for the Private Purpose Area development, including the disposition of County-owned property.

- Execution of necessary deeds between the City and the County necessary to clear title for the Private Purpose Areas.
- Resolution of disputes between the City and County regarding whether a proposed land use is a private or public use.
- Construction, inspection, maintenance, operation, repair, and process for dedication of public rights-of-way.
- Ongoing project management.

7.3.2 Development Agreement

The County and the City intend to enter into a Development Agreement regarding the Plan Area. The terms of that Agreement would be binding upon any Developer with whom the County subsequently contracts.

Section 65864 et seq. of the California Government Code empowers a public agency to enter into a Development Agreement with any entity having control over real property if that entity has an intention to develop that property. Development agreements are contracts established between the agency that approves entitlements for private development (in this case, the City) and the entity proposing the project (in this case, the County). In most situations, a developer or similar interested party negotiates an agreement with the City where both sides commit to a series of actions directly related to a proposed development intended to be implemented or accomplished over a stipulated period of time. Under development agreements, changes in policies governing land uses, intensity of development and the like which occur after approval of a development agreement typically do not affect the property which is the subject of the development agreement. The property owner therefore has a vested right to develop its property in accordance with the provisions of its development agreement.

Development Agreements with property owners and developers may be used to implement the Plan, assure financing and construction of needed public utilities and infrastructure, assure dedication of land for public street right-of-way, public open space and other public purposes, assure compliance with requirements for development pursuant to development constraints, and provide for continuity of implementation of the Plan. Development agreements also may include project phasing and completion schedules, plans for financing of public infrastructure improvements, including any anticipated public financing, adjustments and credits to regulatory fees and development impositions to account for the making of dedications or improvements in excess of the project's "fair share".

Development agreements for projects within the Plan Area must be consistent with the Plan. As required by State Law, the Specific Plan/Master Plan is consistent with the City's General Plan and therefore the development agreements will also be consistent with the General Plan.

Development agreements are subject to approval by an ordinance of the City Council because they are a legislative act. Accordingly, development agreements are subject to the public hearing process, including review and recommendation by the City Planning Commission, prior to being adopted by the City Council.

The Development Agreement between the County and the City may address:

- Vesting of development rights consistent with the provisions of the adopted Plan for the designated Private Purpose Areas. This will provide assurances to the County and end-users that the type and extent of development envisioned in the Plan is vested so that the County can pursue development of the designated Private Purpose Areas consistent with the provisions of the adopted Plan.



- Creation and adoption of specific design standards for the Plan Area.
- Phasing, timing and financing for installation of public infrastructure necessary to serve build-out of the Plan Area.
- Type and extent of development impact fees and other fees to be assessed on proposed Private Purpose Area and Public Purpose Area development.

7.3.3 Cost and Revenue Sharing Agreement

A cost and revenue sharing agreement will be executed by the City and County. The agreement will address:

- Revenues that will be generated by the Plan Area for the benefit of the County, City and Fair Association.
- Costs that will be incurred by the County, Fair and City for public services related to the Plan Area.
- Funding for the Plan Area infrastructure.
- Responsibility for provision of public services to serve the Plan Area.
- Other Plan Area cost and revenue matters.

7.4 REGULATORY AND REVIEW PROCESSES – PRIVATE PURPOSE AREAS

The Plan sets forth the areas which are currently planned to be used for Public Purpose uses as well as those which are planned to be for Private Purpose uses. The Plan sets forth these areas as well as the entitlement process that applies to implementation of Private Purpose uses within the currently planned Private Purpose Areas by the City of Vallejo. In the future, if new or additional uses that have not been considered in this Plan are proposed, City staff and County staff will collaborate in determining whether the proposed use is consistent with this Plan or whether it requires a minor amendment or a major amendment (See VMC 16.116.140). Private uses proposed within an area currently designated as 'public' by this Plan will be subject to the City's land use authority. Any disputes between the County and City will be addressed by a dispute resolution process established in the Implementation MOU.

7.4.1 Relationship to General Plan

The Solano360 Plan establishes policies that will govern future uses and development in the Plan Area and further implement the policies of the City's General Plan. As required by California Government Code section 65454, Plan is consistent with the land use policies and objectives contained in the City of Vallejo's General Plan, as amended to incorporate the Land Use Map and specifications included in this plan.

The following represents a summary of General Plan Amendments required for Solano360 Plan approval by the City of Vallejo. The complete version of the text amendments can be found in Appendix C.

- Amendment to the General Plan Land Use Element and Land Use Map establishing a new Commercial Recreation land use designation for the Plan Area to replace the existing Community Park designation.
- Amendments to the summary section of the General Plan text pertinent to the Solano360 Plan.
- Amendments to various elements of the General Plan text establishing new goals and policies specific to the Solano360 Plan Area for Urban Design, Commercial

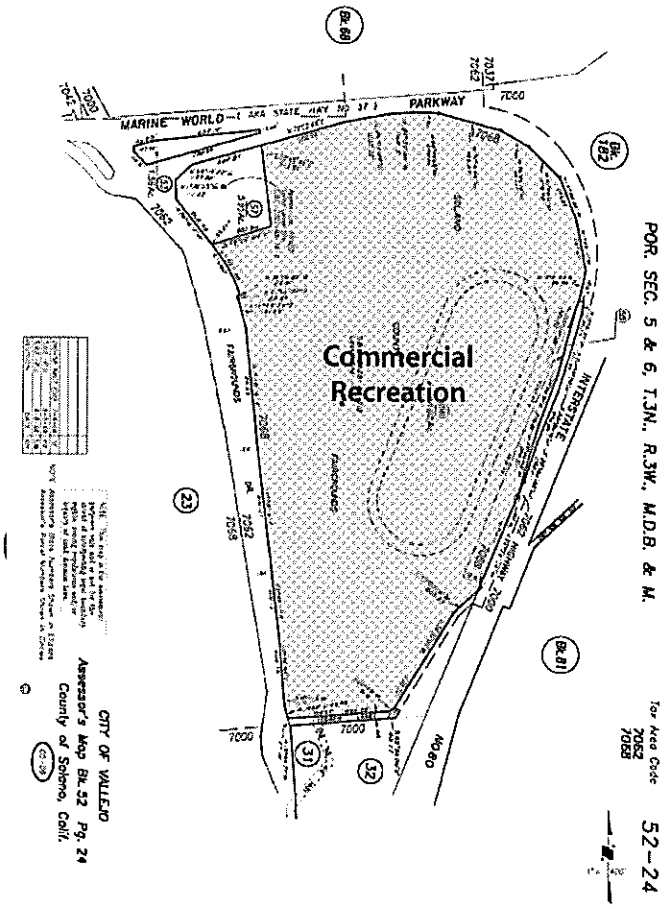


Figure 7.1: General Plan Amendment

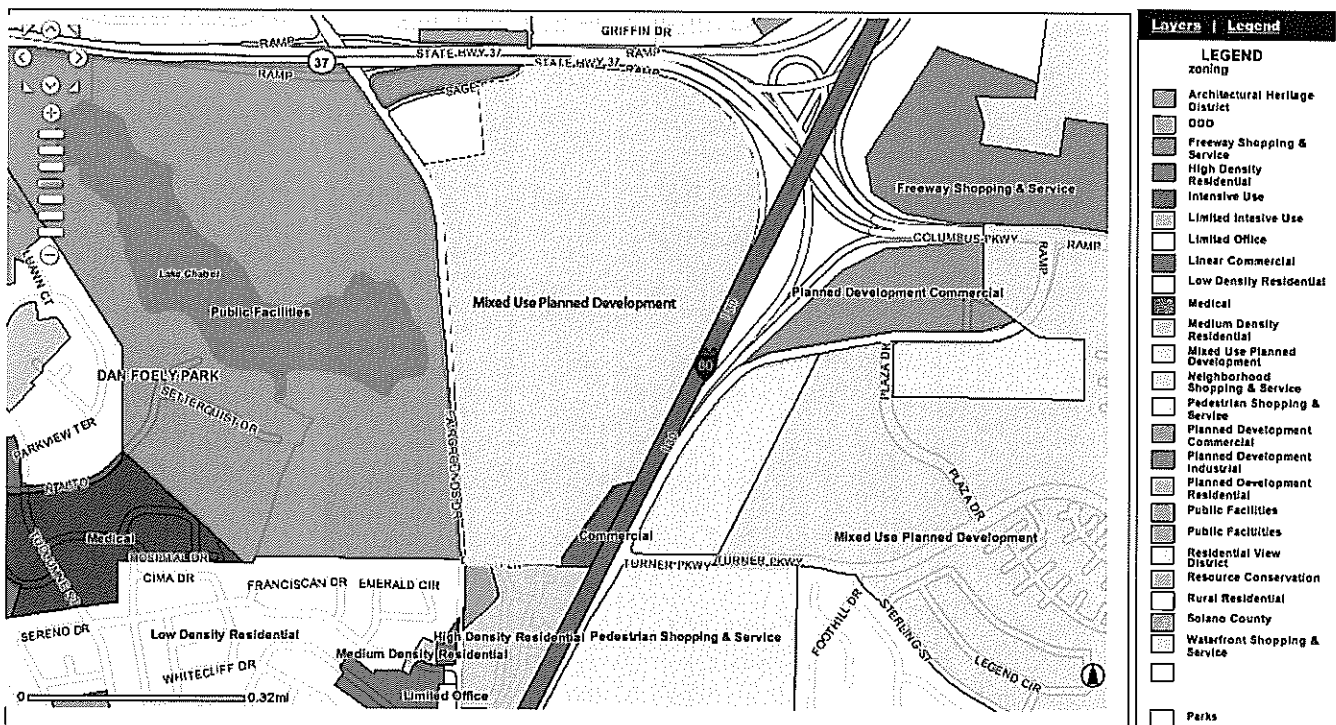


Figure 7.2: Zoning Map Amendment



Development, Transit, Non-Motorized Transportation, and Economic Development.

7.4.2 Zoning

This Plan serves to convert the existing Solano County Fairgrounds property to the City of Vallejo Zoning Designation of Mixed-Use Planned Development (MUPD). The City's Planned Development (PD) is both a zoning district and project development process that allows for flexible application of zoning regulations. The PD process facilitates the development or redevelopment of land which, because of special circumstances, would be difficult to develop through conventional zoning ordinance regulations.

Where there are conflicts between the Plan and the Vallejo Municipal Code, the Plan policies and regulations will apply. Where the Plan is silent, the VMC will apply.

To ensure consistency between the VMC and this Solano360 Plan, the following will be added to VMC Title 16 - Zoning:

16.116.036 – Solano360 Specific Plan/Master Plan.

A Specific Plan for the existing Solano County Fairgrounds shall be prepared to serve as the zoning ordinance for these properties. The Specific Plan shall serve as a Master Plan and shall contain the elements described in Section 16.116.040 applicable to the reuse of these properties as well as an implementation program for subsequent zoning actions. Where the Specific Plan is silent, the Vallejo Municipal Code shall apply

16.116.078 – Solano360 Unit Plans.

Unit plans for reuse and development projects shall be prepared consistent with the policies, standards and implementation program in the Solano360 Specific Plan.

7.4.3 Specific Plan Amendments

The City's Charter authorizes the City to amend a Specific Plan or Master Plan as often as deemed necessary by the legislative body. Plan amendments will be processed in the same manner as a General Plan amendment pursuant to Title 16 of the Vallejo Municipal Code.

The City may amend the Plan by exercising their land use authority at any time pursuant to 16.116.140 of the City's Code in consultation with the County and as needed for successful implementation of development projects within the Private and Public Purpose Areas. The City and the County may choose to contractually limit this authority by executing a Development Agreement as referenced in Section 7.3.2 above.

7.4.4 Subsequent Project Entitlements

The Plan has been prepared to include basic land use entitlements and development standards for the Plan Area. Once the Plan has been approved, development of the Plan Area will proceed and further development approvals and design guidelines will be provided as described below.

Unit Plans

As set forth in Section 16.116.030 of the City of Vallejo's Zoning Ordinance, the Plan serves as a "Master Plan" for each parcel. The Master Plan represents the overall concept for the proposed project and is intended to give the City a comprehensive illustration of the intent and purpose of the Plan Area development.

Master Plans are implemented through Unit Plans which describe the specific design and uses for the project as proposed conceptually in the Master Plan and give the City a more refined and detailed description of structures, landscaping, design features and uses within a particular part of the project.



Accordingly, future private development within the Plan Area will be processed through the City of Vallejo consistent with the following provisions.

- Per Section 16.116 of the City of Vallejo’s Zoning Ordinance, a Unit Plan for the development within the Plan Area shall be prepared consistent with this Plan.
- Upon a finding by the Planning Manager that the uses contemplated by a Unit Plan application are substantially consistent with the provisions of the Plan and provided that no subdivision map or major use permit is required for the Unit Plan, such Unit Plan application will be acted upon by the Planning Division. All property owners within 200 feet of a project under administrative review (staff-level) will be notified. The applicant or any party adversely affected by the decisions of the Planning Manager shall have the right of appeal from such decisions within the time and in the manner prescribed by Chapter 16.102 of the City of Vallejo’s Zoning Ordinance.
- With respect to Unit Plan applications for parcels that also require action on a tentative or vesting tentative subdivision map or that include action on a major conditional use permit, the Planning Division shall forward the Unit Plan application to the Planning Commission (PC) for final action. All property owners within 500 feet of a project under PC review will be notified of the pending PC action. The applicant or any interested party shall have a right to appeal as prescribed by Chapter 16.102 of the City of Vallejo’s Zoning Ordinance.

Subdivision Maps

- Any proposed subdivision of property within the Private Purpose Area will be subject to applicable City of Vallejo subdivision ordinance provisions, requirements and procedures.
- Tentative Maps or Vesting Tentative Maps for subdivision of property will be reviewed by staff and approved by the Planning Commission.
- Final Maps for subdivision of property will be reviewed by staff and approved by the City Council.

Additional Actions

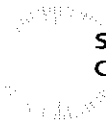
Additional future actions may include, but not be limited to, the issuance of:

- Grading and building permits
- Improvement plans
- Landscape and irrigation plans
- Will-serve letters for potable water
- Minor Use Permits
- Sign Permits
- Administrative Permits (accessory structures and temporary activities)
- Any other permits or approvals as required by the VMC.

7.5 REGULATORY AND REVIEW PROCESSES – PUBLIC PURPOSE AREAS

The provisions that apply to implementation of Public Purpose Areas will be the primary responsibility of the County.





7.5.1 County Approvals

Upon certification of the Solano360 Specific Plan EIR discussed in Section 7.7 below, no further environmental review of the Public Purpose Areas will be needed, except as may be required under CEQA (see Section 7.7.1).

County will engage the services of necessary design professionals to prepare the plans and specifications for the Public Purpose Areas, including the Fair, Major Roads and Fairgrounds Channel in full compliance with applicable building codes, ordinances and other regulatory authorities. The County's Department of Resource Management - Building Division will oversee plan review and applicable building and grading permits. The County's Building Division will ensure compliance with all applicable laws, codes, ordinances, rules or regulations of affected governmental agencies, such as the City of Vallejo Fire Marshall and Vallejo Flood & Sanitation District, affecting the construction.

- Any future modifications to the land use plan contained in this Plan will be presented to the City for determination of conformity to the City's General Plan in accordance with California Government Code section 65402.

7.5.3 Other Agencies

A number of other agencies in addition to the City of Vallejo will serve as Responsible and Trustee Agencies. The EIR will provide environmental information to these agencies and other public agencies, which may be required to grant approvals or coordinate with other agencies, as part of project implementation.

As described in more detail in the EIR, these agencies may include but are not limited to the following.

- U.S. Fish and Wildlife Service (USFWS)
- U.S. Army Corps of Engineers (USACE)
- California Department of Fish and Game (CDFG)
- California Department of Transportation (Caltrans)
- San Francisco Regional Water Quality Control Board (RWQCB)
- Bay Area Air Quality Management District (BAAQMD)
- Greater Vallejo Recreation District (GVRD)
- Vallejo Sanitary and Flood Control District (VSFCD)

7.6 INFRASTRUCTURE FINANCING

The Plan studies include a Public Facilities Financing Plan (PFFP) and a Fiscal Impact Analysis. Executive summaries of these reports are provided as Appendices B and C. Full reports are available separately. In summary:

PFFP: The Solano360 Specific Plan Public Facilities Financing Plan (PFFP) evaluated the ability of Plan land uses to fund required public facilities. It identified appropriate financing tools and aligned them with those public facility needs, providing a long-term forecast of the financial burdens associated with providing infrastructure to the Solano360 project. The PFFP serves as a blueprint to guide subsequent individual development applications and will ensure that future development conforms to the financial strategies outlined in this plan.

Fiscal Impact Analysis: The Solano360 Specific Plan Fiscal Impact Analysis evaluated the potential recurring fiscal impacts to the City and County resulting from development of the Solano360



project. The fiscal impact analysis compared the annual costs associated with providing public services against the annual revenues that will be generated by the proposed development to determine the net fiscal impact.

7.7 COMPLIANCE WITH CEQA

The Solano360 Specific Plan EIR has been prepared in accordance with the California Environmental Quality Act (CEQA) to evaluate the potential environmental impacts associated with implementation of the Specific Plan (State Clearinghouse No. 2011092067). The purpose of the EIR is to inform decision makers, representatives of affected and responsible agencies, the public, and other interested parties of the potential environmental effects that may result from implementation of the proposed project.

The County served as Lead Agency for the EIR. The Solano360 Specific Plan EIR serves for environmental clearance on County and City approvals.

7.7.1 Additional Environmental Review

Proposed private and / or public development may require additional environmental review and documentation in accordance with CEQA depending on the extent of consistency of the specific proposed development to the type / extent of development analyzed in the Specific Plan EIR. The determination as to the need for additional environmental review will be made by the City in the context of the regulatory review process for Private Purpose Area development, and the County in the context of the regulatory review process for Public Purpose Area development.

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. 13-07

RESOLUTION OF THE VALLEJO PLANNING COMMISSION RECOMMENDING THAT CITY COUNCIL APPROVE A DEVELOPMENT AGREEMENT BETWEEN THE CITY OF VALLEJO AND THE COUNTY OF SOLANO FOR THE SOLANO360 PROJECT

* * * * *

BE IT RESOLVED by the Planning Commission of the City of Vallejo as follows:

WHEREAS, the County of Solano (“County”) owns the property commonly referred to as the Solano County Fairgrounds located wholly within the City of Vallejo, comprising approximately 149.1 acres of land; and

WHEREAS, on July 11, 2003, the County, City, and the Solano County Fair Association entered into a Memorandum of Understanding acknowledging that the three entities share a common vision for the revitalization of the Property and recognizing the need to reach agreement regarding the planning and permitting process for the redevelopment of the Fairgrounds project; and

WHEREAS, pursuant to the 2003 MOU, the Parties agreed that the primary focus of the Project was the renovation and revitalization of the County Fair but the location of the Property also afforded the Parties with an opportunity of developing new, supportive commercial, entertainment and recreation-related uses; and

WHEREAS, the Parties agreed to develop the Project Vision based on the following jointly-adopted Guiding Principles:

- a. Generate revenues for Solano County and the City of Vallejo, create jobs and ensure long-term economic sustainability;
- b. Establish a unique place with an unmistakable identity that serves as a destination for visitors as well as a pedestrian-friendly, community gathering place;
- c. Explore a mix a complementary land uses, including retail, commercial, hospitality, recreational, residential, family and youth-oriented, educational and civic uses that seamlessly integrate with the “Fair of the Future”;
- d. Explore increased physical connectivity and synergy with Six Flags Discovery Kingdom, downtown Vallejo, the waterfront and other existing commercial operations;
- e. Provide pedestrian, bicycle, vehicular and transit facilities that foster access to, from and within the site;

f. Incorporate sustainable and green principles in all aspects of the development; and

WHEREAS, on March 10, 2009 the parties concurrently terminated the 2003 MOU and entered into a new Memorandum of Understanding (“**2009 MOU**”) that established the respective roles of the County and the City with regards to their cooperative effort to develop a Project Vision for the redevelopment of the County Fairgrounds; and

WHEREAS, since 2009, the parties amended and restated the 2009 MOU twice to articulate the parties’ shared commitment to implement the Project Vision in an expeditious manner and further clarify the respective roles as such relates to the private development areas of the Project; and

WHEREAS, in order to successfully develop a coordinated Project Vision, the parties established the Solano360 Committee which included two representatives from the Board of Supervisors and the Mayor and two members from the City Council to provide policy direction throughout developing the Project; and

WHEREAS, the Project has culminated in the Solano360 Specific Plan/Master Plan, as approved by the County on February 26, 2013 as its master plan for the coordinated development of the Property pursuant to its authority as landowner and project proponent; and

WHEREAS, the Project was the subject of an environmental impact report (EIR) prepared under the California Environmental Quality Act (CEQA) (set forth in Public Resources Code §§ 21000 *et seq.*) which was certified by the Board of Supervisors, acting as the lead agency, on February 26, 2013. The City acted as a responsible agency as defined by Public Resources Code § 21069. The EIR analyzed the identified environmental impacts for significance as associated with the incremental change in intensity and distribution of land uses pursuant to the Project. The County also adopted a Mitigation Monitoring and Reporting Program (MMRP) for the Project; and

WHEREAS, pursuant to California Government Code Section 65864 *et seq.* and Title 17, Part II of the City of Vallejo Municipal Code, the City is authorized to enter into an agreement for the development of real property with any person having a legal or equitable interest in such property in order to establish development rights in such property. The purpose of the Development Agreement Statutes is to authorize municipalities, in their discretion, to establish certain development rights in real property for a period of years regardless of intervening changes in land use regulations; and

WHEREAS, the complexity, magnitude and long-term build out of the Solano360 Project and site area would not be feasible if the Parties had not agreed to work cooperatively to provide a sufficient degree of certainty in the land use regulatory process to justify the County’s and/or City’s substantial financial investment associated with development of the Private Purpose Areas of the Project; and

WHEREAS, the Planning Commission recommends that the City enter into this Agreement to establish development rights for the Private Purpose Areas, as authorized by the Development Agreement Statutes and to facilitate the process of planning, financing and proceeding with

construction of the Project and promote the achievement of the private and public objectives of the Project; and

WHEREAS, as a material inducement for City to enter into this Agreement, County has agreed to construct or cause the construction of certain on site Infrastructure Improvements and participate in the funding of certain off-site Infrastructure Improvements described in this Agreement within the time set forth herein; and

WHEREAS, County plans to develop the Project in multiple phases, as described in the Solano360 Specific Plan/Master Plan and EIR; and

WHEREAS, the County intends that the cost of providing City services to the Project, including police, fire, parks and streetscape, landscape and lighting maintenance, repair and replacement, be fully recouped from City taxes and fees generated from the Project. County agrees that City would not enter into this Agreement if City's reasonable costs in providing services were not covered in full by the revenues generated by the Project; and

WHEREAS, the Parties recognize and acknowledge that the Solano360 Specific Plan Public Facilities Financing Plan contemplates and recommends that the County issue six Certificates of Participation and three Capital Appreciation Bonds to fund the Public Use Area share of facilities and infrastructure and to initially fund a portion of the Private Use Area share of facilities; and

WHEREAS, the City and the County wish to enter into a revenue sharing agreement, whereby the City will contribute a portion of its revenue from the Project area to the County to assist with the County's financing of the Project; and

WHEREAS, except for the potential creation of a Community Facilities District, the Development Agreement does not require the City to make any commitment to issue debt, or be liable to a third party, including a Certificate Holder on any Project Financing issued by the County, for debt service payments; and

WHEREAS, as proposed, the Agreement will not be detrimental to the health, safety and general welfare, nor will it adversely affect the orderly development of the Solano County Fairgrounds or the preservation of property values in the Fairgrounds area, in that the Development Agreement will: (a) require that development of the Private Purpose Areas of the Fairgrounds conform with the all ordinances, resolutions, regulations, standards, and specifications existing at the time of approval of the Agreement and applicable to the Project, including the Vallejo General Plan, Zoning Ordinance, and Solano360 Specific Plan; (b) allow new City laws and regulations which are reasonably necessary to protect the public health and safety to be applied to the project; (c) result in development that will eliminate blighted conditions in the project areas; (d) result in the extension of public infrastructure to support the planned uses; (e) provide for the development of the Fairgrounds according to the principles of the Vision Plan; (f) provide revenues to the City in the form of taxes and fees; and (g) provide the public benefits set forth above.

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission hereby recommends that the City Council approve Development Agreement # _____, as presented

in the Development Agreement By and Between the City of Vallejo and the County of Solano, for the Solano360 Project provided herein as Exhibit A to Attachment 2.

RECORDING REQUESTED BY
AND WHEN RECORDED RETURN TO:

City of Vallejo
555 Santa Clara Street
Vallejo, CA 94590
Attention: City Clerk

Space Above This Line Reserved for Recorder's Use
Exempt from Recording Fee Per Government Code Section 27383

DEVELOPMENT AGREEMENT

BY AND BETWEEN

CITY OF VALLEJO

AND

COUNTY OF SOLANO

DATED _____

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LIST OF EXHIBITS:

Exhibit A	Project Site Plan
Exhibit B	List of Taxes, Assessments, Fees and Exactions
Exhibit C	Estimated City Annual Revenue Sharing Payment
Exhibit D	Project Infrastructure Schedule

DEVELOPMENT AGREEMENT

THIS DEVELOPMENT AGREEMENT ("*Agreement*") dated for reference purposes as of _____, is entered into between COUNTY OF SOLANO, a political subdivision of the State of California ("*County*"), and the CITY OF VALLEJO, a California municipal corporation ("*City*"). County and City are sometimes referred to individually as a "*Party*" and collectively as "*Parties.*"

RECITALS

A. County represents that it owns the property commonly referred to as the Solano County Fairgrounds located wholly within the City, comprising approximately 149.1 acres of land ("*Property*").

B. On July 11, 2003, the County, City, and the Solano County Fair Association ("*SCFA*") entered into a Memorandum of Understanding (the "*2003 MOU*") acknowledging that the three entities share a common vision for the revitalization of the Property ("*Project Vision*") and recognizing the need to reach agreement regarding the planning and permitting process for the redevelopment of the Fairgrounds project ("*Project*").

C. Pursuant to the 2003 MOU, the Parties agreed that the primary focus of the Project was the renovation and revitalization of the County Fair but the location of the Property also afforded the Parties with an opportunity of developing new, supportive commercial, entertainment and recreation-related uses.

D. The Parties agreed to develop the Project Vision based on the following jointly-adopted Guiding Principles:

(1) Generate revenues for Solano County and the City of Vallejo, create jobs and ensure long-term economic sustainability;

(2) Establish a unique place with an unmistakable identity that serves as a destination for visitors as well as a pedestrian-friendly, community gathering place;

(3) Explore a mix a complementary land uses, including retail, commercial, hospitality, recreational, residential, family and youth-oriented, educational and civic uses that seamlessly integrate with the "Fair of the Future";

(4) Explore increased physical connectivity and synergy with Six Flags Discovery Kingdom, downtown Vallejo, the waterfront and other existing commercial operations;

(5) Provide pedestrian, bicycle, vehicular and transit facilities that foster access to, from and within the site;

(6) Incorporate sustainable and green principles in all aspects of the development.

E. On March 10, 2009 the parties concurrently terminated the 2003 MOU and entered into a new Memorandum of Understanding ("**2009 MOU**") that established the respective roles of the County and the City with regards to their cooperative effort to develop a Project Vision for the redevelopment of the County Fairgrounds.

F. Since 2009, the parties amended and restated the 2009 MOU twice to articulate the partnership between the member agencies and the shared commitment to implement the Project Vision in an expeditious manner and further clarify the respective roles as such relates to the private development areas of the Project.

G. In order to successfully develop a coordinated Project Vision, the parties established the Solano360 Committee which included two representatives from the Board of Supervisors and the Mayor and two members from the City Council to provide policy direction throughout developing the Project.

H. The term of the 2009 MOU was for a period of five years from the date of its execution or upon the execution of a mutually acceptable development agreement between the County and a developer, whichever was to have occurred first.

I. The Parties have agreed that approval of this Agreement satisfies the development agreement contemplated in the 2009 MOU, as subsequently amended, and with its execution of this Agreement, the 2009 MOU is terminated and any outstanding obligations are incorporated in this Agreement.

J. The Project has culminated in the Solano360 Specific Plan/Master Plan, as approved by the County on February 26, 2013 ("**Solano360 Specific Plan/Master Plan**") as its master plan for the coordinated development of the Property pursuant to its authority as landowner and project proponent, and as adopted by the City on May 14, 2013 pursuant to City's land use and regulatory authority, and as more particularly shown in Exhibit "A".

K. Areas of the Project that are proposed to be used for primarily County or fairgrounds related, public, governmental purposes are referred to as "**Public Purpose Areas.**" Areas of the Project that are used for public and/or governmental purposes are exempt from the City's land use authority. Public Purpose Areas do not include easements, facilities, public rights-of-way, or other areas controlled by the City, public utilities, or agencies other than the parties once dedicated for such use.

L. Areas of the Project that are proposed to be used for primarily private, proprietary, or revenue-generating purposes are referred to "**Private Purpose Areas.**" Private Purpose Areas are subject to the City's land use regulations and authority. Private Purpose Areas include those areas of the Property that will be leased by the County to private entities for primarily private, revenue generating purposes and are subject to the City's land use authority regardless of their geographical location in the Project.

M. The Project was the subject of an environmental impact report ("**EIR**") prepared under the California Environmental Quality Act ("**CEQA**") (set forth in Public Resources Code §§ 21000 *et seq.*) which was certified by the Board of Supervisors, acting as the lead agency, on February 26, 2013 ("**FEIR**"). The City acted as a responsible agency as defined by Public Resources Code § 21069. The EIR analyzed the identified environmental impacts for significance as associated with the incremental change in intensity and distribution of land uses pursuant to the Project. The County also adopted a Mitigation Monitoring and Reporting Program ("**MMRP**") for the Project. On or about April 29, 2013, the Vallejo City Planning Commission reviewed and considered the Solano360 Specific Plan/Master Plan governing the land use policy and development process in accordance with Vallejo Municipal Code ("**VMC**"), Title 16.

N. On or about May 14, 2013, the City Council adopted the Solano360 Specific Plan/Master Plan governing the land use policy and development process as an ordinance in accordance with VMC, Title 16.

O. The City, acting as a responsible agency, and in connection with the adoption of the Specific Plan/Master Plan, relied upon the FEIR for purposes of CEQA review for the approvals granted for development of the Private Purpose Area.

P. Prior to or concurrently with approval of this Agreement, City has approved various legislative and/or discretionary actions in connection with the development of the Project on the Private Purpose Areas. These approvals include:

- (1) the General Plan Amendment
- (2) the Zoning Ordinance Amendment
- (3) the Vesting Tentative Map
- (4) the Specific Plan/Master Plan and Design Guidelines

The approvals and development policies described in this Recital P are collectively referred to herein as the "**Existing Project Approvals**".

Q. Subsequent to approval of this Agreement, certain future applications ("**Future Project Administrative Applications**") will be required by the City in order to facilitate the development of the Private Purpose Areas. The Future Project Applications are ministerial or administrative, and may include, without limitation, design review approvals, building permits, unit plans that are consistent with the Specific Plan/Master Plan, grading permits, sewer and water connection permits, certificates of occupancy, any other ministerial or administrative applications, and any amendments to, or actions repealing of, any of the foregoing. To the extent there are additional discretionary, non-administrative applications required by the City, such as lot line adjustments, or rezonings, such applications as are referred to collectively as the "**Future Project Discretionary Applications**."

R. In addition, certain other applications to or agreements by other agencies may be necessary to facilitate the development of the Project which may include, without limitation, Vallejo Sanitation and Flood Control District ("*VSFCD*"); orders, permits, requirements and approvals of BCDC, DTSC and/or the Regional Water Quality Control Board; or any amendments to any of the foregoing. Subsequent applications for project approvals from other agencies are referred to collectively as "*Other Agency Future Project Applications*."

S. The Future Project Administrative Applications, Future Project Discretionary Applications, and the Other Agency Future Project Applications are referred to collectively as the "*Future Project Applications*" which, upon final approval shall be deemed "*Subsequent Project Approvals*" Subsequent Project Approvals together with Existing Project Approvals shall be deemed "*Project Approvals*".

T. California Government Code Section 65864 *et seq.* ("*Development Agreement Statute*") and Title 17, Part II of the City of Vallejo Municipal Code authorize the City to enter into an agreement for the development of real property with any person having a legal or equitable interest in such property in order to establish development rights in such property.

U. The City Council has found that development agreements will strengthen the public planning process, encourage private participation in comprehensive planning by providing a greater degree of certainty in that process, reduce the economic costs of development, allow for the orderly planning of public improvements and services, allocate costs to achieve maximum utilization of public and private resources in the development process, and assure that appropriate measures to enhance and protect the environment are achieved.

V. The complexity, magnitude and long-term build out of the Solano360 Project and site area would not be feasible if the Parties had not agreed to work cooperatively to provide a sufficient degree of certainty in the land use regulatory process to justify the County's and/or City's substantial financial investment associated with development of the Private Purpose Areas of the Project.

W. To strengthen the public planning process, encourage private participation in comprehensive planning and reduce the economic risk of development, the Legislature of the State of California enacted the Development Agreement Statutes, authorizing municipalities to enter into development agreements in connection with the development of real property within their jurisdiction with persons having a legal or equitable interest in such real property.

X. The purpose of the Development Agreement Statutes is to authorize municipalities, in their discretion, to establish certain development rights in real property for a period of years regardless of intervening changes in land use regulations.

Y. The parties desire to enter into this Agreement to establish development rights for the Private Purpose Areas, as authorized by the Development Agreement Statutes

and to facilitate the process of planning, financing and proceeding with construction of the Project and promote the achievement of the private and public objectives of the Project.

Z. The City Council has found that this Agreement is consistent with the City's General Plan and the Specific Plan/Master Plan and it has been reviewed and evaluated in accordance with Section 17.14.010 of the City of Vallejo Municipal Code.

AA. City has determined that by entering into this Agreement, City will promote orderly growth and quality development in accordance with the goals and policies set forth in the General Plan and the Specific Plan/Master Plan, and City will benefit from increased employment and commercial opportunities created by the Project for residents of City.

BB. As a material inducement for City to enter into this Agreement, County has agreed to construct or cause the construction of certain on site Infrastructure Improvements and participate in the funding of certain off-site Infrastructure Improvements ("*Infrastructure Improvements*") described in this Agreement within the time set forth herein.

CC. County has determined that by entering into this Agreement, County will be able to facilitate the integrated iconic development of both the Public Purpose Areas and Private Purpose Areas that will benefit all citizens of Solano County.

DD. As a material inducement for County to enter into this Agreement, the City has agreed to establish an expedited approval process to ensure a coordinated implementation of the Specific Plan/Master Plan as envisioned for the discretionary and ministerial permits under the City's purview.

EE. The Parties intend that the Project will fully cover the cost of providing municipal services to the Project as well as generate revenue to fund the infrastructure necessary to serve the build-out of the Project as set forth in Article 8 below

FF. County plans to develop the Project in multiple phases, as described in the Solano360 Specific Plan/Master Plan and FEIR. The Parties recognize that construction for Phase 2 or 3 is predicated on the construction of the Redwood Parkway/Fairgrounds Drive improvement project at the two interchanges as identified in the STA's Comprehensive Transportation Plan. Although that project is recommended for funding in the MTC Regional Transportation Plan, it is currently unfunded. The Parties acknowledge that to the extent that funding is not available for the off-site improvements that this Agreement may need to be modified to address the Project's implementation.

GG. The Parties agree that revenue generated from the Project will also reimburse the County for costs incurred for the Project Vision as previously agreed to by the Parties in the 2009 MOU as set forth in Article 8.

HH. The terms and conditions of this Agreement have undergone review by City staff, its Planning Commission and the City Council at publicly noticed meetings and have been found to be fair, just and reasonable and in conformance with the Vallejo General Plan and Specific Plan/Master Plan and, further, the City Council finds that the economic

interests of City's citizens and the public health, safety and welfare will be served by entering into this Agreement.

II. On April 29, 2013, the Planning Commission of the City of Vallejo recommended approval of this Agreement. On May 14, 2013, the City Council of the City of Vallejo adopted Ordinance No. _____, approving this Agreement

AGREEMENT

NOW, THEREFORE, the parties agree:

ARTICLE 1. DEFINITIONS

Section 1.01. Definitions.

"2003 MOU" in defined in Recital B.

"2009 MOU" in defined in Recital E.

"Advance Payments" is defined in Section 8.02.

"Agreement" means this Development Agreement.

"Applicable Law" is defined in Section 6.04.

"Assignee" is defined in Section 12.01.

"Available City Revenue" is defined in Section 8.02.

"CAB" is defined in Section 8.01.

"CEQA" is defined in Recital M.

"CFD" is defined in Section 8.05.

"Changes in the Law" is defined in Section 6.12.

"City" means the City of Vallejo, a California municipal corporation.

"City Annual Project Gross Revenue" is defined in Section 8.01.

"City Annual Revenue Sharing Payment" is defined in Section 8.02.

"City Impact Fees" is defined in Section 7.01.

"City Law" is defined in Section 6.04.

"City Indemnities" is defined in Section 14.01.

"Claims" means liabilities, obligations, orders, claims, damages, fines, penalties and expenses, including attorneys' fees and costs.

"COP" is defined in Section 8.01.

"County" means County of Solano, a political subdivision of the State of California.

"County Annual Project Net Revenue" is defined in Section 8.01.

"County Entitlement Costs" is defined in Section 8.04.

"County Indemnities" is defined in Section 14.02.

"Debt Service" is defined in Section 8.01.

"Default" is defined in Section 1301.

"Design Guidelines" is defined in Section 6.01

"Development Agreement Statute" is defined in Recital T.

"Environmental Laws" means all federal, state and local laws, ordinances, rules and regulations now or in force, as amended from time to time, relating to or regulating human health or safety, or industrial hygiene or environmental conditions, or protection of the environment, or pollution or contamination of the air, soil, surface water or groundwater, and includes, but is not limited to, the Comprehensive Environmental Response, Compensation and Liability Act of 1980, 42 U.S.C. §9601 *et seq.*, the Solid Waste Disposal Act, 42 U.S.C. §6901 *et seq.*, the Hazardous Substance Account Act, California Health and Safety Code, §25300 *et seq.*, the Hazardous Waste Control Law, California Health and Safety Code, §25100 *et seq.*, and the Porter-Cologne Water Quality Control Act, California Water Code, §13000 *et seq.*, the Global Warming Solutions Act of 2006, California Health and Safety Code, §38500 *et seq.*, and the California Endangered Species Act (CESA) California Fish & Game Code, §2050, *et seq.*

"Effective Date" is defined in Section 2.01.

"EIR" is defined in Recital M.

"Estimated City Annual Revenue Sharing Payment" is defined in Section 8.02.

"Existing Project Approvals" is defined in Recital P.

"Federal/State Compliance Fees" is defined in Section 7.01.

"FEIR" is defined in Recital M.

"Future Project Applications" is defined in Recital S.

"Future Project Administrative Applications" is defined in Recital Q.

"Future Project Discretionary Applications" is defined in Recital Q.

"Ground Lease" is defined in Section 3.01

"Infrastructure Improvements" is defined in Recital BB.

"Initial Term" is defined in Section 2.02.

"Issuing Party" is defined in Section 8.05.

"LEED" is defined in Section 9.04.

"MMRP" is defined in Recital M.

"Measure B" means the City of Vallejo's Sales and Use Tax measure, adopted by the voters on November 8, 2011.

"Optional Extended Term" is defined in Section 2.02.

"Other Agency Future Project Applications" is defined in Recital R.

"Other Local Agency Compliance Fees" is defined in Section 7.01.

"Party/Parties" is defined in the introductory paragraph preceding the Recitals of this Agreement.

"PFFP" is defined in Section 8.01.

"Phase 1 Project Financing" is defined in Section 8.01.

"Prevailing Wage Laws" is defined in **Error! Reference source not found..**

"Processing Fees" is defined in Section 7.01.

"Project" is defined in Recital A.

"Project Approvals" is defined in Recital S.

"Project Financing" is defined in Section 8.01.

"Project Vision" is defined in Recital B.

"Property" is defined in Recital A.

"Private Purpose Area" is defined in Recital L.

"Public Purpose Area" is defined in Recital K.

"Reversionary Interests" are defined in Section 3.02.

"Solano360 Specific Plan/Master Plan" is defined in Recital J.

"Subsequent Project Approvals" is defined in Recital S.

"Term" is defined in Section 2.02.

"VSFCD" is defined in Recital R.

"Water Fees" is defined in Section 7.01.

ARTICLE 2. EFFECTIVE DATE AND TERM

Section 2.01. Effective Date. This Agreement shall become effective following execution of the Parties and upon the date that the ordinance approving this Agreement becomes effective ("**Effective Date**").

Section 2.02. Term. The term of this Agreement shall be the Initial Term with the Extended Term, if any ("**Term**").

A. Initial Term. The initial term of this Agreement shall commence upon the Effective Date and expire on the date which is fifty (50) years after the Effective Date ("**Initial Term**"). The Initial Term has been established by the Parties as a reasonable estimate of the time required to develop the Project, including the Infrastructure Improvements, and obtain the public benefits of the Project.

B. Optional Extended Term. The parties agree that this Term may be extended, by mutual agreement, beyond the Initial Term to the extent that Infrastructure Financing is needed to extends beyond the Initial Term ("**Optional Extended Term**"). Notwithstanding any other provision of this Agreement, the parties agree that no debt will be issued for the Project that may be due after the end of the Initial Term without written mutual agreement.

C. Termination Following Expiration. Following the expiration of the Term, or the earlier completion of development of the Project and all of City's and County's obligations in connection with this Agreement, this Agreement shall be deemed terminated and of no further force and effect.

Section 2.03. County Representations and Warranties. County represents and warrants to City that, as of the Effective Date:

A. No approvals or consents of any persons are necessary for the execution, delivery or performance of this Agreement by County, except as have been obtained;

B. The execution and delivery of this Agreement and performing the obligations of County have been duly authorized by all necessary action, including the submission and approval of this Agreement by County's Board of Supervisors;

C. This Agreement is a valid obligation of County and is enforceable under its terms; and

D. County has not (i) made a general assignment for the benefit of creditors, (ii) filed any voluntary petition in bankruptcy or suffered filing any involuntary petition by County creditors, (iii) suffered the appointment of a receiver to take possession of all, or substantially all, of County assets, (iv) suffered the attachment or other judicial seizure of all, or substantially all, of County's assets, (v) admitted in writing its inability to pay its debts as they come due, or (vi) made an offer of settlement, extension or composition to its creditors.

During the Term of this Agreement, County shall, upon learning of any fact or condition which would cause the warranties and representations in this Section 2.03 not to be true, immediately give written notice of such fact or condition to City.

Section 2.04. City Representations and Warranties. City represents and warrants to County that, as of the Effective Date:

A. No approvals or consents of any persons are necessary for the execution, delivery or performance of this Agreement by City, except as have been obtained;

B. The execution and delivery of this Agreement and performing the obligations of City have been duly authorized by all necessary action, including the submission and approval of this Agreement by the City Council;

C. This Agreement is a valid obligation of City and is enforceable under its terms; and

D. Subsequent to its Chapter 9 Action in U.S. Bankruptcy Court for the Northern District (entitled *In Re* City of Vallejo, debtor, Case No. 2008-26813), City has not (i) made a general assignment for the benefit of creditors, (ii) filed any voluntary petition in bankruptcy or suffered filing any involuntary petition by City creditors, (iii) suffered the appointment of a receiver to take possession of all, or substantially all, of City assets, (iv) suffered the attachment or other judicial seizure of all, or substantially all, of City's assets, (v) admitted in writing its inability to pay its debts as they come due, or (vi) made an offer of settlement, extension or composition to its creditors.

During the Term of this Agreement, City shall, upon learning of any fact or condition which would cause the warranties and representations in this Section 2.04 not to be true, immediately give written notice of such fact or condition to County.

ARTICLE 3. OWNERSHIP OF PROPERTY

Section 3.01. County as Landowner. County owns the Property and will retain fee title of the Property throughout the development of the Project. County intends to enter into a ground lease(s) ("**Ground Lease**") with a private developer(s) for the development of the Private Purpose Areas.

Section 3.02. Reversionary Interests. As described in the 2009 MOU, the County executed and recorded a quit claim deed transferring back to the City certain Reversionary Interests, namely, (i.e., all interests described in the Quitclaim Deed (Document No. 200900021838) dated July 9, 2003 and further described as follows: "Any and all reversionary interest, rights of reentry and/or powers of termination identified in the deed to the County of Solano dated January 16, 1947, recorded in Book 387, Page 108 of the Official Records of the County of Solano subject to the Memorandum of Understanding entered into by and between the County of Solano, City of Vallejo, and the Solano County Fair Association on July 11, 2003 (the "*Reversionary Interests*").

A. Upon Existing Project Approvals and the expiration of all appeal periods but, in any event, no later than the first issuance of Project Financing, the City agrees to take all steps necessary to clear title of any reversionary interests, rights of reentry and/or powers of termination it has in the Property as set forth in the 2009 MOU.

B. In the event that no building permit is issued for any portion of the Project within 15 years of the date of recordation of the City's Quit Claim Deed, the County shall grant to the City the Reversionary Interests previously conveyed.

C. This Section 3.02 shall survive the termination of this Development Agreement.

D. Notwithstanding any other clause herein, County shall defend, indemnify and hold City harmless in any claim, complaint or cause of action brought by any person or entity regarding these Reversionary Interests.

ARTICLE 4. ESTABLISHMENT OF SOLANO360 IMPLEMENTATION COMMITTEE

Section 4.01. Solano360 Implementation Committee. Recognizing the guiding role of the Solano360 Committee during the visioning and entitlement phases of the Project, the Parties desire to establish a new oversight committee, named the Solano360 Implementation Committee, which is comprised of two members from the Board of Supervisors and two members from the City Council, to provide policy direction and dispute resolution during the term of this Agreement.

Section 4.02. Scope of Authority. The Solano360 Implementation Committee's purpose is to provide policy oversight to the respective staffs and dispute resolution throughout the implementation of the Project. To the extent that disputes between the Parties regarding the implementation or interpretation of the Solano360 Specific Plan/Master Plan or this Agreement cannot be resolved first at the respective staff level and then between the City Manager and County Administrator, a dispute arising out of this Agreement or an interpretation of it or the Solano360 Specific Plan/Master Plan will be presented to the Solano360 Implementation Committee for resolution.

Section 4.03. Staff Coordination Meetings. City and County staff shall work cooperatively and establish support committees throughout the Project, as needed, comprised of staff members with special expertise as required and available from its

engineering, maintenance, operations, land use planning, economic development, legal, or other technical group to meet regularly to assist in the successful advancement of the Project. These support committees will report to the City Manager and County Administrator, who in turn, will report to the Solano360 Implementation Committee.

Section 4.04. Solano360 Implementation Committee Meetings. At a minimum, the Solano360 Implementation Committee shall meet quarterly during the implementation of Phase 1 of the Solano360 Specific Plan/Master Plan and not less than annually during Solano360 Specific Plan/Master Plan Phases 2 & 3. All such meetings of the Solano360 Committee will be held in accordance with the Brown Act.

ARTICLE 5. DEVELOPMENT OF PROPERTY FOR PUBLIC PURPOSES

Section 5.01. County Approvals for Public Purpose Areas. Public Purpose Areas, as defined by this Agreement and the Solano360 Specific Plan/Master Plan, are not subject to Future Project Administrative Applications or Future Project Discretionary Applications to the extent they are used for public, governmental uses. Any disputes between the Parties regarding whether a proposed use is a “public use” or a “private use,” as defined by this Agreement, shall be addressed pursuant to Article 4 of this Agreement. The County will engage the services of necessary design professionals to prepare the plans and specifications for the Public Purpose Areas in full compliance with applicable County building codes, ordinances and other regulatory authorities. The County’s Department of Resource Management - Building Division will oversee plan review and applicable building and grading permits. The County’s Building Division will ensure compliance, with all applicable laws, codes, ordinances, rules or regulations of affected governmental agencies, such as the City Fire Marshall and Vallejo Sanitation and Flood Control District (“*VSFCD*”), affecting the construction. Except as provided in Section 7.01, Public Purpose Areas and facilities are exempt from any impact fees levied by any public agency, which shall include, but not be limited to, City, County, schools, and VSFCD.

Section 5.02. Future Modifications of Public Purpose Areas. In accordance with California Government Code section 65402, any future modifications to the Public Purpose Areas the Solano360 Specific Plan/Master Plan will be presented to the City for determination of conformity to the City’s General Plan.

ARTICLE 6. DEVELOPMENT OF PROPERTY FOR PRIVATE PURPOSES

Section 6.01. Vested Rights for Expedited Processing. The Private Purpose Areas, as defined by this Agreement or the Solano360 Specific Plan/Master Plan, are subject to this Agreement and shall follow the expedited entitlement process established in the Solano360 Specific Plan/Master Plan and this Agreement. County shall have the vested and expedited right and obligation to develop the Private Purpose Areas within the Project and use the Private Purpose Areas as specified in the Specific Plan/Master Plan and for no other uses or purposes except as otherwise expressly provided in this Agreement and the Solano360 Specific Plan/Master Plan. Developing the Private Purpose Areas, or any portion, shall be undertaken only in compliance with the Project Approvals, Applicable Law, and this Agreement and the Specific Plan/Master Plan. County shall have a vested

and expedited right to develop the Private Purpose Areas on the Property under the Project Approvals, Applicable Law and this Agreement. The Project shall be subject to all Future Project Applications (which, upon final approval, shall be deemed Subsequent Project Approvals, and part of the Project Approvals). The City approves the proposed uses of the Private Purpose Areas, the density and intensity of use, and all other conditions as set forth in the Solano360 Specific Plan/Master Plan and its Design Guidelines (“*Design Guidelines*”).

Section 6.02. Consistency with Specific Plan/Master Plan.

A. For purposes of the Private Purpose Areas, the City’s City Manager, or designee, shall determine consistency with the policies, standards and implementation in the Solano360 Specific Plan/Master Plan. Changes in intensity or density from the permitted uses in the Solano360 Specific Plan/Master Plan will require an amendment to this Agreement. To the extent the County disputes City staff’s determination of consistency with the Solano360 Specific Plan/Master Plan, the determination may be referred to the Solano360 Implementation Committee which shall attempt to resolve the dispute pursuant to Article 4 of this Agreement.

B. In order to maintain the consistency with the Solano360 Specific Plan/Master Plan, all ground leases for Private Purposes Areas will incorporate the design standards of the Solano360 Specific Plan/Master Plan, unless otherwise agreed to in writing in advance by the County and City.

Section 6.03. Integration of Private and Public Purpose Areas. Notwithstanding the designations of Private and Public Purpose Areas, the parties acknowledge that the development of the Project may include the coordination of both public and private uses in a single building. To the extent that such combination of public/private use does occur in a single structure, the parties agree that the City shall exercise land use authority over the private uses but not the public uses. Where a dispute arises as to whether a particular use or mix of uses is private or public, the issue will be referred to the Solano360 Implementation Committee, which shall attempt to resolve the dispute pursuant to Article 4 of this Agreement.

Section 6.04. Applicable Law. In recognition of the extraordinary investment and commitment in planning and engineering developing the Property, the Parties agree that except as otherwise expressly set forth in this Agreement, the rules, regulations, official policies, standards and specifications applicable to developing the Private Purpose Areas shall be those in force and in effect on the Effective Date and as set forth in the Existing Project Approvals, Subsequent Project Approvals and this Agreement, and, regarding matters not addressed by these documents, those laws, rules, regulations, official policies, standards and specifications (including any ordinance, resolution, rule, regulation, standard, official policy, condition, or other measure established by the City (each, individually, a “*City Law*”), governing permitted uses, building locations, timing of construction, densities, design, infrastructure, affordable housing, parks and recreation and heights in force and effect on the Effective Date (collectively, the “*Applicable Law*”), with the exception of regulations which are adopted in conformance with and in furtherance of the

Solano360 Specific Plan/Master Plan, as part of the City's proposed comprehensive General Plan and Zoning Code Update adopted pursuant to Section 6.05(B)(5) below. Prior to the Effective Date of this Agreement, the Parties shall prepare two (2) sets of the Project Approvals and Applicable Laws applicable to the Project as of the Effective Date, one (1) set for City and one (1) set for County, to which shall be added from time to time, Subsequent Project Approvals, so that if it becomes necessary in the future to refer to the Project Approvals or Applicable Law, there will be a common set available to the Parties. Failure to include in the sets of Project Approvals and Applicable Law any rule, regulation, policy, standard or specification within the Applicable Law and Project Approvals as described in this provision shall not affect the applicability of such rule, regulation, policy, standard or specification. Except as otherwise set forth in this Agreement, no amendment or repeal of the Applicable Law shall apply to the Project, and County is vested with the right to develop the Private Purpose Areas under this Agreement and the Applicable Law.

Section 6.05. Reservations of Authority.

A. The parties acknowledge and agree City is restricted in its authority to limit its police power by contract and that the limitations, reservations and exceptions in this Agreement should reserve to City all of its police power which cannot be so limited. This Agreement shall be construed to reserve to City all such power and authority which cannot be restricted by contract.

B. Notwithstanding any other provision to the contrary, the following regulations and provisions shall apply to developing the Private Purpose Areas:

(1) Processing fees and charges of every kind and nature usually and uniformly imposed by City on substantially similar development projects and properties to cover the actual costs to City of processing applications for Project Approvals or for monitoring compliance with any Project Approvals granted or issued, as such fees and charges are adjusted from time to time.

(2) Procedural regulations relating to hearing bodies, petitions, applications, notices, findings, records, hearings, reports, recommendations, appeals and any other matter of procedure, provided such procedures are usually and uniformly applied on a city-wide basis to all substantially similar types of development projects and properties.

(3) Regulations governing construction standards and specifications including building code, plumbing code, mechanical code, electrical code, fire code and grading code, and all other uniform construction codes then adopted, or amended by City at the time of permit application.

(4) New City Laws which may be in conflict with this Agreement or the Project Approvals, but only if such New City Laws are necessary to protect the public health and safety and are uniformly applied on a City-wide basis to all substantially similar types of development projects and properties related to property maintenance standards and nuisance abatement/code enforcement provisions.

(5) New City Laws which are adopted in conformance with and in furtherance of the Solano360 Specific Plan/Master Plan, as part of the City's proposed comprehensive General Plan and Zoning Code Update which are developed in consultation with the County and agreed upon in advance by the County. County acknowledges that City is in the process of updating its General Plan and Zoning Code and agrees to negotiate in good faith with City to adopt New City Laws that shall be in conformance with and in furtherance of the Solano360 Specific Plan/Master Plan.

(6) New City Laws applicable to the Private Purpose Areas, which do not conflict with this Agreement or the Project Approvals, provided such New City Laws are uniformly and usually applied on a City-wide basis to all substantially similar types of development projects and properties.

(7) City Impact Fees or other monetary and non-monetary exactions, connection fees and other fees and exactions of every kind and nature imposed by the City and adopted and in effect as of the Effective Date of this Agreement, with any increases or modifications to those existing City Impact Fees and exactions, provided such increases or modifications are uniformly and usually applied on a City-wide basis to all substantially similar types of development projects and properties subject to a development agreement.

Section 6.06. Regulation by Other Public Agencies. City and County acknowledge and agree that other public agencies not within the control of City possess authority to regulate aspects of developing the Private Purpose Areas separately from or jointly with City, and this Agreement does not limit the authority of such other public agencies. County shall, at the time required by County under County's construction schedule, apply for all such other permits and approvals as required by other governmental or quasi-governmental entities, including without limitation VSFCDC, for the development of, or the provision of services to, the Project. County, or its assignee, shall also pay all required fees when due to such public agencies for the Private Purpose Areas. County acknowledges that City does not control the any such fees. City shall cooperate with County in County's effort to obtain such permits and approvals; provided, however, City shall have no obligation to incur any costs, without compensation or reimbursement, or to amend any City policy, regulation or ordinance in connection therewith.

Section 6.07. Life of Project Approvals. The term of any and all Project Approvals shall automatically be extended for the longer of the Term of this Agreement or the term otherwise applicable to such Project Approvals.

Section 6.08. Vesting Tentative Maps. If this Agreement, or any part of it, is determined by a final judgment to be invalid or unenforceable and a tentative map has been approved for development of the Property as a vesting map as defined under the Subdivision Map Act, Government Code §§ 66410 *et seq.* where it grants a vested right to County for development of the Project, then and to that extent all rights and protections afforded County under the laws and ordinances applicable to vesting maps shall survive.

Section 6.09. County's Right to Rebuild. City agrees that County may renovate or rebuild portions of the Project within the Term of this Agreement should it become necessary due to natural disaster, damage, casualty or changes in seismic requirements. Such renovations or reconstruction shall be processed consistent with Project Approvals. Any such renovation or rebuilding shall be subject to all design, density and other limitations and requirements imposed by this Agreement, and shall comply with the Project Approvals, the building codes existing at the time of such rebuilding or reconstruction, and the requirements of CEQA and other Applicable Law.

Section 6.10. Initiatives and Referenda. If any City Law is enacted or imposed by a citizen-sponsored initiative or referendum, which City Law would conflict with the Project Approvals, Applicable Law or this Agreement or reduce the development rights or assurances provided by this Agreement, such City Law shall not apply to the Private Purpose Areas or Project; provided, however, the Parties acknowledge City's approval of this Agreement is a legislative action subject to referendum. Without limiting the generality of the foregoing, no moratorium or other limitation (whether relating to the rate, timing, phasing or sequencing of development) affecting subdivision maps, building permits or other entitlements to use approved or to be approved, issued or granted by City shall apply to the Project or Private Purpose Areas. County agrees and understands that City does not have authority or jurisdiction over any other public agency's ability to grant governmental approvals or permits or to impose a moratorium or other limitations that may affect the Project. City shall cooperate with County and, at County's expense, shall undertake such actions to ensure this Agreement remains in full force and effect. City, except to submit to vote of the electorate initiatives and referendums required by law to be placed on a ballot, shall not support, adopt or enact any City Law, or take any other action which would violate the express provisions or spirit and intent of this Agreement or the Project Approvals.

Section 6.11. Environmental Mitigation. The Parties understand that the EIR is intended to be used not only for the Existing Project Approvals, but also for the Subsequent Project Approvals needed for the Project. Consistent with the CEQA streamlining policies applicable to specific plans, City and County acknowledge City's obligation, for Subsequent Project Approvals, not to require a supplemental or subsequent EIR, mitigated negative declaration, or negative declaration, unless required by Public Resources Code section 21166, and CEQA section 15161 or 15162. Further, City shall rely on the exemption referenced in CEQA sections 15182 to the fullest extent permitted by law. To the extent supplemental or additional environmental review is required with Subsequent Project Approvals, County may choose to continue in its role as the Lead Agency.

Section 6.12. State and Federal Law. As provided in Government Code section 65869.5, this Agreement shall not preclude the applicability to the Project of changes in laws, regulations, plans or policies, if such changes are mandated and required by changes in State or Federal laws or by changes in laws, regulations, plans or policies of special districts or other governmental entities, other than the City, created or operating under the laws of California ("**Changes in the Law**"). If Changes in the Law prevent or preclude compliance with one or more provisions of this Agreement, the Parties shall meet and confer in good faith to determine whether such provisions of this Agreement shall be

modified or suspended, or performance delayed to comply with Changes in the Law, and City and County shall agree to such action as may be reasonably required. Nothing in this Agreement shall preclude County from contesting by any available means (including administrative or judicial proceedings) the applicability to the Project of any such Changes in the Law.

Section 6.13. American with Disabilities Act (ADA) Compliance. County shall comply with the ADA and all other requirements of applicable Federal and State laws regarding the development of the Project.

Section 6.14. Timing of Development. The parties acknowledge that County or City cannot predict when or the rate at which the Property will be developed. Such decisions depend upon numerous factors which are not within the control of County, such as market orientation and demand, interest rates, absorption, completion of necessary traffic improvements by Caltrans, and other similar factors. Without any limitation of the foregoing, since the California Supreme Court held in *Pardee Construction Co. v. City of Camarillo*, 37 Cal.3d 465 (1984), that the failure of the parties to consider and expressly provide for the timing of development resulted in a later-adopted initiative restricting the timing of development prevailing over such parties' agreement, it is the parties' desire to avoid that result by acknowledging County shall have the vested right to develop the Project in such order and at such rate and at such times as County deems appropriate in exercising its business judgment, subject to the terms, requirements and conditions of the Project Approvals and this Agreement, except that failure to begin any construction of Phase 1 of the Project within 5 years after the issuance of any project financing for the Project shall constitute a default pursuant to Section 13.03 of this Agreement provided all necessary permits have been obtained.

Section 6.15. City Obligations.

A. City's Good Faith in Processing. In consideration of County entering into this Agreement, and provided that County exercises due diligence and good faith and files full, accurate and complete applications with timely payment of all fees, City agrees it will accept, process and review, in good faith and in a timely manner, all applications related to the Project filed by County or those with rights to lease or otherwise obtain rights to develop any portion of the Private Purpose Areas consistent with the Project Approvals, under this Agreement and the Applicable Law. City agrees that the scope of its review of requests for Subsequent Project Approvals shall be exercised consistent with this Agreement and the Applicable Law as such scope of review relates to those services and permits under the City's jurisdiction.

B. City Commitment to Grant or Cooperate to Cause Others to Grant Easements to County. City agrees to assist County and use good faith, diligent efforts to ensure that County is granted all easements and rights of way required to develop the Project during the term of this Agreement, including but not limited to ingress/egress, utilities, demolition/construction, flood control, support, slope, and rights of way, whether from City, or third parties.

C. Acceptance of Public Rights of Way, Roads, Public Improvements, Water, Sewer, Storm Drain and Other Utility Systems and Landscaping. Subject to Section 6.15(E) of this Agreement below, City shall accept dedication of the public rights of way, roads, public improvements, water, sewer, storm drain and other utility systems and landscaping identified in the Project Approvals once completed by County in conformance with City/ VSFCD requirements and standards.

D. Maintenance of Public Rights of Way, Roads, Public Improvements, Water, Sewer, Storm Drain and Other Public Utility Systems and Landscaping. Subject to Section 6.15(E) of this Agreement below, City shall maintain and operate, either directly or by contract, upon acceptance pursuant to Section 6.14(C) above, the public rights of way, roads, public improvements, water, sewer and storm drain systems and other public utility systems and landscaping in public rights of way under Applicable Law in the manner required to serve the Project to be paid from Project revenue and/or permit fees.

E. Irrigation and Landscaping. City will provide non-potable water for landscaping irrigation to the Property, in an amount consistent with County's historical irrigation use of approximately 35 million gallons annually, for the existing golf course and/or public landscaping throughout the Project, subject to a separate agreement between the parties. Maintenance of private on-site landscaping will be provided through a benefit assessment district or at County's option as a performance obligation under the ground lease(s). Upon acceptance of public rights of way, City will provide maintenance to landscaped areas therein.

Section 6.16. Undergrounding of Utilities. All existing above-ground utilities on the Property shall be placed underground, and all new utilities on the Property will be placed underground, at County's sole cost and expense and under City development standards and permits/inspections and the applicable utility companies and to the extent feasible. Notwithstanding the foregoing, County, with approval by the City Building Official, in his or her reasonable discretion, may install temporary above-ground utility installations to the extent necessary to serve the Property.

ARTICLE 7. FEES AND INFRASTRUCTURE IMPROVEMENTS

Section 7.01. Taxes, Assessments, Fees and Exactions.

A. Public Purpose Areas. The Parties agree that the Public Purpose Areas are not subject to nor will the County pay any existing, increased or modified fees, taxes, assessments, impact fees, and other monetary and non-monetary exactions, including Federal/State Compliance Fees, Other Local Agency Compliance Fees, Processing Fees, City Impact Fees, and other City fees as provided in this Section 7.01, excepting those fees related to capacity charges such as sewer or potable water which are in force and effect on a City-wide basis at the time those Water Fees, as defined in Section 7.01B(4) below, are payable at the rates then in effect and only in such amount equal to the net capacity used by the Public Purpose Areas.

B. Private Purpose Areas

(1) The Parties agree that the uses in Private Purpose Areas are subject to any existing, increased or modified fees, taxes, assessments, impact fees, and other monetary and non-monetary exactions, including Federal/State Compliance Fees, Other Local Agency Compliance Fees, Processing Fees, City Impact Fees, Consultant Fees, Architect Fees and other City fees as provided in this Section 7.01. Except for the foregoing fees and except as otherwise provided by this Agreement, City may impose no new impact fees or other monetary or non-monetary exactions on the Project or the uses in Private Purpose Areas except pursuant to a general increase in sales tax or other tax as may be mandated through voter approval. For convenience of reference, the fees, taxes, assessments, impact fees in effect as of the Effective Date are identified on the attached Exhibit G. The parties acknowledge and agree that failure to include any fee, tax, assessment, or impact fee within Exhibit G shall not affect the applicability of such fee, tax, assessment, or impact fee and the parties agree to supplement Exhibit G from time to time to maintain a comprehensive and complete list of such fees, taxes, assessments, and impact fees in effect as of the Effective Date. Notwithstanding the foregoing, the Parties agree to discuss deferral of the timing payment of such fees if requested by a third-party developer.

(2) City may charge and County agrees to pay any new, increased or modified taxes, assessments, impact fees or other monetary and non-monetary exactions, whether imposed as a condition of or for any Subsequent Project Approvals or otherwise, which are uniformly imposed and reasonably necessary to comply with the requirements of any Federal or State statute or regulation enacted or adopted after the Effective Date of this Agreement ("**Federal/State Compliance Fees**").

(3) City may charge and County agrees to pay any new, increased or modified taxes, assessments, impact fees or other monetary and non-monetary exactions, whether imposed as a condition of or for any Subsequent Project Approvals or otherwise, which are uniformly imposed and reasonably necessary to comply with the requirements of other local governmental agencies ("**Other Local Agency Compliance Fees**").

(4) City may charge and County agrees to pay all water connection fees ("**Water Fees**"), which are in force and effect on a City-wide basis at the time those Water Fees are payable at the rates then in effect.

(5) City may charge and County agrees to pay all processing fees, including application and inspection and monitoring fees ("**Processing Fees**"), for land use approvals, grading and building permits, general plan maintenance fees, and other permits and entitlements, which are in force and effect on a City-wide basis at the time those permits, approvals or entitlements are applied for on any or all portions of the Project, and which are intended to cover the actual costs of processing the foregoing.

(6) City may charge and County agrees to pay all City impact fees ("**City Impact Fees**") which are in force and effect as of the Effective Date and any subsequent increases in the same, provided such increases are usually and uniformly applied on a City-wide basis to similar development projects and properties, including,

without limitation, City-assessed park fees allocated to Greater Vallejo Recreation District, transportation impact mitigation fees, property development excise taxes, general plan update fees or other governmental fees such as VSFCDD sewer fees. City represents and warrants that, as of this Agreement City is not planning, considering or contemplating any increase in City Impact Fees, except as otherwise authorized by the City's fee schedule, resolutions or ordinances adopted as of this Agreement. Notwithstanding the above, City Impact fees shall not be charged upon any public use in the Public Purpose Area unless otherwise agreed to by the Parties.

C. Pursuant to Government Code section 51350, City may contract with the County to perform all or any portion of the monitoring, inspection, testing and evaluation services and to process applications, construction and development of the Project by the County Building Official.

ARTICLE 8. SHARING OF REVENUE GENERATED AND COSTS INCURRED

Section 8.01. Project Financing.

A. The Parties intend that the cost of providing City services to the Project, including police, fire, parks and streetscape, landscape and lighting maintenance, repair and replacement, will be fully recouped and/or funded from City taxes and fees generated from the Project. County agrees that City would not enter into this Agreement if City's reasonable costs in providing services were not covered in full by the revenues generated by the Project.

B. The Parties recognize and acknowledge that the Solano360 Specific Plan Public Facilities Financing Plan ("**PFFP**") (Goodwin Consulting Group, November 9, 2012), contemplates and recommends that the County issue six Certificates of Participation ("**COP**") and three Capital Appreciation Bonds ("**CAB**") to fund the Public Use Area share of facilities and infrastructure and to initially fund a portion of the Private Use Area share of facilities.

The Parties recognize and acknowledge that other debt financing mechanisms in lieu of COP and CAB may be utilized during the build-out of the Plan Area. The Parties intend that any other debt financing mechanism would involve interest rates, terms and other provisions substantially similar to what would have been involved in a COP or CAB issuance.

C. Together, COP and CAB and other similar financing mechanisms are defined as Project Financing ("**Project Financing**"). All such Project Financing must be reasonably related to development of the Plan Area. The Parties recognize and acknowledge that the PFFP is non-binding, representing one option for financing the Project, and that other financing mechanisms may be determined to be appropriate when financing is required to implement the Project. The Parties agree that a portion of the Project Financing will also fund oversizing for infrastructure required for the Public Use Area the Private Use Area of the Plan Area and this cost for oversizing infrastructure will be reimbursed at a future time when other financing is in place, as set forth in Section 8.03 below.

D. “**County Annual Project Net Revenue**” is defined as all new annual County General Fund revenue and ground lease revenue resulting from developing the Plan Area, less new annual County operating expenses incurred for public services related to the Plan Area, plus new annual Fair net revenue resulting from development of the Plan Area, as described, for information purposes only and to inform the County and City as part of the Existing Approvals, in the Solano360 Specific Plan Fiscal Impact Analysis (“**FIA**”) (Goodwin Consulting Group, November 9, 2012).

E. “**City Annual Project Gross Revenue**” is defined as all new annual City General Fund revenue resulting from developing the Plan Area, including Property Tax, Sales Tax, Utility Users Tax, Transient Occupancy Tax, and any other source of revenue actually received and related to the Plan Area as described in the FIA, for information purposes only and to inform the County and City as part of the Existing Approvals.

F. The Parties acknowledge that the FIA assumes that each Party will retain a project manager to facilitate implementation of the Project and this Agreement. It is the City’s intent to not retain the City’s project manager until the end of Phase 1.

G. The Parties acknowledge that the FIA assumes, and this Agreement confirms, that the City will maintain the public roads, right-of-way landscaping and signals proposed to be installed in the second year of the Project, in Phase 1a. County agrees to require any contractors who construct such improvements to provide a one-year maintenance warranty, to eliminate the City’s obligation to maintain, and cost to maintain, such improvements for the duration of the one-year warranty period.

H. “**Debt Service**” is defined as the principal and interest payments due on Project Financing issued by the County as described in this Article.

I. The Parties agree that County Annual Project Net Revenue and City Annual Revenue Sharing as defined in Section 8.02 below, are intended to be pledged as Debt Service on Project Financing issued for the Project and that the highest priority for County Annual Project Net Revenue and City Annual Revenue Sharing from the Plan Area will be for servicing the debt on any outstanding Project Financing.

J. Except as provided in Section 8.05 of this Agreement, City makes no commitment to issue debt, or be liable to a third party, including a Certificate Holder on any Project Financing issued by the County, for debt service payments.

K. During Phase 1 of the Project, as such phase is described in the Solano360 Specific Plan/Master Plan, County shall issue Project Financing, estimated in the PFFP to be \$40,600,000 in 2012 dollars, with terms and interest rates similar to those described in the PFFP, but in an amount to be determined by the Parties at the time of issuance to complete the Phase 1 facilities and infrastructure contemplated in the PFFP to be funded by the Project Financing (“**Phase 1 Project Financing**”).

L. County shall pay all Project Financing Debt Service payments in a timely manner and on or before the date Project Financing Debt Service payments are due to the trustee of the Project Financing.

M. In the event that development of the Private Use Area or that Project Financing does not proceed as contemplated in the PFFP and this Agreement, the Solano360 Implementation Committee may consider County or City proposals regarding Project Financing and provisions of this Article, and may recommend amendments to the Agreement.

Section 8.02. City Annual Revenue Sharing Payment

A. City shall provide an annual payment to County representing City’s contribution to the Project, including City’s contribution toward infrastructure improvements and reimbursement to County for City’s one-half share of County’s entitlement costs as stated in the 2009 MOU (“*City Annual Revenue Sharing Payment*”) as defined in this Section 8.02.

B. The City Annual Revenue Sharing Payment shall be calculated as a percentage of the City’s receipt of Secured and Unsecured Property Tax, Property Tax in Lieu of Vehicle License Fee (“VLF”), Sales Tax (exclusive of “*Measure B*”), Utilities User’s Tax and Business License Tax generated from the Plan Area each fiscal year through Fiscal Year (FY) 2062-62. These five revenue sources constitute the “*Available City Revenue*”. The percentage of Annual City Revenue will change throughout the term of the Project and has been calculated based on the projected revenue in the FIA. In the event that State legislation changes the formula by which any component of Available City Revenue is calculated, or for any other reason such formulas are changed, the parties agree to adopt alternate methodologies that will result in City Annual Revenue Sharing Payments similar to those that would occur under formulas in effect as of the date of this Agreement.

The parties agree that the percentage used in each fiscal year to calculate the City Annual Revenue Sharing Payment will be as follows:

Fiscal Years	CITY ANNUAL REVENUE SHARING PERCENTAGE OF AVAILABLE CITY REVENUES
2013-14 through 2015-16	63%
2016-17 through 2021-22	97%
2022-23 through 2042-43	55%
2043-44 through 2062-63	24%

C. An estimate of the City Annual Revenue Sharing Payment (*“Estimated City Annual Revenue Sharing Payment”*) and the annual revenue sharing percentage by fiscal year is included in Exhibit C for the purpose of calculating Advance Payments as described in this Article 8.

D. . To the extent Measure B is renewed or any subsequent City fee or tax is adopted that results in a new source of revenue from the Plan Area, the parties agree that the intent of this Agreement is that the percentages in Section 8.02B above will apply to the new source of City revenue and agree to meet and confer regarding any necessary revision to the definition of Available City Revenue

E. City shall make two advance payments (*“Advance Payments”*) toward the City Annual Revenue Sharing Payment during each fiscal year. One-half of the Estimated City Annual Revenue Sharing Payment shall be due on December 31 of the applicable fiscal year, and one-half of the Estimated City Annual Revenue Sharing Payment shall be due on June 30 of the applicable fiscal year.

F. Not later than 180 days following the end of a fiscal year, City shall provide to County a calculation of the prior year’s City Annual Revenue Sharing Payment as specified in Section 8.02B. Within sixty days of receipt of the calculation of the City Annual Revenue Sharing Payment, County shall provide written acceptance or rejection of the calculation of the City Annual Revenue Sharing Payment. If so rejected, the matter shall be referred to the Solano360 Implementation Committee, whose decision shall be final.

G. Within thirty days of the County’s acceptance of the calculation of the City Annual Revenue Sharing Payment, or within thirty days of a final decision by the Solano360 Implementation Committee, the City shall provide payment to County for any amount due for the prior year, or the County shall provide payment to City for any amount overpaid for the prior fiscal year.

Section 8.03. Project Financing - Phase 2 and Phase 3

A. County shall inform City whenever it begins a process to issue a Project Financing in Phase 2 and Phase 3 of the Project (or for any Project Financing issued after the Phase 1 Project Financing described in Section 8.01). If, prior to issuance of a Phase 2 or Phase 3 Project Financing, City Annual Revenue Sharing Payments or County Annual Project Net Revenue, as defined in this Agreement, are estimated to be insufficient to pay Phase 2 or Phase 3 Project Financing Debt Service, the Parties agree to review the proposed financing and negotiate, prior to the issuance of any new Project Financing, how the Debt Service will be paid by the Parties, which may include all or any combination of the following, or other mechanisms mutually agreed to by the Parties: revise City Annual Revenue Sharing Payment percentages, secure other sources of funding for the facilities and/or infrastructure proposed to be financed by Project Financing, or pledge other revenue sources to pay Debt Service. The mechanism for financing and payment of Debt Service

and/or City Annual Revenue Sharing Payments will be agreed upon in advance of the issuance of Phase 2 or Phase 3 Project Financing.

B. Prior to issuance of additional Project Financing after Phase 1, the parties shall review actual cash flow and updated market conditions and confirm revenue sharing ratios. Prior to Phase 2 commencing, the Parties agree to review market conditions, including current land uses defined in the Solano360 Specific Plan/Master Plan and updated financial analyses, to secure the most feasible and prudent debt financing for facilities and infrastructure, which may trigger the need for the Parties to review, confirm and agree to revised debt and revenue sharing prior to issuance of new Project Financing.

C. Prior to Phase 2, the Parties agree they will work cooperatively and support each other to seek grant and other funding sources to fund the offsite improvements to enable the Solano360 Project to proceed as anticipated in the Solano360 Specific Plan/Master Plan, and to improve traffic circulation for the neighboring area.

D. Any Project Financing proposed to be issued with Debt Service payments beyond the initial term (50 years) of this Agreement must be mutually agreed upon in writing by the Parties.

Section 8.04. City Reimbursement of a Proportionate Share of County Entitlement Cost

A. Beginning in fiscal year 2008/09 and continuing through fiscal year 2012/13, County incurred certain costs in the planning, entitlement and development of the Project (“**County Entitlement Cost**”). City and County agree that the County Entitlement Cost, including interest at 5% per annum on the prior years’ cumulative costs, is \$4,265,000 as of June 30, 2013.

B. It is the intent of this Agreement that the City Annual Revenue Sharing Payments are calculated to include City’s reimbursement to County for one-half of the County Entitlement Cost, as stated in the 2009 MOU (\$2,132,500).

Section 8.05. Community Facilities District Financing

A. City and County will confer to determine which Party shall issue conduit debt financing through Mello-Roos Community Facilities District (“**CFD**”) bonds, or other similar conduit financing mechanisms, to finance the Private Area share of facilities and infrastructure. Under a CFD, special taxes would be collected from the Private Use Area parcels and shared public parking parcels to pay Debt Service on the CFD bonds and to reimburse the County, or to reduce the Debt Service on the portion of the Project Financing that is used to fund oversizing for infrastructure required for the Private Use Area of the Plan Area.

B. City, County or an alternate entity (“*Issuing Party*”) may establish an infrastructure financing CFD under the Mello-Roos Act on its own, acting under its own

authority under the Mello-Roos Act to finance all or a portion of the costs of designing and constructing the Infrastructure Improvements or any other improvement which may lawfully be financed under the Mello-Roos Act and other applicable law, subject to (i) the Issuing Party reserves full and complete discretion regarding legally required findings that must be made with formation of any such proposed infrastructure financing CFD, (ii) nothing in this Agreement is intended to or shall limit the Issuing Party's ability to adopt legally required findings regarding formation of a proposed CFD, and (iii) nothing in this Agreement is intended to or shall prejudice or commit to the Issuing Party regarding the findings and determinations to be made with respect thereto. County shall inform City whenever a CFD bond is required to fund the Private Area share of facilities and infrastructure. City shall use its best effort to issue CFD bonds, and shall consult with County on the terms of CFD bonds. If City or County determine, based on the opinion of qualified Bond Counsel, Financial Advisor, and/or Underwriter that City issuance of CFD bonds is infeasible or not advisable, the Parties agree to work cooperatively to form a CFD and issue the CFD bonds through an alternative entity, such as the Association of Bay Area Governments or a Joint Powers Authority.

C. County and City recognize that other conduit debt financing mechanisms may be utilized during the build-out of the Plan Area, and the principles for conduit financing described shall apply to any conduit debt financing mechanism that may be utilized.

D. The Parties recognize and acknowledge security for CFD financing is the Private Use Area parcels, including any leasehold interest, and not the full faith and credit of County and City, and any other conduit financing should have similar property-based recourse and security.

E. City and County recognize that CFD bonds or other types of conduit bonds may not need to be issued if Private Area developer(s) elect not to utilize such financing and other sources of private financing are instead secured.

Section 8.06. Reporting and Auditing.

The Parties shall make their records available upon reasonable notice and report annually to the Solano360 Implementation Committee regarding the status of Performance of Article 8 of this Agreement.

Section 8.07 Severability.

If any term, provision, covenant, or condition of this Agreement is held by a court of competent jurisdiction to be invalid, void, or unenforceable or the subject of a referendum, the Parties' financial obligations shall continue in full force and effect until the County has been fully reimbursed.

ARTICLE 9. DEVELOPMENT STANDARDS AND REQUIREMENTS

Section 9.01. Compliance with State and Federal Law. County, at its sole cost and expense, shall comply with requirements of, and obtain all permits and approvals required by local, State and Federal agencies having jurisdiction over the Project.

Section 9.02. Construction of the Project. It is the County's intent to commence and complete the construction and installation of the Infrastructure Improvements within the time in the Project Infrastructure Schedule attached as Exhibit D. Such construction is dependent upon the development of Private Purpose Areas and therefore, shall commence in accordance with the availability of private financing. For this Section 9.02, the term "commence construction" means commencement of construction of the Project building foundation or the Infrastructure Improvements. City and County staff shall have regular meetings during the term of this Agreement to discuss the progress of the development and construction of the Project as set forth in Section 4.03 above. Such meetings shall be attended by representatives of the Parties with experience and expertise in the relevant disciplines to the stage of the development and construction process.

Section 9.03. Prevailing Wage Requirements.

A. County agrees that development of the Public Purpose Areas, including the Infrastructure Improvements will constitute construction, alteration, demolition, installation, or repair work done under contract and paid for in whole or in part out of public funds under California Labor Code Section 1720(a) and (b)(3). County shall comply and shall ensure its contractors and subcontractors comply with all California Labor Code requirements, including implementing regulations of the Department of Industrial Relations, applicable to public works and payment of prevailing wages, and all Federal prevailing wage laws, including the Davis-Bacon Act of 1931, as amended, and implementing regulations (collectively, "***Prevailing Wage Laws***") for construction and development of the Project. Without limiting the generality of the foregoing, County shall (i) require its contractors and subcontractors to submit certified copies of payroll records to County; (ii) maintain complete copies of such certified payroll records; and (iii) make such records available to City and its designees for inspection and copying during regular business hours at the County.

B. County shall defend, indemnify and hold harmless City and its officers, employees, volunteers, agents and representatives against any and all present and future Claims, arising out of or connected with County's obligation to comply with all Prevailing Wage Laws for construction of the Project and the Infrastructure Improvements, including all Claims that may be made by contractors, subcontractors or other third party claimants under Labor Code sections 1726 and 1781.

Section 9.04. Leadership in Energy and Environmental Design. The Leadership in Energy and Environmental Design ("***LEED***") Green Building Rating System is the nationally accepted benchmark for the design, construction, and operation of high performance green buildings. County shall use commercially reasonable efforts to design the Project in a manner to meet and achieve LEED "platinum" certification. County shall

submit a complete application for LEED certification of the Project within sixty (60) days following completion of construction and provide copies of any and all LEED certifications to City within ten (10) business days following receipt. If County utilizes commercially reasonable efforts but cannot design the Project to meet LEED "platinum" certification or otherwise fails to receive LEED platinum certification, City staff shall inform the City Council of such failure and County shall utilize commercially reasonable efforts to meet and achieve LEED "gold" certification. Notwithstanding the foregoing, if County utilizes commercially reasonable efforts but cannot design the Project to meet LEED "gold" certification or otherwise fails to receive LEED gold certification, County shall not be in Default under this Agreement, and shall be permitted to proceed with all design, construction and operation of all such buildings.

Section 9.05. Periodic Review.

A. The annual review date for this Agreement shall be conducted by the City Manager, or designee, and initiated during the month of January of each year of the Term of this Agreement, commencing with January, 2014 in accordance with Government Code section 65865.1 and Chapter 17.20 of the Vallejo Municipal Code. County shall provide all documentation necessary to effectuate an annual review. If the City Manager, or designee, requests any documentation from County in furtherance of an annual review, County shall provide such documentation within ten (10) days of County's receipt of such request, unless otherwise mutually agreed upon by City and County.

B. The City Manager, or designee, shall provide County with notice of the annual review no less than twenty (20) days prior to the City Manager's, or designee, anticipated commencement date of the annual review. If the City Manager, or designee, finds good faith compliance by County with this Agreement, the City Manager, or designee, shall so notify County and the Solano360 Implementation Committee in writing.

C. The City Manager, or designee, shall provide written notice to the County if the County is not performing under the material terms and conditions of this Agreement, or if the City Manager, or designee, has any reasonable doubts concerning County's performance. The City Manager, or designee, shall first meet and confer with County to discuss the matter and attempt resolution of the dissatisfaction or doubts that the City Manager, or designee, may have. If, after meeting and conferring with County, the City Manager, or designee, still has reasonable doubts concerning County's performance, the City Manager, or designee, shall refer the matter to the Solano360 Implementation Committee for review.

D. If after review and consideration by the Solano360 Implementation Committee, the City Manager remains in reasonable doubt regarding the County performing under the material terms and conditions of this Agreement, the City Manager shall present the review to the City Council. The City Council shall notify County in writing of its intention to conduct a hearing on whether County has complied in good faith with the material terms and conditions of this Agreement and whether the Agreement should be modified or terminated. The notice shall include the time and place of the hearing, a copy of the Solano360 Implementation Committee's recommendation, and any

other information the City Council considers necessary to inform County of the proceeding. County shall be given an opportunity to submit evidence and to be heard. If the City Council determines that County has complied in good faith with the material terms and conditions of this Agreement, the review for that period shall be concluded. If, however, the City Council determines, based upon substantial evidence in the record, there are significant questions whether County has complied in good faith with the material terms and conditions of this Agreement, the City Council may continue the hearing and shall notify County of the City's intent to meet and confer with County within thirty (30) days of such determination, prior to taking further action. Following such meeting, the City Council shall resume the hearing to further consider the matter and to make a determination, regarding County's good faith compliance with the material terms and conditions of the Agreement and to take those actions it deems appropriate, including but not limited to, modification or termination of this Agreement, under California Government Code section 65865.1 and Vallejo Municipal Code Chapter 17.22.

E. Failure of City to conduct an annual review shall not constitute a waiver by the City of its rights to otherwise enforce this Agreement nor shall County have or assert any defense to such enforcement by any such failure to conduct an annual review.

ARTICLE 10. AMENDMENT OF AGREEMENT AND EXISTING PROJECT APPROVALS

Section 10.01. Amendment of Agreement By Mutual Consent. This Agreement may be amended in writing from time to time by mutual written consent of the Parties or their successors-in-interest or assigns and under Vallejo Municipal Code Chapter 17.10.

Section 10.02. Requirement for Writing. No modification, amendment or other change to this Agreement or any provision shall be effective for any purpose unless set forth in a writing which refers expressly to this Agreement and is signed by duly authorized representatives of both Parties and/or successors.

Section 10.03. Amendments to Development Agreement Statute. This Agreement has been entered into in reliance upon the Development Agreement Statute as those provisions existed at the date of execution of this Agreement. No amendment or addition to those provisions which would materially affect the interpretation or enforceability of this Agreement shall apply to this Agreement, unless such amendment or addition is required by the California State Legislature, or is mandated by a court of competent jurisdiction. If such amendment or change is permissive (as opposed to mandatory), this Agreement shall not be affected by same unless the parties mutually agree in writing to amend this Agreement to permit such applicability.

ARTICLE 11. COOPERATION AND IMPLEMENTATION

Section 11.01. Future Project Applications. County and City acknowledge and agree that County intends to submit Future Project Applications. For any Future Project Application, the City shall exercise its discretion under Applicable Law, the Project Approvals and this Agreement, including the reservations of authority in Section 6.05.

County acknowledges that City's approval of and entering into this Agreement does not constitute City's approval of County's applications for Future Project Applications.

A. Future ministerial applications are permits or approvals that are required by Applicable Law and that are to be issued upon compliance with uniform, objective standards and regulations. They include applications for road construction permits or authorizations; grading and excavation permits; demolition permits; building permits, including electrical, plumbing, mechanical, Title 24 Electrical, and Title 24 Handicap permits or approvals; certificates of occupancy; encroachment permits; and other similar permits required for the development and operation of the Project. City agrees to use its best efforts to expedite processing of future ministerial applications to the extent the submittals are in substantial accordance as envisioned in the Solano360 Specific Plan/Master Plan and pursuant to requirements of the VMC and Applicable Law.

Section 11.02. Processing Applications for Future Project Applications.

A. County acknowledges that City cannot begin processing Future Project Applications until County submits complete applications on a timely basis. County shall use diligent, good faith efforts to (i) provide to City in a timely manner any and all documents, applications, plans, and other information necessary for City to carry out its obligations; and (ii) cause County's planners, engineers, and all other consultants to provide to City in a timely manner all such documents, applications, plans and other materials required under Applicable Law. It is the express intent of County and City to cooperate and diligently work to obtain any and all Future Project Applications.

B. Upon submission by County of all appropriate applications and processing fees for any pending Future Project Application, City shall, to the full extent allowed by law, promptly and diligently, subject to City ordinances, policies and procedures regarding hiring and contracting, commence and complete all steps necessary to act on County's pending Future Project Applications including: (i) expedited planning and processing of each pending Future Project Applications; (ii) if legally required, providing notice and holding public hearings; and (iii) acting on any such pending Future Project Application.

C. With the Existing Project Approvals, City has made a final policy decision that the Project is in the best interests of the public health, safety and general welfare. Applications for Future Ministerial Applications consistent with this Agreement and the Existing Project Approvals shall be processed and considered in a manner consistent with the vested rights granted by this Agreement and shall be deemed tools to implement those final policy decisions, and shall be approved by City so long as they are consistent with this Agreement, Existing Project Approvals, and Applicable Law.

D. Nothing shall limit the ability of City to require the necessary reports, analysis or studies to assist in determining the requested Future Ministerial Application is consistent with this Agreement and the Existing Project Approvals. If the City reasonably determines a Future Ministerial Application is not consistent with this Agreement or the Existing Project Approvals and should be processed as a Future Discretionary Application

rather than a Future Ministerial Application, the City shall notify the County in writing. County shall then either modify the application to conform to this Agreement and the Existing Project Approvals or process the application as a Future Discretionary Application.

Section 11.03. Other Agency Subsequent Project Approvals; Authority of City.

A. City shall cooperate with County, to the extent appropriate and as permitted by law, in County's efforts to obtain, as required, approvals on Other Agency Future Project Applications.

B. Notwithstanding the issuance to County of Other Agency Subsequent Project Approvals, County agrees that City shall have the right to review, modify, approve and/or reject any and all submissions subject to the Other Agency Future Project Applications which, but for the authority of the other governmental or quasi-governmental entities to approve the Other Agency Future Project Applications, would otherwise require City approval.

C. To the extent that other public agencies having jurisdiction over the Project require modifications to the Solano360 Specific Plan/Master Plan for purposes of implementing or constructing any improvement, the Parties agree to work cooperatively to seek such necessary approvals to effectuate the intent of this Agreement.

Section 11.04. Vallejo Sanitation and Flood Control District. VSFCD provides sanitary sewer and flood control services for the Project. County, at its sole expense, shall work cooperatively with VSFCD to provide sanitary and flood control services needed in whole or in part to serve the Project or to mitigate the impacts and to pay its fair share of the costs of such improvements in accordance with the Solano360 Specific Plan/Master Plan and the EIR.

Section 11.05. Utilities. Pacific Gas and Electric is a private utility company providing electricity and natural gas services under a franchise with the City. County shall, at its sole expense, work with Pacific Gas and Electric and/or other applicable utility provider(s), to provide all electricity and natural gas utilities needed to serve the Project.

Section 11.06. Implementation of Necessary Mitigation Measures. The County has adopted certain mitigation measures and approved a MMRP for the Project. County shall, at its sole cost and expense, comply with all MMRP requirements applicable to the Project, and Infrastructure Improvements.

ARTICLE 12. ASSIGNMENT, TRANSFER AND NOTICE

Section 12.01. Assignment. To the extent that County enters into an agreement with a developer(s) to construct the horizontal and/or vertical developmental of Project as envisioned in the Solano360 Specific Plan/Master Plan, the County may request that all or portion of this Agreement relating to the Private Purpose Areas is assigned or transferred to the developer(s) ("*Assignee*"). The transfer or assignment of any right or interest of the Private Purpose Area shall be made only after consultation with the Solano360 Implementation Committee.

Section 12.02. Conditional Release of Transferring Party. Notwithstanding any transfer or assignment of its obligations under this Agreement, County shall continue to be obligated under this Agreement as to all or the portion so transferred unless City in its sole discretion is satisfied that the Assignee is fully able to comply with County's obligations under this Agreement (both financially and otherwise) and County is given a release in writing. County shall provide to City all information reasonably necessary for City to determine the financial and other capabilities of Assignee. City shall, execute such consent only if:

A. County is not then in Default under this Agreement and has received no notice of breach under Section 13.01.

B. County has provided City with notice and the fully executed assignment and assumption agreement.

C. Assignee provides City with security necessary to secure performance of its obligations, including the Infrastructure Improvements.

ARTICLE 13. DEFAULT; REMEDIES; TERMINATION

Section 13.01. Breach; Default.

A. Subject to extensions of time under Section 13.04 or by mutual consent in writing, the failure or delay by either Party to perform any term or provision shall constitute a breach of this Agreement. If alleged breach of this Agreement occurs, the Party alleging such breach shall give the other Party notice in writing specifying the breach and the manner in which said breach or default may be satisfactorily cured, and the Party in breach shall have sixty (60) days following such notice to cure such breach, except that if a breach of an obligation to make a payment occurs, the Party in breach shall have thirty (30) days to cure the breach. If the breach is of a type that cannot be cured within sixty (60) days, the breaching Party shall not be in Default (defined below) provided the breaching Party (i) commences to cure the breach within such 60-day period, (ii) notifies the non-breaching Party within the initial 60-day period of the time it will take to cure such breach which shall be a reasonable period under the circumstances, and (iii) diligently and continuously prosecutes such cure to completion. If the breaching Party has not cured the breach or default within the applicable time specified above, such Party shall be in default ("**Default**"), and the non-breaching Party, at its option, may terminate the Agreement, institute legal proceedings pursuant to this Agreement and shall have such remedies as are set forth in Section 13.03 below.

B. Other circumstances constituting a Default shall be:

(1) The discovery that a representation or warranty made by either of the Parties proves to have been incorrect when made;

(2) Either Party being made the subject of an order for relief by a bankruptcy court, or is unable or admits its inability to pay its debts as they mature, or makes an assignment for the benefit of creditors;

(3) Sale or other transfer of the Property or the Project, or any portion thereof, by the County, without compliance with the applicable provisions of this Agreement;

(4) Issuance of debt by the County beyond what is contemplated by this Agreement or without compliance with the applicable provisions of this Agreement;

(5) Failure by the County to begin construction of Phase 1 of the project within 20 years of the Effective Date unless a later time is agreed to in writing by City;

(6) Failure of either Party to pay proportionate share of Debt Service after reasonable notice and opportunity to cure.

C. In the event of the occurrence of a Default listed in Section B above, no additional cure period shall be provided and the non-breaching Party shall be excused from further performance under this Agreement and may, at its option, terminate the Agreement, institute legal proceedings pursuant to this Agreement and shall have such remedies as are set forth in Section 13.03 below, or any remedy at law or in equity.

Section 13.02. Withholding of Permits. If a Default occurs by County, City shall have the right to refuse to issue any approvals on Future Project Applications. This provision is besides and in addition to any other allowable actions that City may take to enforce the conditions of the Project Approvals.

Section 13.03. Remedies.

A. If County is in Default in its performance, City shall provide notice and an opportunity to cure pursuant to Section 13.01 of this Agreement. If the City wishes to terminate the Agreement, the City shall notify the County of its intent to terminate under Government Code § 65868 and the VMC.

B. City and County agree that if Default occurs by either Party, the Parties intend that the primary remedy shall be specific performance of this Agreement. A claim by a Party for actual monetary damages may only be considered if specific performance is not granted by the Court.

C. Besides any other rights or remedies, either Party may institute legal action to cure, correct or remedy any Default, to enforce any covenants or agreements, to enjoin any threatened or attempted violation, or to obtain any other remedies consistent with the purpose of this Agreement. Any such legal action shall be brought in the Superior Court for Solano County, California.

Section 13.04. Enforced Delay; Extension of Time of Performance. Subject to the limitations set forth below, performance by either party shall not be deemed in default, and all performance and other dates specified in this Agreement shall be extended, where delays are due to: war; insurrection; strikes; lockouts; riots; floods; earthquakes; fires; casualties; acts of God; acts of terrorism; acts of the public enemy; epidemics; quarantine restrictions;

freight embargoes; governmental restrictions or priority; litigation; unusually severe weather; acts or omissions of the other Party; or acts or failures to act of any other public or governmental agency or entity (other than the acts or failures to act of City which shall not excuse performance by City). An extension of time for any such cause shall be for the period of the enforced delay and shall run from the time of the commencement of the cause (but shall not exceed a cumulative total of two (2) years), if notice by the party claiming such extension is sent to the other party within thirty (30) days of the commencement of the cause. The Parties agree that the commencement of any litigation concerning this Agreement, the Ordinance approving this Agreement or the Existing Project Approvals shall constitute cause for an extension of time for performance of obligations under this Agreement up to a maximum of two (2) years, and that the Initial Term of this Agreement shall be automatically extended for the period such litigation is pending (subject, however, to the two (2) year maximum extension).

Section 13.05. Resolution of Disputes. Regarding any dispute involving the Project, resolution of which is not provided for by this Agreement or Applicable Law, County shall, at City's request, meet with City. The parties to any such meetings shall attempt in good faith to resolve any such disputes. Nothing in this Section 13.05 shall be interpreted as requiring County and City and/or City's designee agree regarding those matters being addressed, nor shall the outcome of these meetings be binding on City or County unless expressly agreed to by the parties to such meetings.

ARTICLE 14. INDEMNITY AND INSURANCE

Section 14.01. County to indemnify City.

A. County agrees to defend, indemnify, and hold harmless the City of Vallejo, its officers, agents, employees, and independent contractors (collectively, "*City Indemnitees*") from any and all claims, actions or proceedings brought against any of the foregoing individuals or entities, seeking to attack, set aside, void or annul any Existing Project Approvals, Future Project Approvals, or related decision, or the processing or adoption of the EIR which relate to the approvals. This indemnification shall include, but is not limited to, all damages, costs, expenses, attorney fees or expert witness fees that may be awarded to the prevailing party arising out of or in connection with the approval(s) of the application or any related decisions, whether or not there is concurrent, passive or active negligence on the part of the City Indemnitees.

B. County shall also indemnify and hold the City Indemnitees free and harmless from any liability whatsoever, (i) based or asserted upon any negligent or intentional act or omission of the County, its officers, agents, employees, subcontractors and independent contractors, for property damage, bodily injury, or death (the Developer's employees included) or any other element of damage of any kind or nature, relating to or in any way connected with the Property or arising from the activities contemplated under this Agreement, save and except claims for damages arising through the gross negligence or willful misconduct of the City, or (ii) arising out of an event of Default by County. The County shall defend, at its expense, including attorneys' fees, the City Indemnitees in any legal action based upon such alleged acts or omissions.

C. County shall also shall indemnify and hold the City, its officers, agents, and employees free and harmless from any liability, based or asserted, upon any act or omission of the County, its officers, agents, employees, subcontractors and independent contractors for any violation of any federal, state or local law, ordinance or regulation relating to hazardous or toxic materials, industrial hygiene, or environmental conditions created by the County or its officers, agents, employees, contractors, subcontractors and independent contractors on, under or about the Property, including, but not limited to, soil and groundwater conditions, and the County shall defend, at its expense, including attorneys' fees, the City, its officers, agents and employees in any action based or asserted upon any such alleged act or omission.

D. City agrees to act reasonably to promptly notify the County of any claim, action, or proceeding arising out of County's obligations pursuant to this Section 14.01 and provide County access to City records and/or files to defend against such claims, actions or proceedings. At its sole discretion, the City may participate at its own expense in the defense of any such claim, action or proceeding, but such participation shall not relieve the County of any obligation imposed by this Section.

Section 14.02. City to Indemnify County. City agrees to defend and indemnify the County, its agents, officers and employees (collectively referred to in this paragraph as "**County Indemnitees**") from any claim, action or proceeding against the County Indemnitees, arising out of the acts or omissions of the City in the performance of this Agreement or (ii) arising out of an event of Default by City. At its sole discretion, the County may participate at its own expense in the defense of any such claim, action or proceeding, but such participation shall not relieve the City of any obligation imposed by this Section. The County shall notify the City promptly of any known claim, action or proceeding and cooperate fully in the defense. For purposes of this Section, City's performance under this Agreement includes Future Project Applications or related decisions, but only to the extent the City Council unilaterally modifies or imposes additional conditions on County-initiated applications prior to approving Future Project Application, after the County has protested the modification or condition in writing.

Section 14.03. Insurance Requirements.

A. During any period of construction of public improvements within the City right-of-way, the County shall procure and maintain, or cause its contractor(s) to procure and maintain, for the duration of this Agreement commercial general liability insurance, workers compensation, and other types of insurance in limit amounts and on such forms that may be required by the City in its reasonable discretion and then commonly available in the commercial insurance marketplace. County's insurance shall be placed with insurers with a current A.M. Best's rating of no less than A-:VII or a rating otherwise approved by the County Counsel in his or her sole discretion. County shall furnish at City's request appropriate certificate(s) of insurance evidencing the insurance coverage required by City, and the City Parties shall be named as additional insured parties under the required policies. The certificate of insurance shall contain a statement of obligation on the part of the carrier to notify City of any material change, cancellation or termination of the coverage at least thirty (30) days in advance of the effective date of any such material change,

cancellation or termination (ten (10) days advance notice in the case of cancellation for nonpayment of premiums) where the insurance carrier provides such notice to the County. Coverage provided by the County shall be primary insurance and shall not be contributing with any insurance, self-insurance or joint self-insurance maintained by City, and the policy shall contain such an endorsement. The insurance policy or the endorsement shall contain a waiver of subrogation for the benefit of City and its insurers, if such waiver is available in the commercial insurance marketplace. County shall include or cause its contractor(s) to include all subcontractors as insureds under its policies or shall furnish separate certificates and endorsements for each contractor and subcontractor. All coverage for contractors and subcontractors shall be subject to all the requirements stated above.

ARTICLE 15. MISCELLANEOUS PROVISIONS

Section 15.01. Incorporation of Recitals, Exhibits and Introductory Paragraph. The Recitals in this Agreement, the introductory paragraph preceding the Recitals and the attached Exhibits are incorporated into this Agreement as if fully set forth.

Section 15.02. Severability. If any term or provision, or applying any term or provision to a situation, is held by a court of competent jurisdiction to be invalid, void or unenforceable, the remaining terms and provisions of this Agreement, or applying this Agreement to other situations, shall continue in full force and effect unless amended or modified by mutual consent of the Parties. Notwithstanding the foregoing, if any material provision, or applying such provision to a situation, is held to be invalid, void or unenforceable, the Party adversely affected may (in its sole and absolute discretion) terminate this Agreement by providing written notice of such termination to the other Party.

Section 15.03. Construction. Each reference in this Agreement to this Agreement or the Existing Project Approvals or Subsequent Project Approvals shall be deemed to refer to the Agreement, Existing Project Approvals or Subsequent Project Approval as it may be amended from time to time, whether or not the reference refers to such possible amendment. Section headings in this Agreement are for convenience only and are not intended to be used in interpreting or construing the terms, covenants or conditions of this Agreement. This Agreement has been reviewed and revised by legal counsel for both City and County, and no presumption or rule that ambiguities shall be construed against the drafting party shall apply to the interpretation or enforcement of this Agreement. Unless the context clearly requires otherwise, (i) the plural and singular numbers shall each be deemed to include the other; (ii) the masculine, feminine, and neuter genders shall each be deemed to include the others; (iii) "shall," "will," or "agrees" are mandatory, and "may" is permissive; (iv) "or" is not exclusive; (v) "include," "includes" and "including" are not limiting and shall be construed as if followed by the words "without limitation," and (vi) "days" means calendar days unless provided otherwise.

Section 15.04. Covenants Running with the Land. All of the provisions in this Agreement shall be binding upon the parties and their respective heirs, successors and assigns, representatives, lessees, and all other persons acquiring all or a portion of the Property or Project, or any interest, whether by operation of law or in any manner. All of the provisions in this Agreement shall be enforceable as equitable servitudes and shall

constitute covenants running with the land under California law including California Civil Code section 1468. Each covenant to act or do not act benefits or a burden upon the Private Purpose Areas runs with the Private Purpose Areas and is binding upon County and each successor or assign of County during developing such Private Purpose Areas of the Project.

Section 15.05. Entire Agreement, Counterparts and Exhibits. This Agreement may be executed in multiple counterparts, each of which shall be deemed an original. This Agreement, with the attached Exhibits, constitutes the final and exclusive understanding and agreement of the Parties and supersedes all negotiations or previous agreements of the Parties regarding all or any part of the matter.

Section 15.06. Recordation of Agreement. Under California Government Code section 65868.5, no later than ten (10) days after City enters into this Agreement, the City Clerk shall record an executed copy of this Agreement in the Official Records of the County of Solano.

Section 15.07. No Joint Venture or Partnership. It is understood and agreed to by and between the parties that: (i) City has no interest or responsibilities for, or duty to, third parties concerning any public improvements until such time, and only until such time, that City accepts the same under this Agreement or for the various Existing Project Approvals or Subsequent Project Approvals; (ii) County shall have full power over and exclusive control of the Project described, subject only to the limitations and obligations of County under this Agreement, the Existing Project Approvals, Subsequent Project Approvals, and Applicable Law; and (iv) City and County renounce the existence of any form of agency relationship, joint venture or partnership between City and County except as set forth in this Agreement.

Section 15.08. Waivers. All waivers of this Agreement shall be in writing and signed by the appropriate authorities of City and County.

Section 15.09. California Law. This Agreement shall be construed and enforced under the laws of California.

IN WITNESS WHEREOF, this Agreement has been entered into by and between County and City as of the day and year first above written.

[SIGNATURES ON FOLLOWING PAGE]

COUNTY:

County of Solano, a political subdivision of the State of California

By: _____
Birgitta Corsello, County Administrator

Date Signed: _____

APPROVED AS TO FORM:

By: _____
County Counsel

CITY:

City of Vallejo, a municipal corporation

By: _____
Daniel E. Keen, City Manager

Date Signed: _____

APPROVED AS TO FORM:

By: _____
Claudia Quintana, City Attorney

ATTEST:

By: _____
City Clerk

APPROVED AS TO INSURANCE REQUIREMENTS

By: _____
Darrell Handy, Risk Manager

EXHIBIT A

Site Plan

HIGHWAY 37

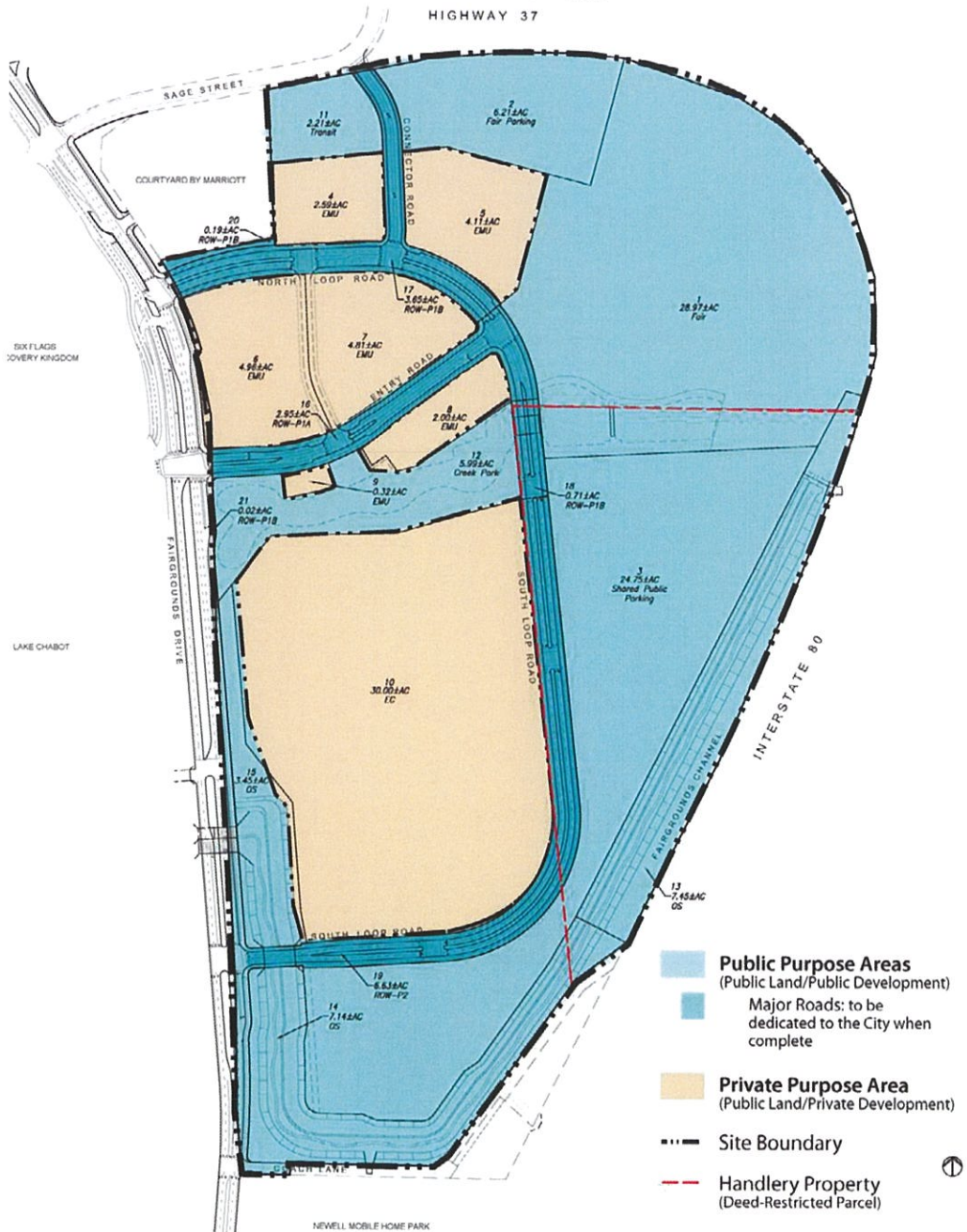


EXHIBIT B

List of Taxes, Assessments, Fees and Exactions

EXHIBIT C

City Annual Revenue Sharing Percentage of City Available Revenue (Property Taxes (including Secured and Unsecured) Property Tax In-Lieu of VLF), Sales and Use Taxes (excluding Measure B), Utility Users Taxes, and Business License Taxes; Estimated City Annual Revenue Sharing Payments

FISCAL YEAR	CITY ANNUAL REVENUE SHARING PERCENTAGE OF AVAILABLE CITY REVENUES	ESTIMATED CITY ANNUAL REVENUE SHARING PAYMENT
2013-14	63%	\$ -
2014-15	63%	\$ -
2015-16	63%	\$ 155,076
2016-17	97%	\$ 452,417
2017-18	97%	\$ 493,258
2018-19	97%	\$ 632,080
2019-20	97%	\$ 680,066
2020-21	97%	\$ 1,020,606
2021-22	97%	\$ 1,171,242
2022-23	55%	\$ 681,583
2023-24	55%	\$ 691,865
2024-25	55%	\$ 696,123
2025-26	55%	\$ 704,024
2026-27	55%	\$ 702,407
2027-28	55%	\$ 753,477
2028-29	55%	\$ 764,171
2029-30	55%	\$ 876,834
2030-31	55%	\$ 915,679
2031-32	55%	\$ 1,016,142
2032-33	55%	\$ 1,135,902
2033-34	55%	\$ 1,209,165
2034-35	55%	\$ 1,292,669
2035-36	55%	\$ 1,315,806
2036-37	55%	\$ 1,315,087
2037-38	55%	\$ 1,367,233
2038-39	55%	\$ 1,377,657
2039-40	55%	\$ 1,376,054
2040-41	55%	\$ 1,374,663
2041-42	55%	\$ 1,373,473
2042-43	55%	\$ 1,372,476
2043-44	24%	\$ 598,544
2044-45	24%	\$ 598,266
2045-46	24%	\$ 598,060
2046-47	24%	\$ 597,925
2047-48	24%	\$ 597,857
2048-49	24%	\$ 597,852
2049-50	24%	\$ 597,909
2050-51	24%	\$ 598,026
2051-52	24%	\$ 598,198
2052-53	24%	\$ 598,425
2053-54	24%	\$ 598,704
2054-55	24%	\$ 599,034
2055-56	24%	\$ 599,412
2056-57	24%	\$ 599,838
2057-58	24%	\$ 600,308
2058-59	24%	\$ 600,823
2059-60	24%	\$ 601,380
2060-61	24%	\$ 601,978
2061-62	24%	\$ 602,616
2062-63	24%	\$ 603,293
Total Estimated City Payments		\$38,905,684

EXHIBIT D

Project Infrastructure Schedule

Infrastructure Schedule

INFRASTRUCTURE IMPROVEMENT	PHASE	ESTIMATED COST	COMMENCE CONSTRUCTION¹
Initiate demolition	1A/1B	\$888,000/\$15,000	5 years/10 years
Initiate remedial grading	1A/1B	\$1,140,000/\$120,000	5 years/10 years
Initiate mass grading	1A/1B	\$738,000/\$353,000	5 years/10 years
Begin construction of water feature	1A/1B	\$758,000/\$1,009,000	5 years/10 years
Begin construction of Entry Road - Fairgrounds Drive to North Loop Road	1A	\$1,954,000	10 years
Begin construction of South Loop Road – Entry Road to Phase 1A limit (intersection improvements)	1A	\$243,000	10 years
Begin construction of North Loop Road – Entry Road to Fairgrounds Drive	1A	\$1,683,000	10 years
Begin construction of South Loop Road – Phase 1A limit to Phase 1B limit	1A	\$392,000	10 years
Begin construction of Connector Road – North Loop to Sage Street	1B	\$688,000	10 years

Phase 2 and Phase 3 infrastructure schedules will be determined at the time Phase 2 and Phase 3 financing is determined by the Parties pursuant to the terms of the Development Agreement.

¹ Commence Construction refers to the number of years following the first Phase 1 Project Financing provided all necessary permits have been obtained.

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. PC 13-08

**RESOLUTION OF THE VALLEJO PLANNING COMMISSION
CONDITIONALLY APPROVING VESTING TENTATIVE MAP #13-0001
FOR THE SOLANO360 PROJECT**

BE IT RESOLVED by the Planning Commission of the City of Vallejo as follows:

I. GENERAL FINDINGS

WHEREAS, the County of Solano (“County”), the City of Vallejo (“City”), and the Solano County Fair Association (“Fair Association”) have jointly prepared the Solano360 Specific Plan (“Plan”) to facilitate the redevelopment of the Solano County Fairgrounds, a 149.1 acre property owned by Solano County and located at the crossroads of Interstate 80 and State Route 37 in Vallejo, CA; and

WHEREAS, the County has submitted an application for a Vesting Tentative Map, prepared by MacKay and Soms, dated April 3, 2013 to subdivide the Plan Area into 23 parcels to facilitate physical improvement and future leasing of the property consistent with the Plan; and

WHEREAS, City staff has reviewed the Vesting Tentative Map and has determined that the map is in conformance with Title 15 Subdivisions of the Vallejo Municipal Code; and

WHEREAS, staff recommends that the proposed Vesting Tentative Map be approved subject to the attached conditions of approval, herein provided as Exhibit A to this Resolution; and

WHEREAS, on April 29, 2013, a duly noticed public hearing was held by the Planning Commission on the project; and

WHEREAS, after hearing all qualified and interested persons and receiving and considering all relevant evidence, the Planning Commission finds and determines as follows:

II. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS

The Planning Commission finds that on the basis of the whole record before it, the City of Vallejo, as a local agency, acknowledges that a Final Environmental Impact Report (FEIR) for the project has been prepared by the County and circulated for public review. The FEIR concluded that the project would result in impacts that can be mitigated to a level of insignificance, and impacts that are significant and unavoidable. The Solano County Board of Supervisors certified the FEIR and approved the Mitigation Monitoring and Reporting Program and Statement of Overriding Considerations on February 26, 2013.

III. FINDINGS RELEVANT TO APPROVING THE VESTING TENTATIVE MAP

Section 1. The Planning Commission finds that the County has submitted an application for a Vesting Tentative Map, pursuant to the City of Vallejo Municipal Code Chapter 15.10 Vesting Tentative Maps for the creation of legal parcels.

Section 2. The Planning Commission finds, based on the facts contained in the staff report incorporated herein by this reference, and given the evidence presented at the public hearing, and subject to the conditions attached to this resolution, that:

1. The Vesting Tentative Map is consistent with the Solano360 Specific Plan and is therefore consistent with the goals and policies of the Vallejo General Plan
2. The Vesting Tentative Map is consistent with the Solano360 Specific Plan and therefore in conformance with the zoning ordinance.
3. The Vesting Tentative Map is consistent with Title 15 of the Vallejo Municipal Code and is therefore consistent with the Subdivision Map Act.

IV. RESOLUTION APPROVING VESTING TENTATIVE MAP #13-0001 FOR THE SOLANO360 SPECIFIC PLAN

NOW, THEREFORE, LET IT BE RESOLVED that the Planning Commission hereby APPROVES Tentative Map #13-0001 for a Vesting Tentative Map, herein provided as Exhibit B to Attachment 3, that would allow the creation of 23 parcels of property identified as the Solano360 Specific Plan Area, based on the findings contained in this Resolution and subject to the Conditions of Approval herein provided as Exhibit A to Attachment 3.

SOLANO360 TENTATIVE MAP #13-0001

CONDITIONS OF APPROVAL/PROJECT REQUIREMENTS

The following conditions of approval/project requirements shall apply to Tentative Map #13-0001, the "Solano360 Vesting Tentative Map" prepared by MacKay & Soms, dated April 3, 2013. Development has not been considered or approved as part of this Tentative Map. Such development will be considered and conditioned, if at all, as part of future development permits or other appropriate entitlement. Conditions of approval related to future parcel development shall only apply to those parcels designated for private purpose identified as Parcels 5-16 on the Tentative Map, and shall not apply to parcels designated for public purpose identified as Parcels 1-4.

Due to the proposed phasing of the project, multiple Final Maps may be filed.

A. PLANNING DIVISION

1. Submit a numbered list to the Planning Division stating how each condition of project approval contained in this report will be satisfied. The list should be submitted to the Planning Manager who will coordinate the Project.
2. All future development projects and proposals are subject to the requirements of the Solano360 Specific Plan including but not limited to a Planned Development Unit Plan.
3. Improvement projects inconsistent with the Solano360 Specific Plan may require a Specific Plan Amendment and shall be referred to the Solano360 Committee for interpretation.
4. Certified Environmental Impact Report (EIR) Mitigation Measures for the Solano360 Specific Plan, pursuant to the California Environmental Quality Act (CEQA), are incorporated by reference as a condition of approval for this Project.
5. The approval of the Vesting Tentative Map or Final Map shall not constitute the approval of the construction of any improvements on the private parcels within the Project area boundaries.

B. BUILDING DIVISION

6. All improvements, repairs, and new construction shall require a building permit as required by the 2010 California Building Code.

C. PUBLIC WORKS**Prior to Final Map(s) Approval**

7. Solano County or any subsequent Master Developer ("Solano County") shall submit a final map prepared by a qualified registered civil engineer or Land Surveyor that is in compliance with the Subdivision Map Act and Vallejo Municipal Code (VMC) for review

and approval. Submit preliminary title report and all pertinent documents for map review.

8. Solano County shall dedicate on the Final Map at no cost to the City all of the required additional right of way for the construction of offsite Fairgrounds Drive improvements as part of the Project's fair share contribution to the Redwood Parkway Improvement Project.
9. Solano County shall submit master address map for review and approval by the City Engineer.
10. Solano County shall dedicate on the Final Map or by separate deed at no cost to the City all of the required public utility easements required for public improvements.
11. Solano County shall establish, financing mechanisms for funding the construction of public improvements for the Project.
12. City or Solano County may establish an improvement district in accordance with Chapter 14.44 of the VMC or a similar funding mechanism, including but not limited to a Community Facilities District.
13. Solano County shall comply with the Certified Environmental Impact Report (EIR) Mitigation Measures-Transportation and shall provide evidence of compliance consistent with the terms of Transportation including the timing as established by the improvement Phasing Schedule and type of improvements.
14. Solano County shall submit a mass grading plan together with a comprehensive geotechnical investigation report for this project for review. An independent soils and geotechnical review of the Project may be required by the City Engineer. The City and County agree to jointly select the soils engineer with the cost of the review to be borne by Solano County. Mass grading plan shall show dirt distribution, cut and fill, over excavation import and export quantities and any other details needed for mitigation of soft underlain materials, liquefaction, expansive soil, etc. for further review.
15. Solano County shall submit with the Final Map of the subdivision grading and erosion control plans, street improvement plans and trench utility and street light plans, and landscaping, irrigation and fencing plans. The Final Map submittal must be complete with all supporting documentations and pertinent reports.
16. Prior to each Final Map, Solano County will submit Provision C.3 requirements for the Project pursuant to the City's Municipal Regional Stormwater NPDES Permit (MRP). The submittal shall be detailed pursuant to the Stormwater Control Plans (SWCP) and Low Impact Design (LID) treatment options in the MRP.
17. Solano County shall submit a stormwater quality management plan to City for review and approval by the City Engineer pursuant to MM HYD-2a and MM HYD-2b of the Final Environmental Impact Report, Solano360 Specific Plan, dated February 26, 2013 (FEIR).

18. Solano County shall submit a Landscape and Irrigation Plan for each phase of street improvement to the City in compliance with VMC Chapter 16.70 for review and approval by the City Planning Manager and City Engineer. Solano County shall follow the City's requirements for low-water using and drought-resistant plant materials. Solano County shall incorporate water conservation practices pursuant to the 2010 California Building Code, 2010 Green Building Standards Code, and the applicable VMC in effect at the time, and the City's Water Management Plan. Water conservation practices shall include the installation of low-flow water devices.
19. If required by the County of Solano, prior to approval of the Final Map submit a draft copy of the Covenants, Conditions and Restrictions (CC&Rs) for the Project. The CC&Rs are subject to review and approval of the Planning Manager, Public Works Director, and the City Attorney.
20. Submit street names to Public Works Department for review and approval by City's Fire, Police, and Public Works departments and U.S. Postal Service.
21. Solano County shall provide copies of the recorded Final Map to the City at no cost to the City.
22. Solano County shall pay to City map checking fees based on the City's fee schedule in existence at the time of approval as set forth in the Development Agreement (DA).
23. Prior to construction of either Phase 2 or 3 of the Solano360 Specific Plan, all traffic and intersection operations shall be funded and mitigated including widening of the Fairgrounds Drive and the two interchanges as identified in the Solano Transportation Authority's (STA) Comprehensive Transportation Plan. The Project's frontage improvements from Sage Street to Coach Lane at the ultimate widened Fairgrounds Drive location shall be designed and constructed prior to implementing Phases 2 or 3 of the Solano360 Plan.
24. Solano County shall provide funding to mitigate their fair share of any adverse impact on the existing storm drain culvert under Fairgrounds Drive that will result from drainage flows from the Project to the satisfaction of the City Engineer and Vallejo Sanitation and Flood Control District (VSFCD).

Grading

25. Prior to issuance of grading permit by the City, Solano County shall obtain permits from resource agencies (e.g., Corps of Engineers, Fish & Game, etc.) if required for this Project.
26. Solano County shall submit on-site grading, improvements, utility and landscaping plans for review and approval and construction to the satisfaction of City Engineer.
27. During Project construction, grading and slope preparation activities shall be conducted under the supervision of a Registered Geotechnical Engineer or Certified Engineering Geologist and any design modifications necessitated by changes in field conditions shall be reviewed and approved by the City Engineer.

28. Prior to grading any parcel within the 100 year flood zone, Solano County shall apply to Federal Emergency Management Agency (FEMA) for a Letter of Map Revision (LOMR). If FEMA requires levee certification for the existing levee or if a new levee has not been built and certified at the time of LOMR application, then Solano County shall obtain said certification.
29. Grading activities shall be scheduled to avoid soil disturbance during the rainy season unless approved by the City Engineer or appropriate regulatory authority and conducted in conformance with the applicable SWPP and any applicable ordinances and requirements.
30. Before grading is concluded, a positive gradient away from the slopes must be established to carry the runoff away from the slopes to areas where erosion and sedimentation can be controlled.
31. The Stormwater Pollution Prevention Plan (SWPPP) shall be submitted to the City Engineer for approval. Construction-related erosion and sedimentation control measures shall be incorporated in a comprehensive erosion control plan to be reviewed and approved by the City prior to the issuance of a grading permit. Solano County shall obtain a Waste Discharge Identification Number (WDID #) prior to issuance of a grading permit by the City.
32. During construction, it shall be the responsibility of Solano County's contractor to provide for safe traffic control in and around the site. This may include but not be limited to signs, flashing lights, barricades and flag persons. If required by the City a traffic control plan shall be submitted for review and approval by the City Traffic Engineer.
33. During each phase of construction, interim erosion controls measures shall be implemented, such as water bars, mulching of exposed slopes, installation of temporary culverts, rock slope protection, sediment traps, silt fences and/or straw wattles consistent with the City's Municipal Regional Stormwater NPDES Permit, as may be demonstrated in the Association of Bay Area Governments Manual of Standards for Erosion and Sedimentation Control Measures or the San Francisco Bay Region Regional Water Quality Control Board Erosion and Sediment Control Field Manual.
34. In locations underlain by expansive soils and/or non-engineered fill, the designers of proposed building foundations and improvements (including sidewalks, roads, and utilities) shall consider these conditions. The design-level geotechnical investigation shall include measures to ensure that potential damage related to expansive soils and non-uniformly compacted fill are corrected. Options to correct these conditions may range from removal of the problematic soils and replacement, as needed, with properly conditioned and compacted fill, to design and construction of improvements to withstand the forces exerted during the expected shrink-swell cycles and settlements.
35. Solano County shall submit a Best Management Practice (BMP) design for each storm drain outfall that discharges to the Project's Water Feature to the City Engineer for approval.

Landscaping

36. Landscaping, irrigation and fencing shall be installed pursuant to the Solano360 Specific Plan. Fence design and location shall be approved by the City Engineer and Planning Manager.
37. Solano County shall submit line of sight traffic plans for all intersections for review and approval by the City Traffic Engineer. Line of sight at all streets shall be clear of any proposed slopes, landscaping or other obstructions.
38. Prior to acceptance of the Project, the Project's landscape architect shall perform a complete and thorough field review of the landscape irrigation and planting within the Project and provide the City Engineer in writing a certificate that all landscaping, planting, and irrigation within the Project is in full compliance with the City ordinances and guidelines and approved landscape, planting and irrigation plans. At the end of twelve month warranty period, the Project's landscape architect shall evaluate each open space parcels to assure that 75% of the planted vegetation has been established on the slopes.

General

39. Solano County shall install curb and gutter on the Sage Street frontage of the Solano County owned parcel number 0052-240-530 commencing from its common property line from the existing improvements to the commencement of Caltrans right-of-way. Such improvements shall be completed as part of Phase 2.
40. Before Solano County proceeds to construct either Phases 2 or 3 of the Solano360 Plan, as described in the November 9, 2012 DEIR-Solano360 Specific Plan documents, all cumulative freeway traffic and intersection operations shall be mitigated by construction of the Redwood Parkway/Fairgrounds Drive improvement project at the two interchanges as identified in the STA's Comprehensive Transportation Plan.
41. Solano County shall install fence and concrete band to demarcate the property line between the Coach Lane and the drainage channel as required by the City Engineer.
42. Design and construction of the private and public improvements, e.g., streets and utility improvements, shall be in accordance with City Regulations and Standard Specifications and Standard Drawings for Public Improvements in effect at the time of construction (City Standards).
43. Roadway grades shall conform to City Standards.
44. Provide bus turn-outs for all bus stop locations and shall conform to City Standards.
45. Design and construction of all bridges shall meet Caltrans standard in effect at the time of construction.
46. Street lights for this Project shall be pursuant to the Solano360 Specific Plan, or as approved by City Engineer.

47. Roadway sections, pursuant to the Solano360 Specific Plan, all traffic signals, signage and striping shall comply with City Standards.
48. Vertical and horizontal alignment of all streets shall conform to City Standards.
49. Solano County shall comply with the "City of Vallejo Complete Street Policy" as specified in Resolution No. 12-155 N.C. adopted on November 13, 2012 and as it may be amended.
50. Once the Project public improvements are accepted by the City as public rights-of-way, they are not to be used for staging building construction activities, including but not limited to, storage of construction material and equipment. The street and sidewalks must be kept free of construction debris, mud and other obstacles and must remain open to traffic at all times.
51. All existing overhead utility wires through and fronting the subdivision shall be undergrounded by Solano County at no cost to the City.

PUBLIC WORKS (STANDARD CONDITIONS)

52. **HOW PROJECT CONDITIONS SATISFIED.** Prior to building permit issuance, submit a numbered list to the Planning Division stating how each condition of project approval contained in this report will be satisfied. The list should be submitted to the Planning Division.
53. **PUBLIC IMPROVEMENT STANDARDS.** All public improvements shall be designed to City of Vallejo standards and to accepted engineering design standards. The City Engineer has all such standards on file and the Engineer's decision shall be final regarding the specific standards that shall apply. (City Standards).
54. **IMPROVEMENT PLANS.** Prior to building permit submittals, submit three sets of plans to the Department of Public Works for plan check review and approval. (Improvement or civil plans are to be prepared by a licensed civil engineer) Plans are to include, but may not be limited to, grading and erosion control plans, improvement plans, joint trench utility, street light plans, and landscaping, irrigation and fencing plans and all supporting documentation, calculations and pertinent reports. (City Standards Section 1.1.7-A).
55. **GRADING.** Prior to issuance of grading permit, submit a soils report for review. An independent soils and geological review of the project may be required. The City shall select the soils engineer with the cost of the review to be borne by the Solano County. Site grading shall comply with City Municipal Code. (VMC Chapter 12.40).
56. **LINE OF SIGHT CRITERION.** In design of grading and landscaping, line of sight distance shall be provided based on Caltrans standards in effect at the time. Installation of fencing, signage, above ground utility boxes, etc. shall not block the line of sight of traffic and must be set back as necessary. (VMC Section 10.14).
57. **ON-SITE SOILS ENGINEER.** During grading operations, the project geologist or soils engineer or their representative and necessary soils testing equipment must be

present on site. In the absence of the soils engineer or his representative on site, the Department of Public Works shall shut down the grading operation. (VMC Section 12.40.080).

58. DUST AND EROSION CONTROL. All dust and erosion control shall be in conformance with City standards and ordinances. (VMC Sections 12.40.050 and 12.40.070).
59. COMPACTION TESTS. Prior to building permit issuance or acceptance of grading, compaction test results and certification letter from the project soils engineer and civil engineer confirming that the grading is in conformance with the approved plans must be submitted to the Department of Public Works for review and approval. Test values must meet minimum relative compaction recommended by the soils engineer (usually at least 90 percent). (VMC Section 12.40.070-R).
60. DRIVEWAY STANDARDS. Entrances to any private project must be standard driveway approaches unless deviation is permitted by the City Engineer. (VMC Section 10.04.260).
61. STREET EXCAVATION PERMIT. Obtain a street excavation permit from the Department of Public Works prior to performing any work within City streets or rights-of-way, or prior to any cutting and restoration work in existing public streets for utility trenches. All work shall conform to City standards. (VMC Chapter 10.08).
62. ENCROACHMENT PERMIT. Prior to building permit issuance, obtain an encroachment permit from the Department of Public Works for all work proposed within the public right-of-way. (VMC Chapter 10.16).
63. TRAFFIC CONTROL PLAN. Prior to start of construction; submit a traffic control plan to the Department of Public Works for review and approval. (CA MUTCD).
64. COORDINATION OF CONSTRUCTION INSPECTION. Construction inspection shall be coordinated with the Department of Public Works and no construction shall deviate from the approved plans. (City Standards Sections 1.1.4 and 1.1.5).
65. PLAN CHANGES. The Project design engineer shall be responsible for the project plans. If plan deviations are necessary, the Project engineer must first prepare a revised plan or details of the proposed change for review by the City Engineer and, when applicable, by Vallejo Sanitation and Flood Control District. Changes shall be made in the field only after approval by the City Engineer. At the completion of the Project, the design engineer must prepare and sign the "as built" plans. (City Standards Section 1.1.9).
66. BONDS AND FEES. Prior to approval of construction plans, provide bonds and pay applicable fees. Bonding shall be provided to the City in the form of a "Performance Surety" and a separate "Labor and Materials Surety" in amounts stipulated by VMC, Section 15.12.090 and fees shall be the amount in effect, pursuant to implementing resolution at the time.

67. **INSTALL IMPROVEMENTS.** Prior to occupancy/final building inspection, install the improvements required by the Department of Public Works including but not limited to streets and utilities.
68. **SIDEWALK REPAIR.** Prior to occupancy/final building inspection, remove and replace any broken curb, gutter, sidewalk or driveway approach as directed in the field by the City Engineer. (VMC Chapter 10.04).
69. **FLOOD PROTECTION.** The Project is within the 100-year flood zone. Prior to obtaining grading permit, apply to FEMA for a Conditional Letter of Map Revision (CLOMR). Prior to obtaining building permit, apply to FEMA for a Letter of Map Revision (LOMR). Prior to obtaining certificate of occupancy or acceptance by the City, whichever is applicable, obtain an approved Letter of Map Revision from FEMA. It will take FEMA at least 90 days to obtain CLOMR or LOMR. FEMA can be contacted at telephone (510) 627-7184, or Mr. Jack Eldridge, Chief, National Flood Insurance Program Branch, FEMA, Region IX, 1111 Broadway Street, Suite 1200, Oakland, CA 94607-4052. (VMC Chapter 7.98 - Flood Damage Protection).
70. **STREET TREES.** Prior to release for occupancy, plant required street trees in accordance with the Solano360 Specific Plan. Street tree(s) shall be inspected by Public Works Landscape Inspector prior to release for occupancy. (VMC Section 15.06.190 and City Standards Section 3.3.48).
71. **JOINT TRENCH.** Solano County shall provide joint trench plans for the underground electrical, gas, telephone, cable television and communications conduits and cables including the size, location and details of all trenches, location of all building utility service stubs and meters and placement or arrangements of junction structures as a part of the Improvement Plans submitted for the Project. The composite drawings and/or utility improvement plans shall be signed by a licensed civil engineer. (VMC, Sections 15.06.160 and 15.06.170).
72. **SIGNAL INTERCONNECT CABLES.** There are fiber optic and /or copper signal interconnect cables located at the edge of the roadway or under the sidewalk. The plans should address either the relocation of these cables or a note should be made of the cable location. A warning should be included on the plans stating that if the cable damaged, the entire length of the cable between the two nearest hubs will be will be replaced unless otherwise authorized by the City Engineer.

D. WATER DIVISION

73. **WATER SYSTEM PLANS.** All water system improvements shall be consistent with the Vallejo Water System Master Plan, 1985, prepared by Kennedy/Jenks Engineers as updated by Brown & Caldwell, 1996. Prior to Improvement Plan approval and building permit issuance, water system improvement plans shall be submitted to the Water Superintendent for review and approval, and shall contain at least:
 - a. Location and size of fire sprinkler service connection(s).
 - b. Location and size of domestic service connection(s).
 - c. Location and size of irrigation service connection(s).
 - d. Location of fire hydrants.

- e. Location of structures with respect to existing public water system improvements, such as mains, meters, etc.
 - f. Location and size of any new water mains.
 - g. Location and size of backflow prevention devices (required on water service connections to irrigation systems, certain commercial water users, and to commercial fire sprinkler systems, per City Ordinance 922 N.C. (2d).
74. FIRE FLOW REQUIREMENTS. Fire flow requirements of the Fire Department shall be complied with. Fire flow at no less than 25 psig residual pressure shall be available within 1,000 feet of any structure. One half of the fire flow shall be available within 300 feet of any structure.
75. HYDRAULIC CALCULATIONS. Prior to Improvement Plan approval and building permit issuance, hydraulic calculations shall be submitted to the Water Superintendent demonstrating that the fire flow requirements are complied with.
76. FIRE PROTECTION SYSTEMS. Fire hydrant placement and fire sprinkler system installation, if any, shall meet the requirements of the Fire Department. For combined water and fire services, the requirements of both the Fire Department and the Vallejo Water System Master Plan, with latest revisions, shall be satisfied.
77. WATER EASEMENTS. Easements shall be granted for all water system improvements installed outside the public right-of-way in the City's Standard Form for Grant of Water Line Easement with the following widths:
- a. 15 ft. wide (minimum) for water mains.
 - b. 10 ft. wide (minimum) for fire hydrants, water meters, backflow preventers, double detector check valves, etc.
 - c. Other facilities will be reviewed by the Water Division.
79. WATER SERVICE BONDS AND FEES. Water service shall be provided by the City following completion of the required water system improvements and payment of applicable fees. Performance and payment bonds shall be provided to the City of Vallejo prior to construction of water system improvements. Fees include those fees specified in the VMC including connection and elevated storage fees, etc., and fees for tapping, tie-ins, inspections, disinfection, construction water, and other services provided by the City with respect to the water system improvements.
80. WATER SYSTEM INSTALLATION. Prior to occupancy or final building inspection, install water system improvements as required. Backflow devices, where required, shall be installed in areas hidden from public view and/or shall be mitigated by landscaping.

PROJECT SPECIFIC CONDITIONS AND OTHER COMMENTS/CONDITIONS NOT COVERED BY THE STANDARD CONDITIONS LISTED ABOVE.

81. General – The proposed non potable water (NPW) mains that are shown in the proposed City streets shall be owned and maintained by Solano County. Solano County shall obtain an encroachment permit from the City for these NPW mains. The NPW mains shall have a minimum of 10 feet separation from the proposed City's

potable water mains and the NPW mains shall have different water valves and valve pots than the proposed potable water mains and be labeled as NPW.

82. Prior to the first Parcel or Final Map approval by the City for recording that proposed to use raw water, the City and Solano County shall enter into an agreement that addresses payment and use of the raw water from Lake Chabot and/or raw water from the City's Cal Pac main.
83. Phase 1A of the Solano360 Specific Plan—The proposed public potable water main in Entry Road shall not connect to the existing Solano County's private water system. (See comment 75 for the required hydraulic calculations to size the proposed public potable water main).
84. Prior to the construction of Phase 1B of the Solano360 Specific Plan by Solano County, payment shall be made to the City to relocate the existing master meter and BFD at the north end of the project to a location at Connector Road.
85. Phase 1B of the Solano360 Specific Plan, the proposed public potable water main in North Loop Road shall not connect to the existing Solano County's private water system. If a connection is required, as determined by fire, domestic and irrigation flow hydraulic calculations, then before Solano County proceeds to construct Phase 1B they shall pay the City for a new connection to Solano County's private water system.
86. Prior to the construction of Phase 2 or 3 of the Solano360 Specific Plan by Solano County, a comprehensive water study (master plan) shall be prepared and submitted to and approved by the Water Superintendent that addresses the potable water main size in Fairgrounds Drive from Coach Lane to Sage Street that connects to the existing 24-inch potable water mains at Sage Street and Coach Lane. In addition, the existing 12-inch potable water main in Coach Lane shall be extended to connect to the potable water main in Fairgrounds Drive.

E. VALLEJO SANITATION AND FLOOD CONTROL DISTRICT (VSFCD)

87. Prior to building permit issuance, submit complete improvement plans and supporting documentation for proposed sanitary sewage and storm drainage work to VSFCD for review and approval.
88. Prior to building permit issuance, a VSFCD Connection Permit is required. Pay all applicable review and connection fees.
89. All individual parcels shall drain and sewer directly to the public system.
90. Non-District facilities serving more than one lot will not be allowed.
91. The following information is needed for future site development projects:
 - a. All proposed and existing District facilities to serve the project. Provide site utility plan showing existing and proposed sanitary sewer and storm drain facilities, mains, laterals, connections, etc.
 - b. Required notes concerning District Master Plans and District Standards.

- c. Please show location of sanitary sewer and cleanout on site utility plan.
- 92. Comply with the current VSFCDC regulations regarding improvement of existing offsite storm drains and/or sanitary sewer facilities.
- 93. VSFCDC sanitary sewer or storm drains shall not be installed in the rear of any of the lots.
- 94. Prior to building permit issuance, provide an agreement establishing maintenance/repair responsibility among commercial parcels using common private storm or sanitary sewer systems.
- 95. Prior to occupancy/final building inspection, provide all weather access to District facilities for VSFCDC maintenance vehicles. Backing more than 50 feet shall not be allowed.

STORM DRAIN

- 96. Prior to building permit issuance, submit grading and erosion control plans, to VSFCDC for review and approval. Do not block drainage from adjacent lands.
- 97. The 100-year storm overflow pattern shall be shown on the grading plans. Provide a secondary (surface flow) drainage system to handle flows in excess of the capacity of the primary system (piped or channelized). The proposed building locations shall avoid this secondary path. Also, determine the 100-year storm tributary area. This may differ from the 15-year tributary area.
- 98. All storm drainage shall be collected onsite and conveyed underground to the public storm drain system.

SANITARY SEWER

- 99. Comply with VSFCDC pretreatment requirements for sanitary sewage.
- 100. Prior to occupancy/final building inspection, provide a standard VSFCDC cleanout at the right-of-way/easement line per District standards and a two-way cleanout at the building pursuant to the Uniform Plumbing Code.
- 101. The use of the existing private sanitary sewer main and/or lateral is conditioned upon passing a standard VSFCDC air test.
- 102. Area within refuse enclosures shall drain to the sanitary sewer system. The outside perimeter of the trash enclosure shall be graded to prevent storm water from draining into the sanitary sewer system. The trash enclosure shall be covered with a roof or awning.
- 103. Prior to occupancy/final building inspection, provide a grease trap, sand trap, or interceptor.

STORM WATER QUALITY

104. Pretreatment of storm drainage water runoff is required pursuant to City's Municipal Regional Stormwater NPDES Permit Provision C.3., storm drainage runoff shall be conveyed over landscaped areas or otherwise treated, as feasible, before discharging into the public system. This is to improve the storm water quality leaving the site. The project architect or civil engineer should contact VSFCD for possible design solutions and their impact on the design of the project.
105. No dumping drains to bay markings pursuant to District's requirements shall be installed at each storm water drain inlet.
106. All parcels shall be transferred with post construction storm water pollution prevention guidelines given to new owners. These guidelines shall be submitted with the improvement plans for review and acceptance for distribution as part of the improvement process.
107. Submit proof of NOI, Storm Water Pollution Prevention Plan, and Post Construction Best Management Practice Design Plan (see current VSFCD Storm Water Management Plan Appendix 4B).
108. Grading and improvement plans shall include storm water pollution prevention plans for use during site development and building construction to mitigate impacts of this development. This plan shall include calculations, measures related to debris, refueling areas, disposal of excess materials, site cleanup; hazardous substance containment, street cleaning, catch basin cleaning, and other similar measures (see current VSFCD Storm Water Management Plan Section 4.4.5).
109. Direct roof drainage across non-paved areas prior to entering storm drain inlets and gutter, when feasible.

RIGHT OF ENTRY/EASEMENTS

110. Prior to building permit issuance, Solano County shall grant VSFCD a right-of-way (GRANT OF RIGHT OF ENTRY) to test and repair sanitary sewer laterals per VSFCD Resolution No. 89-4210.
111. Offsite VSFCD sanitary and/or storm drainage facilities shall be extended in proper easements as necessary.
112. VSFCD pipeline facilities shall be located in 15 foot wide easements or street rights-of-way.

F. FIRE PREVENTION

113. All buildings, facilities, and improvements shall be accessible to Fire Department apparatus by way of approved access roadways.
114. Access roads shall be designed and maintained to support the imposed loads of fire apparatus weighing 70,000 lbs. with an all-weather driving surface. The turning radius

shall be determined by the Fire Official. Please indicate on the Project plans the entire route of access in 1 inch equals 20 feet or 1 inch equals 10 feet scale, or as otherwise approved by the Fire Department.

115. Access roads shall have an unobstructed width of not less than 20' (16' for single residential) with a maximum grade of 15%. Access roads shall have a minimum of 13'-6" of vertical clearance, including trees, wires and overhead signs. Speed bumps or other vertical traffic calming devices are prohibited on routes of fire access.
116. Access roads that are fire lanes shall either be posted with permanent all-weather signs constructed and installed in accordance with the Fire Department Fire Lane Marking Standards and shall all have curbs painted red and a sign posted at each entrance. Contact the Fire Prevention Bureau for further information and design templates. Location of such area's shall be determined prior to final approval.
117. All access roads and fire hydrants shall be installed and maintained serviceable prior to and during combustible construction.
118. Please indicate the location of all fire hydrants on the plans. Fire Hydrants shall be located in accordance with the 2010 California Fire Code, Appendix C. Approved impact protection shall be installed as necessary to protect hydrants from vehicular damage.
119. Solano County shall be responsible for painting of hydrants, verifying street valves are fully opened and the placement of blue reflective dots prior to requesting a final inspection. Maintenance, inspection and testing of private hydrants are the responsibility of the property owner. Private hydrants shall be painted yellow.
120. Fire-flow requirements for buildings shall be in accordance with the 2010 California Fire Code Appendix B Table B105.1. Please indicate on the Project plan all fire flow requirements for this Project.
121. Underground fire service mains require a separate permit from the Fire Prevention Division. Please submit 2 sets of plans for the underground fire service main for review and approval by the Fire Official prior to installation requirements.
122. An inspector from the Vallejo Fire Department must be present during all testing procedures.

G. GENERAL CONDITIONS

123. Unless specifically provided otherwise herein, all references to the Final Map shall mean a map or maps prepared for recordation.
124. These conditions are for a phased project. If the phasing is changed, the City reserves the right to add, modify or remove conditions.
125. The conditions herein contained shall run with the property and shall be binding on the owner as described above, and all heirs, executors, administrators, and successors in interest to the real property that is the subject of this approval.

126. Solano County shall defend, indemnify, and hold harmless the City of Vallejo or its agents, officers, and employees from any claim, action, or proceeding against the City or its agents, officers, or employees to attack, set aside, void, or annul, the approval of this Project by the City. The City shall promptly notify Solano County of any action. The City may elect, in its discretion, to participate in the defense of any action.

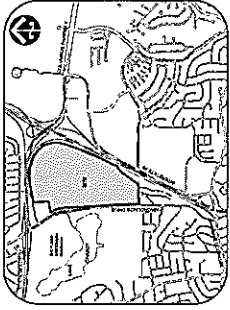
VESTING TENTATIVE MAP SOLANO360 PROJECT

VESTING TENTATIVE MAP

SOLANO 360

BEING A PORTION OF SECTION 5&6, T.3N., R.3W., M.D.B.&M.
CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA

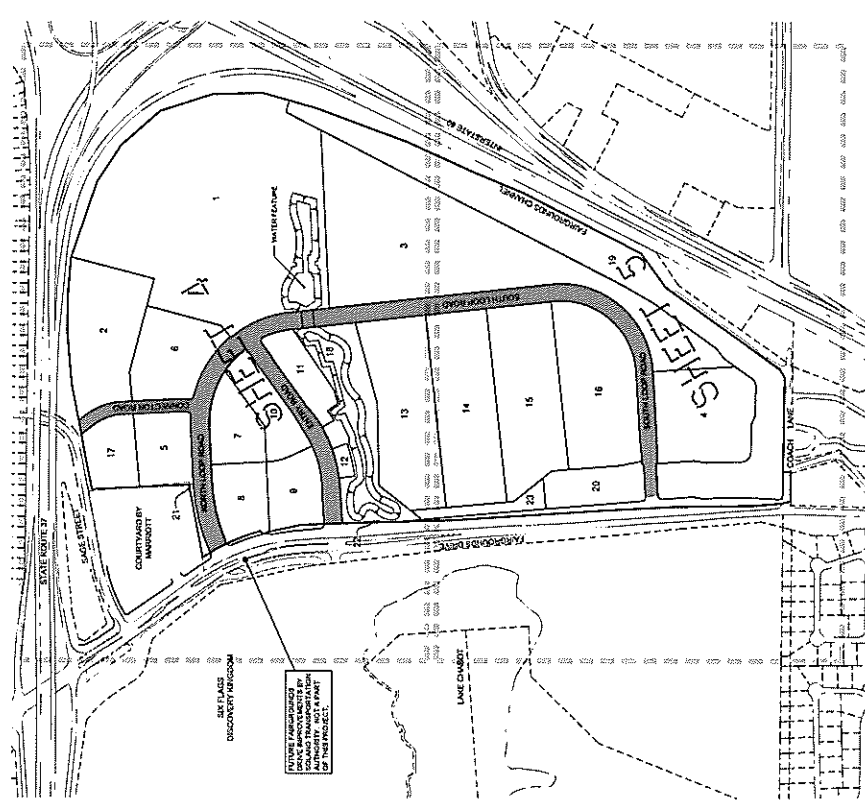
APRIL 2013



GENERAL NOTES:

- OWNER: CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA. PROJECT ADDRESS: 1000 MARKET STREET, SUITE 2000, VALLEJO, CA 94592.
- DATE: APRIL 2013
- CITY ENGINEER: MACKAY & SOMPS CIVIL ENGINEERS, INC., 1000 MARKET STREET, SUITE 2000, VALLEJO, CA 94592. CONTACT: CHRIS SUMAN (925) 250-6998
- GEO-TECHNICAL ENGINEER: DR. JAMES R. STEWART, CIVIL ENGINEER, 2015 COLTON CANYON PLACE, SUITE 250, VALLEJO, CA 94592. CONTACT: TUD JENNAN (925) 846-9800
- BOUNDARY SURVEY: THIS PROJECT IS BOUNDARY SURVEYED. THE CORNER BETWEEN THE CORNER AT THE INTERSECTION OF SHAWAN STREET AND PIDGE STREET AND THE CORNER 2' BRASS CAP STAMPED "P.S. CO. CIVIL ENGR. 2193" IS SHOWN AS THE POINT OF BEGINNING FOR THE SURVEY. THE POINTS ARE BEING RE-SURVEYED AND 1922.25' IS SHOWN AS THE DISTANCE FROM THE POINT OF BEGINNING TO THE POINT OF BEGINNING AT PAGES 202A.
- ASSESSOR'S PARCEL NUMBER: 013-044201-013-04420
- PROPERTY AREA: 148.1± ACRES (APPROXIMATE)
- EXISTING USE: FARMING AND FACILITIES (GROUP COMMERCE, EDUCATION, RECREATION, BUSINESS), OPEN SPACE, TRANSPORTATION.
- PROPOSED USE: ENTERTAINMENT, MEDIUM DENSITY, RECREATION COMMERCIAL, FAIR, OPEN SPACE, TRANSPORTATION.
- TOTAL NUMBER OF LOTS: 23
- PORTIONS OF PROPERTY ARE SUBJECT TO FLOODING AS SHOWN ON THE FEDERAL EMERGENCY MANAGEMENT INSTITUTE (FEMA) FLOOD INSURANCE RATE MAP (FIRM) AND NEARBY HIGHWAY AND HIGHWAY RIGHT-OF-WAY (ROW) MAPS FOR ALL SHEETS THAT APPLY TO THIS MAP.
- ALL STREET IMPROVEMENTS, STREET LIGHTS, AND WATER SYSTEMS TO BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF VALLEJO'S STANDARD SPECIFICATIONS FOR CONSTRUCTION AND WATER UTILITY PLANS OR AS OTHERWISE SPECIFIED HEREON. THE CITY OF VALLEJO'S STANDARD SPECIFICATIONS FOR CONSTRUCTION AND WATER UTILITY PLANS SHALL BE APPLICABLE TO ALL CONSTRUCTION WORK TO BE DONE ON ANY PORTION OF THIS MAP, UNLESS OTHERWISE SPECIFIED ON THIS MAP. THE CITY OF VALLEJO'S STANDARD SPECIFICATIONS FOR CONSTRUCTION AND WATER UTILITY PLANS SHALL BE APPLICABLE TO ALL CONSTRUCTION WORK TO BE DONE ON ANY PORTION OF THIS MAP, UNLESS OTHERWISE SPECIFIED ON THIS MAP.
- PORTABLE WATER CONNECTION TO CITY OF VALLEJO WATER SYSTEM.
- STORMWATER CONNECTION TO CITY OF VALLEJO STORMWATER SYSTEM.
- GAS AND ELECTRIC SERVICES TO BE PROVIDED BY GAS AND ELECTRIC. INSTALLATION SHALL BE UNDERGROUND IN ACCORDANCE WITH CITY REQUIREMENTS.
- TELEPHONE SERVICE TO BE PROVIDED BY AT&T. INSTALLATION SHALL BE UNDERGROUND IN ACCORDANCE WITH CITY REQUIREMENTS.
- UTILITY LOCATIONS AND DEPT. DIMENSIONS ARE PRELIMINARY AND SUBJECT TO FINAL ENGINEERING DESIGN.
- THIS PROJECT SHALL BE FINANCED BY THE PROJECT OWNER AND THE CITY OF VALLEJO SHALL BE RESPONSIBLE FOR THE COST OF THE PROJECT. THE PROJECT OWNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF VALLEJO AND DOES NOT REPRESENT A GUARANTEE OF THE PROJECT.
- THE PROPOSAL AS SHOWN IS COMPLETED FROM RESEARCH INFORMATION AND DOES NOT REPRESENT A GUARANTEE OF THE PROJECT. THE PROPOSAL IS BASED ON THE INFORMATION AVAILABLE TO THE ENGINEER AND DOES NOT REPRESENT A GUARANTEE OF THE PROJECT.
- UNLESS OTHERWISE SPECIFICALLY STATED IN THE CONDITIONS OF APPROVAL, LOCAL AGENCY APPROVAL OF THIS MAP SHALL CONSTITUTE AN EXPRESSED OPINION THAT THE PROPOSED SUBDIVISION AND DEVELOPMENT OF THE PROPOSED LOTS ARE IN ACCORDANCE WITH THE CITY OF VALLEJO'S STANDARD SPECIFICATIONS FOR CONSTRUCTION AND WATER UTILITY PLANS DESCRIBED IN GOVERNMENT CODE SECTION 94000.020.
- REFER TO DEVELOPER'S TRUE REPORT BY NOTARISENCE, DATED FEBRUARY 2013 FOR LOCATION AND SPECIES OF EXISTING TREES OVER 6" IN DIAMETER.
- ALL STRUCTURES TO BE DEVELOPED SHALL BE OTHERWISE NOTED.

FUTURE IMPROVEMENTS TO STATE ROUTE 37 AND PIDGE STREET ARE NOT PART OF THIS TENTATIVE MAP. SUCH IMPROVEMENTS WILL BE SUBJECT TO FUTURE TENTATIVE MAPS APPROVED WITH CONDITIONS ON.



LEGEND

PROPOSED	EXISTING
DESCRIPTION	DESCRIPTION
PROJECT BOUNDARY	PROJECT BOUNDARY
PARCEL LINGUOT LINE	PARCEL LINGUOT LINE
RIGHT-OF-WAY	RIGHT-OF-WAY
PUBLIC UTILITY EXISTENT	PUBLIC UTILITY EXISTENT
STORMWATER	STORMWATER
POTABLE WATER	POTABLE WATER
SLOPE	SLOPE

SHEET INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	BOUNDARY / TOPOGRAPHY MAP
3	CROSS SECTIONS
4-6	SITE / UTILITY PLAN
6-7	WATER WASTEWATER PLAN
8	STORMWATER QUALITY PLAN
9	CONSTRUCTION / BUILDING PHASING PLAN
10	DEVELOPMENT / BUILDING PHASING PLAN
11	WATER LIFE PHASING PLAN

AREA / LAND USE SUMMARY

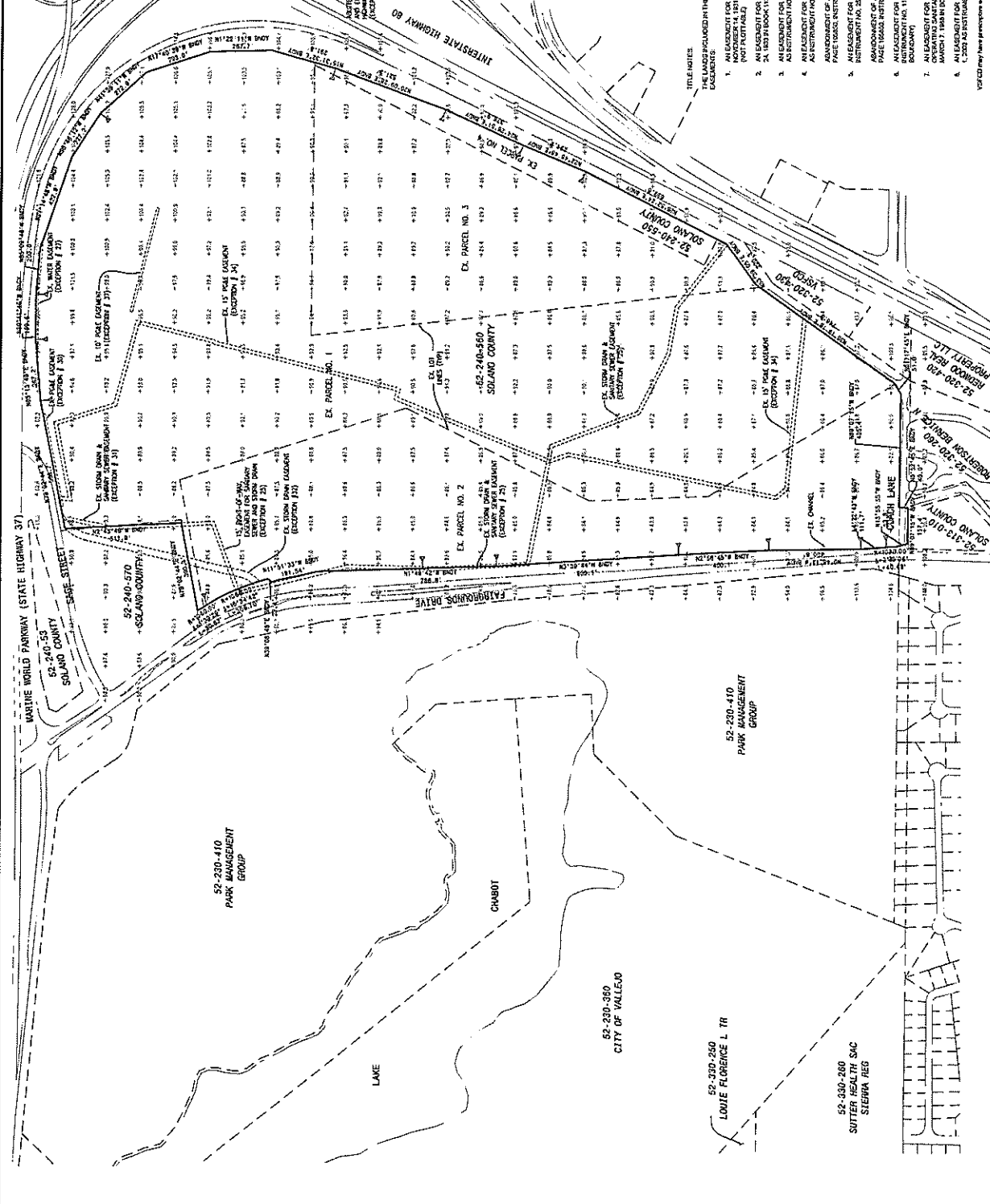
LAND USE	AREA	PERCENT
OPEN SPACE	69.4 AC.	46.8%
CONCRETE / COMMERICAL USE	26.1 AC.	17.6%
INDUSTRIAL	26.1 AC.	17.6%
RECREATION	26.1 AC.	17.6%
UTILITY	26.1 AC.	17.6%
TRANSPORTATION	26.1 AC.	17.6%
RESIDENTIAL	26.1 AC.	17.6%
OTHER	26.1 AC.	17.6%
TOTAL	148.1 AC.	100.0%

PROPOSED OWNERSHIP / MAINTENANCE

- RIGHT-OF-WAY: CITY OF VALLEJO
- WATER: STATE OF CALIFORNIA
- STORMWATER: COUNTY OF SOLANO
- MAINTENANCE BY VALLEJO SANITATION DISTRICT
- OPEN SPACE: COUNTY OF SOLANO
- SHAWAN PUBLIK PARKS COUNTY
- CONDON PARK
- TANAKA COUNTY
- ENTERTAINMENT MEDIUM DENSITY: CITY OF VALLEJO
- ENTERTAINMENT COMMERCIAL: COUNTY OF SOLANO

NO.	REVISION
1	[REDACTED]
2	[REDACTED]
3	[REDACTED]
4	[REDACTED]
5	[REDACTED]
6	[REDACTED]
7	[REDACTED]
8	[REDACTED]
9	[REDACTED]
10	[REDACTED]

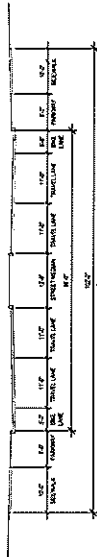
NOTES:
1. THE BOUNDARY SHOWN IS COMPRISED FROM RECORD INFORMATION AND DOES NOT REPRESENT THE RESULTS OF A FIELD SURVEY.
2. THE BOUNDARY DESCRIPTIONS SHOWN ARE FOR THE PRELIMINARY TITLE AND ARE SUBJECT TO THE FINAL TITLE.
3. THE BOUNDARY DESCRIPTIONS SHOWN ARE FOR THE PRELIMINARY TITLE AND ARE SUBJECT TO THE FINAL TITLE.
4. THE BOUNDARY DESCRIPTIONS SHOWN ARE FOR THE PRELIMINARY TITLE AND ARE SUBJECT TO THE FINAL TITLE.



TITLE NOTES:
THE LANDS INCLUDED IN THIS PARCEL MAP ARE SUBJECT TO THE FOLLOWING LEGAL PROVISIONS AND ENCUMBRANCES:
1. AN EASEMENT FOR GAS, OIL AND WATER PIPELINES AND INCIDENTAL PURPOSES, RECORDED NOVEMBER 18, 1952 AS INSTRUMENT NO. 2259 IN BOOK 698 PAGE 75 OF OFFICIAL RECORDS.
2. AN EASEMENT FOR PUBLIC HIGHWAY USE AND INCIDENTAL PURPOSES, RECORDED OCTOBER 24, 1931 IN BOOK 113 PAGE 78 OF OFFICIAL RECORDS, (PART PLOTTABLE).
3. AN EASEMENT FOR SEWER LINES AND INCIDENTAL PURPOSES, RECORDED NOVEMBER 18, 1952 AS INSTRUMENT NO. 2259 IN BOOK 698 PAGE 75 OF OFFICIAL RECORDS, (PART PLOTTABLE).
4. AN EASEMENT FOR SEWER LINES AND INCIDENTAL PURPOSES, RECORDED NOVEMBER 18, 1952 AS INSTRUMENT NO. 2259 IN BOOK 698 PAGE 75 OF OFFICIAL RECORDS.
5. AN EASEMENT FOR SEWER AND INCIDENTAL PURPOSES, RECORDED DECEMBER 27, 1952 AS INSTRUMENT NO. 2474 IN BOOK 1001 PAGE 11 OF OFFICIAL RECORDS.
6. AN EASEMENT FOR A PORTION OF AN EASEMENT RECORDED DECEMBER 14, 1944 BOOK 1004 PAGE 16002 INSTRUMENT NO. 5462, (PART PLOTTABLE).
7. AN EASEMENT FOR SEWER AND INCIDENTAL PURPOSES, RECORDED DECEMBER 27, 1952 AS INSTRUMENT NO. 2474 IN BOOK 1001 PAGE 11 OF OFFICIAL RECORDS.
8. AN EASEMENT FOR SEWER AND INCIDENTAL PURPOSES, RECORDED JUNE 19, 1960 AS INSTRUMENT NO. 11219 IN BOOK 1001 PAGE 75 OF OFFICIAL RECORDS, (PART OF BOUNDARY).
9. AN EASEMENT FOR CONSTRUCTION, RECONSTRUCTION, MAINTENANCE AND OPERATION OF SANITARY SEWER AND STORM DRAINAGE AND INCIDENTAL PURPOSES, RECORDED JANUARY 7, 1959 IN BOOK 1088 PAGE 2239 OF OFFICIAL RECORDS, (PART OF BOUNDARY).
10. AN EASEMENT FOR SEWER AND INCIDENTAL PURPOSES, RECORDED NOVEMBER 18, 1952 AS INSTRUMENT NO. 2259 IN BOOK 698 PAGE 75 OF OFFICIAL RECORDS, (PART OF BOUNDARY).

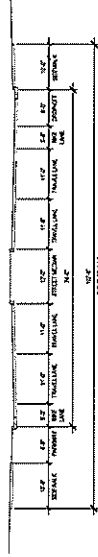
1/2" = 500' SCALE

NOTE: REFER TO SPECIFIC PLAN FOR LANDSCAPE BUFFER REQUIREMENTS BEHIND RIGHT-OF-WAY



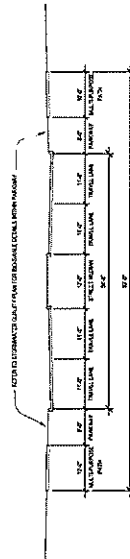
SECTION A-A - NORTH LOOP ROAD
(FAIRGROUNDS DRIVE TO CONNECTOR ROAD)

NTS



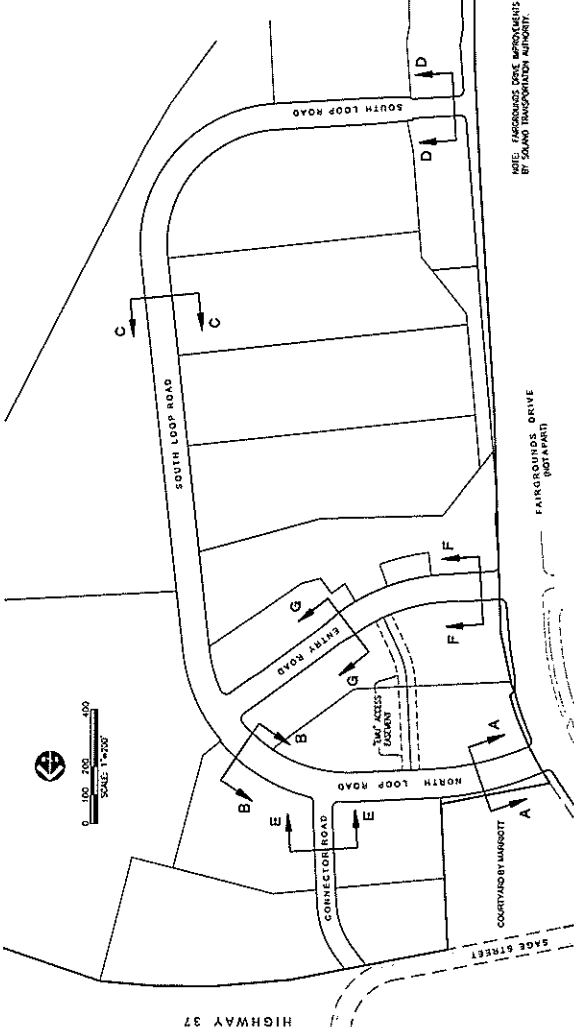
SECTION B-B - NORTH LOOP ROAD
(CONNECTOR ROAD TO ENTRY ROAD)

NTS

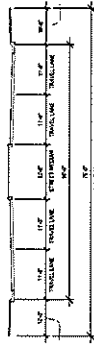


SECTION C-C - SOUTH LOOP ROAD
(EXCLUDES CHANNEL CROSSING)

NTS



NOTE: FAIRGROUNDS DRIVE IMPROVEMENTS BY SANDO TRANSPORTATION AUTHORITY.



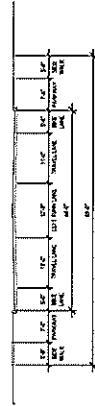
SECTION D-D - SOUTH LOOP ROAD
(AT CHANNEL CROSSING)

NTS



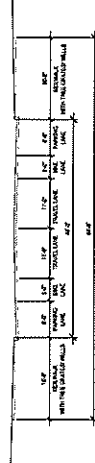
SECTION F-F - ENTRY ROAD
(FAIRGROUNDS DRIVE TO 'EMU' ACCESS EASEMENT)

NTS



SECTION E-E - CONNECTOR ROAD
(SAGE STREET TO NORTH LOOP ROAD)

NTS



SECTION G-G - ENTRY ROAD
('EMU' ACCESS ESBMT TO NORTH LOOP ROAD)

NTS

DRAFT

SCALE: 1"=100'

4 of 11

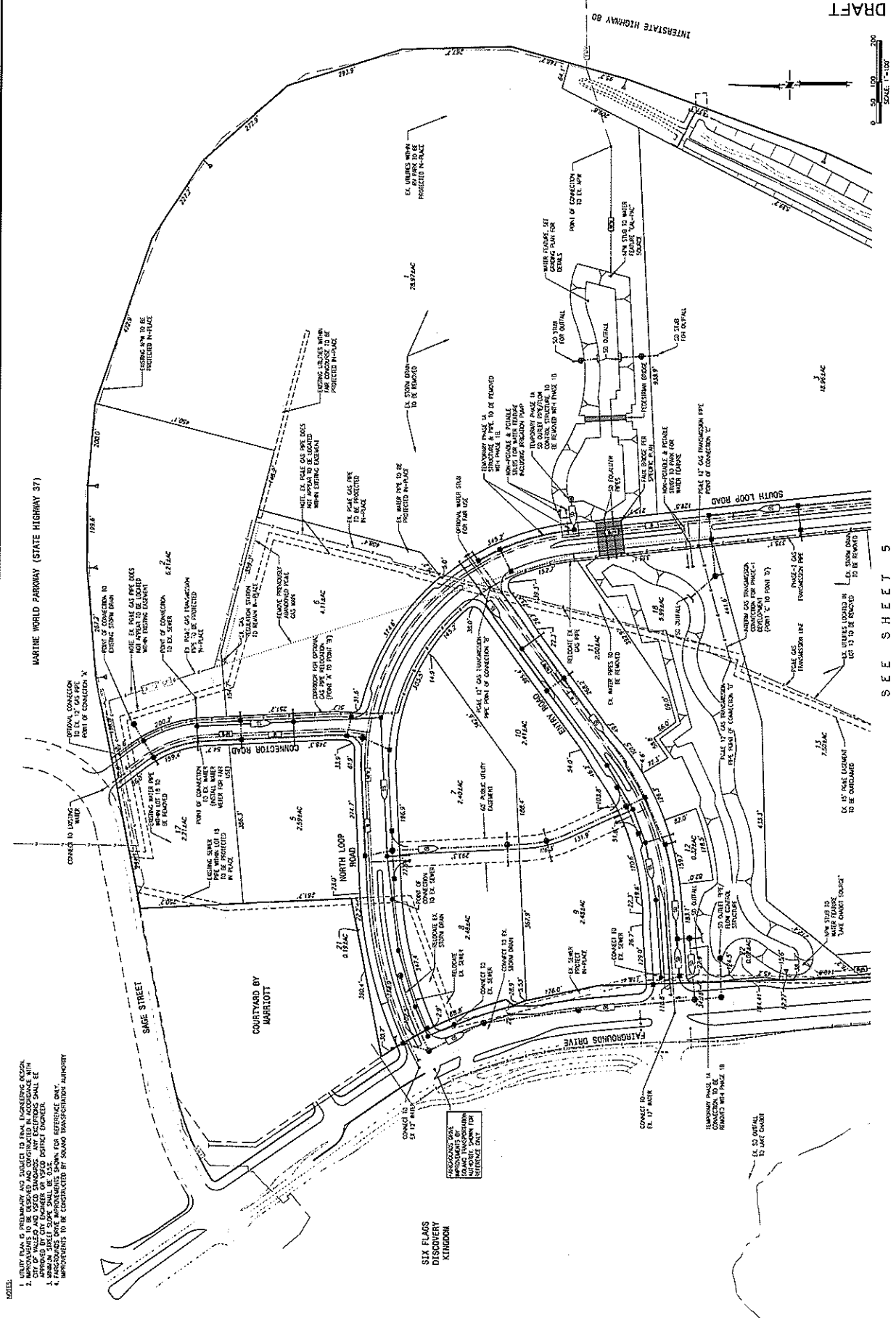
SOLANO 380
 3RD FLOOR PLAN
 19451.100

CITY OF WILKES

PLANS PREPARED UNDER THE DIRECTION OF
 JAMES P. TIBBETT, CIVIL ENGINEER
 ENGINEER, MARCH 1, 2014

Mackay & Somp
 1400 14th Street, Suite 100
 Berkeley, CA 94710
 PHONE: (415) 863-1111
 FAX: (415) 863-1112
 DATE: APRIL 2, 2013

NO.	DATE	DESCRIPTION
1	APRIL 2, 2013	ISSUE FOR PERMIT
2		
3		
4		
5		

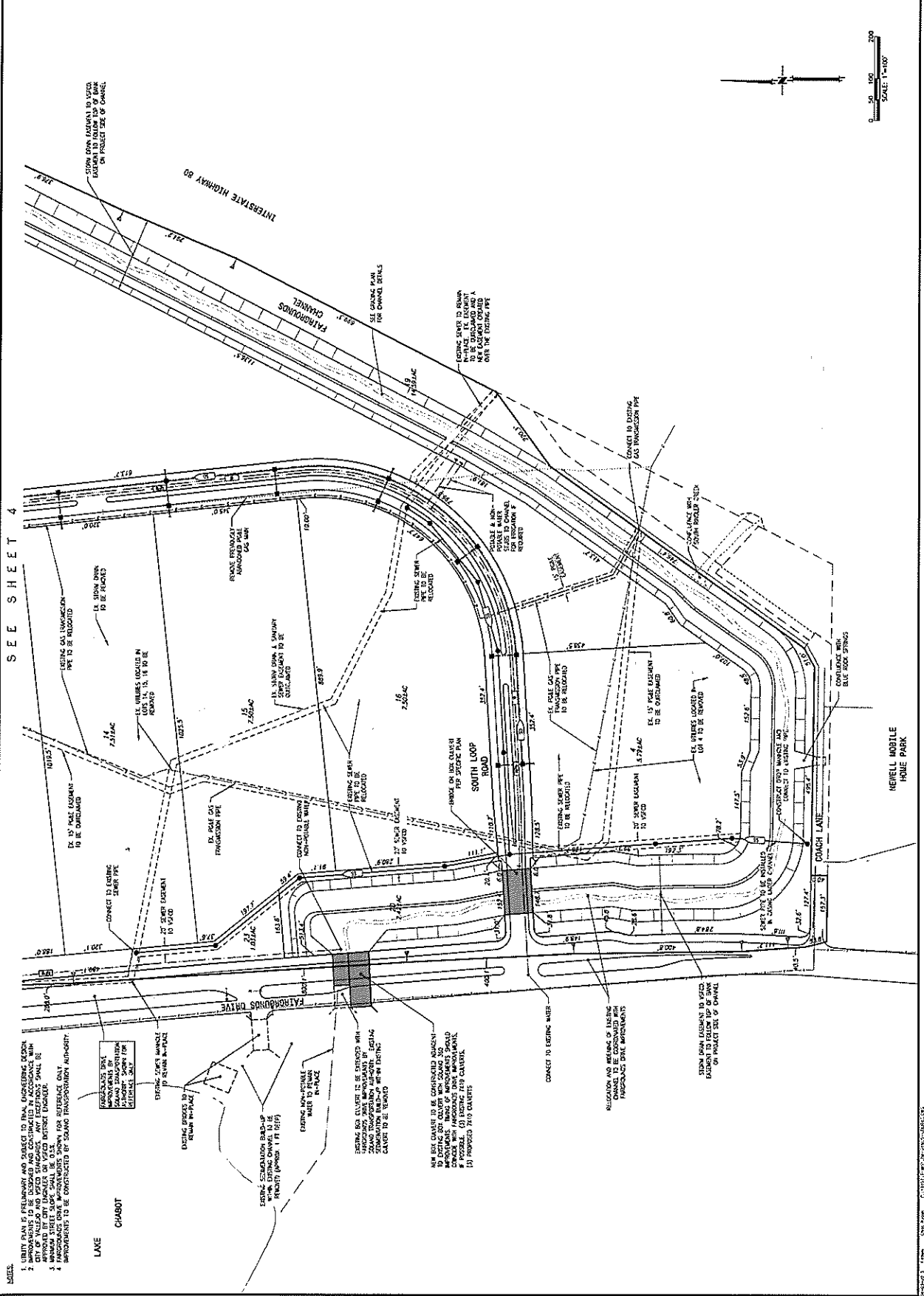


NOTES:
 1. THIS PLAN IS PRELIMINARY AND SUBJECT TO PERMITS FROM THE ENGINEERING DEPARTMENT.
 2. IMPROVEMENTS TO BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE CITY OF WILKES ENGINEERING DEPARTMENT SPECIFICATIONS.
 3. ALL STREET CLOSURES SHALL BE IN ACCORDANCE WITH THE CITY ENGINEER'S REQUIREMENTS.
 4. ALL IMPROVEMENTS TO BE CONSTRUCTED BY SOLANO TRANSPORTATION AUTHORITY.

SIX FLAGS
 DISCOVERY
 KINGDOM

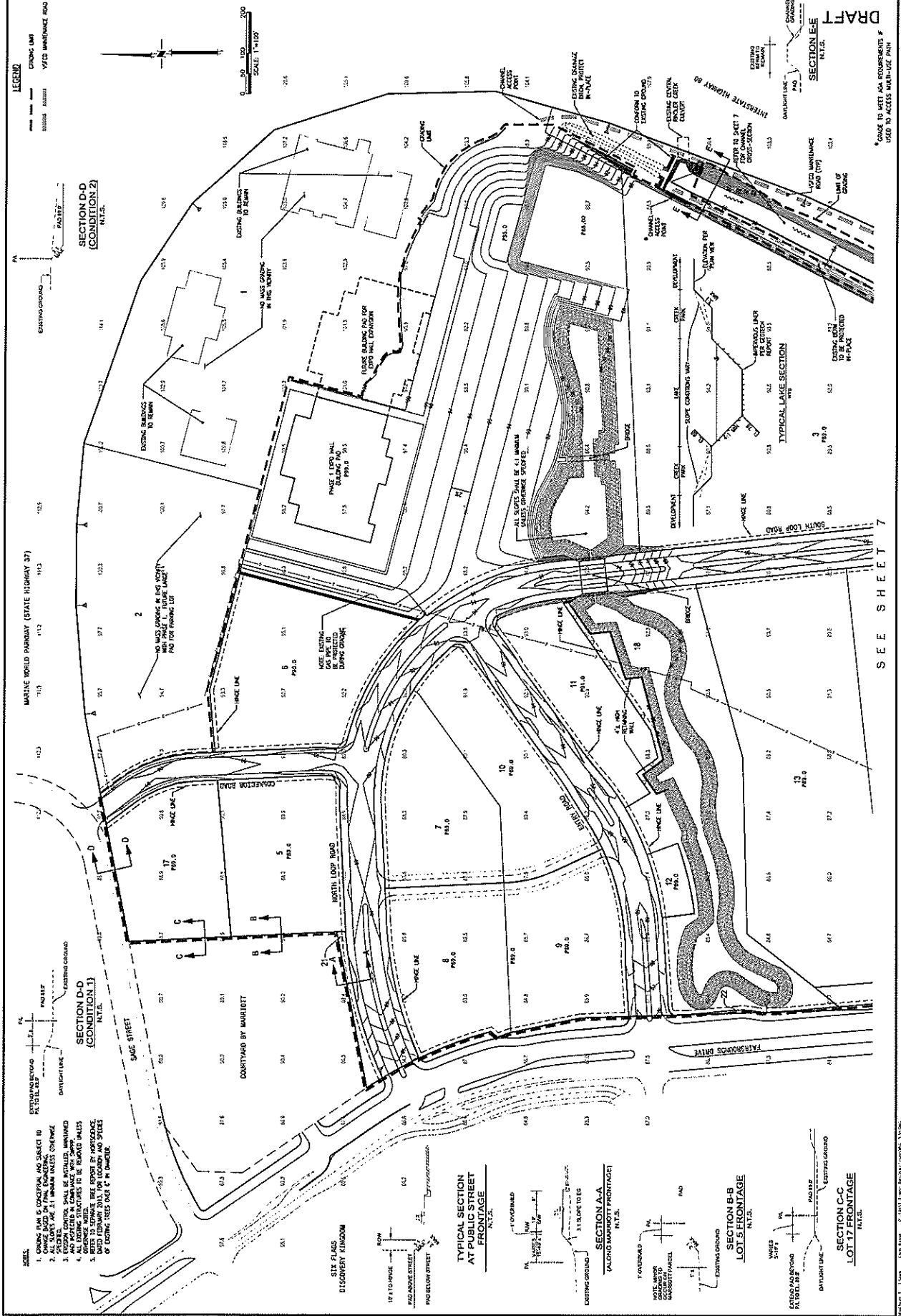
SEE SHEET 5

DRAFT



- NOTES:
1. ALL UTILITIES SHOWN ARE BASED ON THE RECORD DRAWINGS AND FIELD SURVEY. ANY DISCREPANCIES SHOULD BE CORRECTED AND RECORDED AS APPROPRIATE.
 2. ALL UTILITIES SHOWN ARE BASED ON THE RECORD DRAWINGS AND FIELD SURVEY. ANY DISCREPANCIES SHOULD BE CORRECTED AND RECORDED AS APPROPRIATE.
 3. ALL UTILITIES SHOWN ARE BASED ON THE RECORD DRAWINGS AND FIELD SURVEY. ANY DISCREPANCIES SHOULD BE CORRECTED AND RECORDED AS APPROPRIATE.
 4. ALL UTILITIES SHOWN ARE BASED ON THE RECORD DRAWINGS AND FIELD SURVEY. ANY DISCREPANCIES SHOULD BE CORRECTED AND RECORDED AS APPROPRIATE.

SEE SHEET 4



NOTES

1. ENGINEER HAS CONDUCTED VISUAL CHECK TO VERIFY ALL DIMENSIONS AND CORNER POSITIONS.
2. EXISTING DIMENSIONS SHALL BE MAINTAINED UNLESS OTHERWISE NOTED.
3. ALL DIMENSIONS SHALL BE IN FEET AND INCHES.
4. ALL DIMENSIONS SHALL BE TO THE CENTERLINE UNLESS OTHERWISE NOTED.
5. DIMENSIONS TO THE CENTERLINE OF THE ROAD AND STREETS SHALL BE TO THE CENTERLINE OF THE ROAD.

SECTION D-D (CONDITION 1)
N.T.S.

SECTION D-D (CONDITION 2)
N.T.S.

SECTION A-A
N.T.S.

SECTION B-B
N.T.S.

SECTION C-C
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION ALONG MARRIOTT FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
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TYPICAL SECTION AT PUBLIC STREET FRONTAGE
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TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

LEGEND

- EXISTING GROUND
- PROPOSED GROUND
- PROPOSED LOT
- PROPOSED ROAD
- PROPOSED SIDEWALK
- PROPOSED CURB
- PROPOSED DRAINAGE
- PROPOSED UTILITY
- PROPOSED FENCE
- PROPOSED SIGN
- PROPOSED LIGHT
- PROPOSED TREE
- PROPOSED PLANT
- PROPOSED LANDSCAPE
- PROPOSED HEDGING
- PROPOSED WALL
- PROPOSED RAILING
- PROPOSED DECK
- PROPOSED PORCH
- PROPOSED PATIO
- PROPOSED DRIVEWAY
- PROPOSED GARAGE
- PROPOSED PORCH
- PROPOSED PATIO
- PROPOSED DRIVEWAY
- PROPOSED GARAGE

SECTION D-D (CONDITION 1)
N.T.S.

SECTION D-D (CONDITION 2)
N.T.S.

SECTION A-A
N.T.S.

SECTION B-B
N.T.S.

SECTION C-C
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION ALONG MARRIOTT FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
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TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

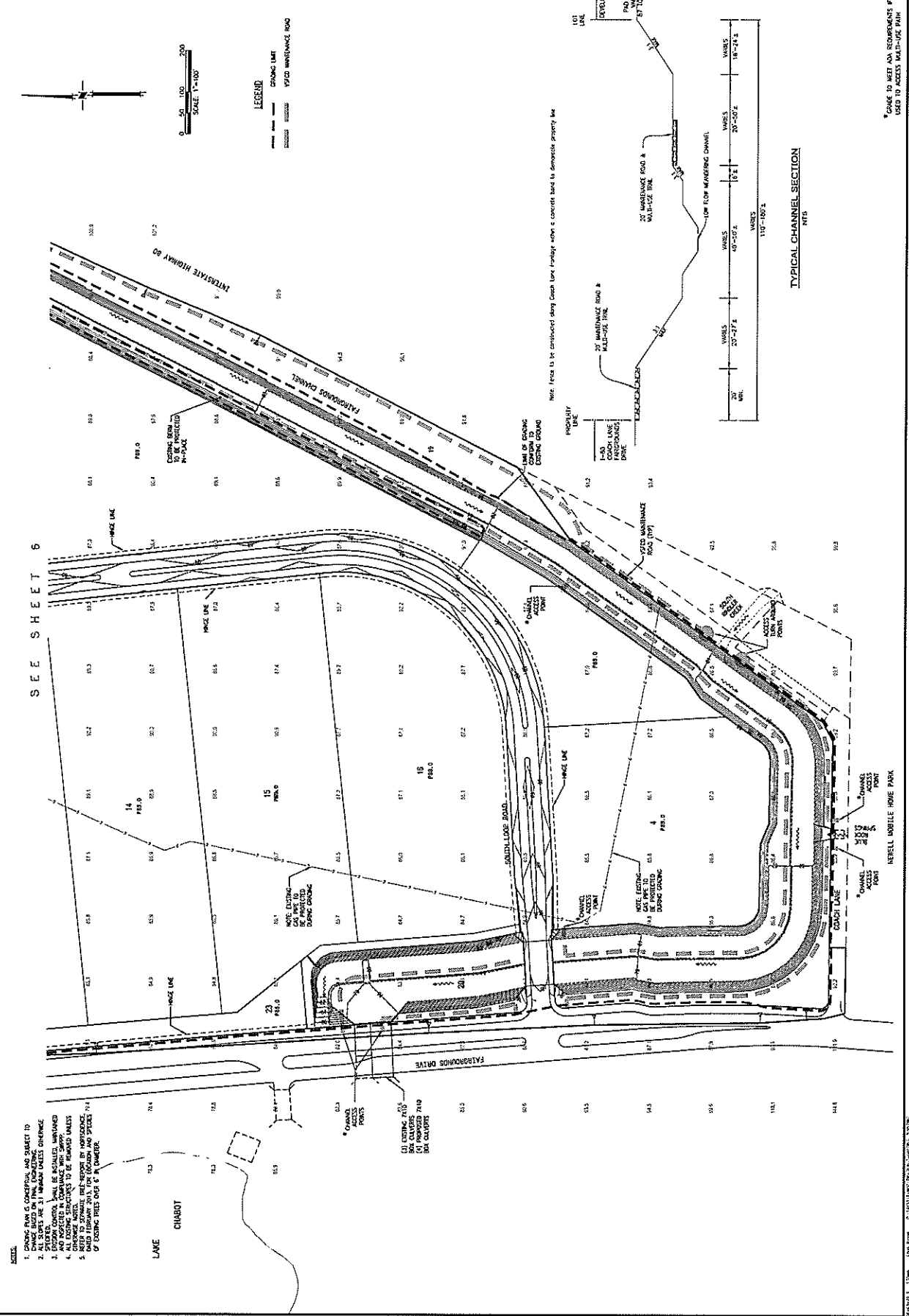
TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

TYPICAL SECTION AT PUBLIC STREET FRONTAGE
N.T.S.

SEE SHEET 7
 4. GRASS TO MEET ADA REQUIREMENTS & USED TO ACCESS MULTI-USE PATH

DRAFT

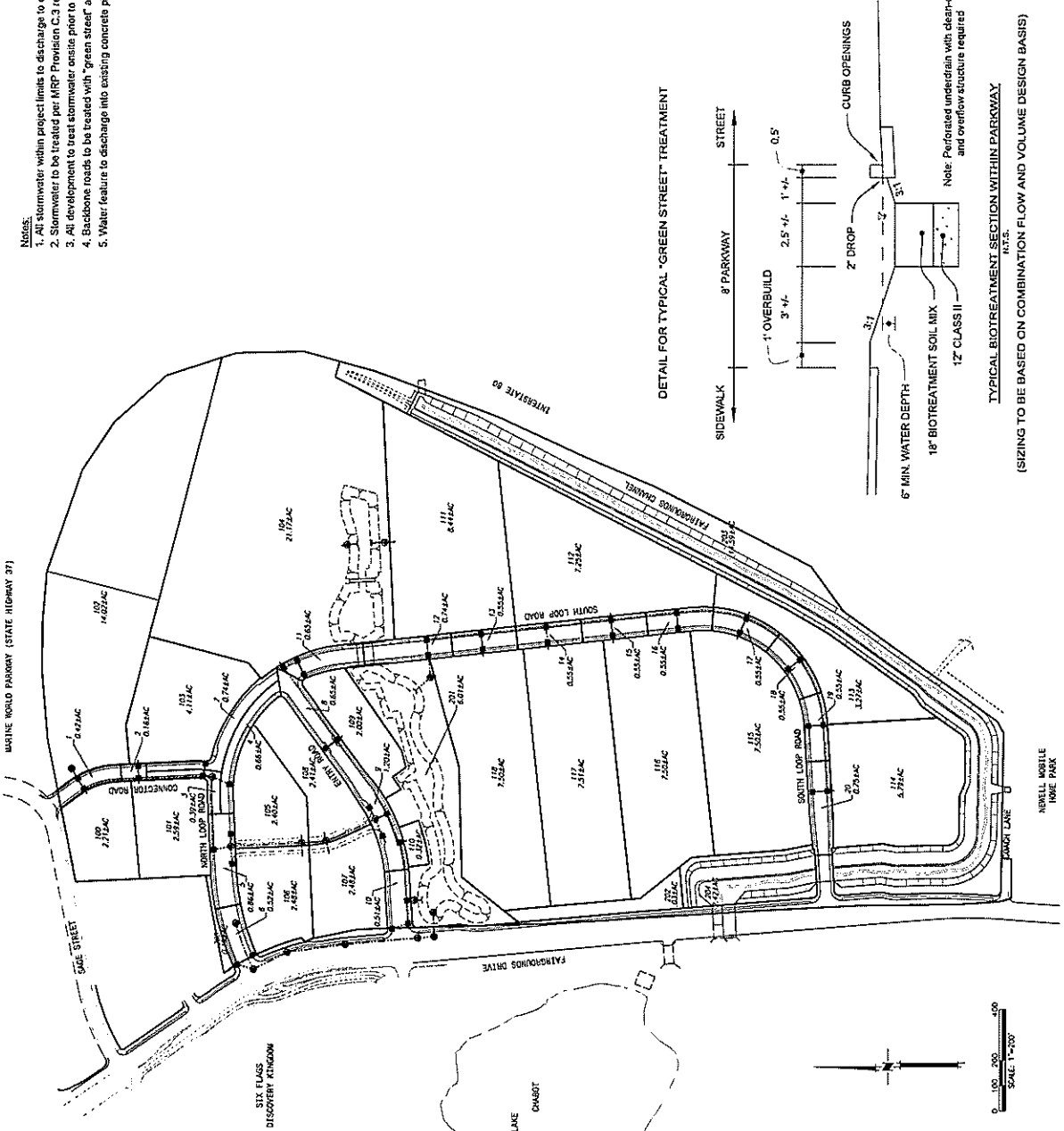
*GRADE TO MEET ADA REQUIREMENTS & USED TO ACCESS MULTI-USE PATH



- NOTES:
1. EXISTING BASIN 5 CONCEPTUAL AND SUBJECT TO 1. CHECK BASED ON FINAL DRAINAGE.
 2. EXISTING CHANNEL SHALL BE REGRADED, UNLESS OTHERWISE NOTED.
 3. EXISTING CHANNEL SHALL BE REGRADED, UNLESS OTHERWISE NOTED.
 4. ALL EXISTING STRUCTURES TO BE REPAIRED UNLESS OTHERWISE NOTED.
 5. REFER TO EXISTING SITE REPORT BY HORTON/CORREIA & ASSOCIATES, INC., DATED FEBRUARY 2013, FOR EXISTING AND STUDIED EXISTING FIELD DATA & INFORMATION.

- NOTES:
1. All stormwater within project limits to discharge to onsite "water feature"
 2. Stormwater to be treated per MRP Provision C.3 requirements prior to discharging into "water feature"
 3. All development to treat stormwater onsite prior to discharging into backbone storm drain system located in frontage roads
 4. Backbone roads to be treated with "green street" approach (i.e. bio-swale within landscape pathway)
 5. Water feature to discharge into existing concrete pipe located in Fairgrounds Drive

Plot #	Area	Treatment Within
1	0.22 ± AC.	Street - Bioretention in parkway strip
2	0.15 ± AC.	Street - Bioretention in parkway strip
3	0.29 ± AC.	Street - Bioretention in parkway strip
4	0.26 ± AC.	Street - Bioretention in parkway strip
5	0.25 ± AC.	Street - Bioretention in parkway strip
6	0.25 ± AC.	Street - Bioretention in parkway strip
7	0.74 ± AC.	Street - Bioretention in parkway strip
8	0.85 ± AC.	Street - Bioretention in curb ball-cubs & beaded curb
9	1.25 ± AC.	Street - Bioretention in curb ball-cubs & beaded curb
10	0.51 ± AC.	Street - Bioretention in parkway strip
11	0.83 ± AC.	Street - Bioretention in parkway strip
12	0.74 ± AC.	Street - Bioretention in parkway strip
13	0.13 ± AC.	Street - Bioretention in parkway strip
14	0.83 ± AC.	Street - Bioretention in parkway strip
15	0.28 ± AC.	Street - Bioretention in parkway strip
16	0.28 ± AC.	Street - Bioretention in parkway strip
17	0.28 ± AC.	Street - Bioretention in parkway strip
18	0.28 ± AC.	Street - Bioretention in parkway strip
19	0.28 ± AC.	Street - Bioretention in parkway strip
20	0.28 ± AC.	Street - Bioretention in parkway strip
21	0.28 ± AC.	Street - Bioretention in parkway strip
22	0.28 ± AC.	Street - Bioretention in parkway strip
23	0.28 ± AC.	Street - Bioretention in parkway strip
24	0.28 ± AC.	Street - Bioretention in parkway strip
25	0.28 ± AC.	Street - Bioretention in parkway strip
26	0.28 ± AC.	Street - Bioretention in parkway strip
27	0.28 ± AC.	Street - Bioretention in parkway strip
28	0.28 ± AC.	Street - Bioretention in parkway strip
29	0.28 ± AC.	Street - Bioretention in parkway strip
30	0.28 ± AC.	Street - Bioretention in parkway strip
31	0.28 ± AC.	Street - Bioretention in parkway strip
32	0.28 ± AC.	Street - Bioretention in parkway strip
33	0.28 ± AC.	Street - Bioretention in parkway strip
34	0.28 ± AC.	Street - Bioretention in parkway strip
35	0.28 ± AC.	Street - Bioretention in parkway strip
36	0.28 ± AC.	Street - Bioretention in parkway strip
37	0.28 ± AC.	Street - Bioretention in parkway strip
38	0.28 ± AC.	Street - Bioretention in parkway strip
39	0.28 ± AC.	Street - Bioretention in parkway strip
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124	0.28 ± AC.	Street - Bioretention in parkway strip



DETAIL FOR TYPICAL "GREEN STREET" TREATMENT

8' PARKWAY

STREET

SIDEWALK

1' OVERBUILD

2.5' +/-

1' +/-

CUREB OPENINGS

z DROP

6" MIN. WATER DEPTH

18" BIOTREATMENT SOIL MIX

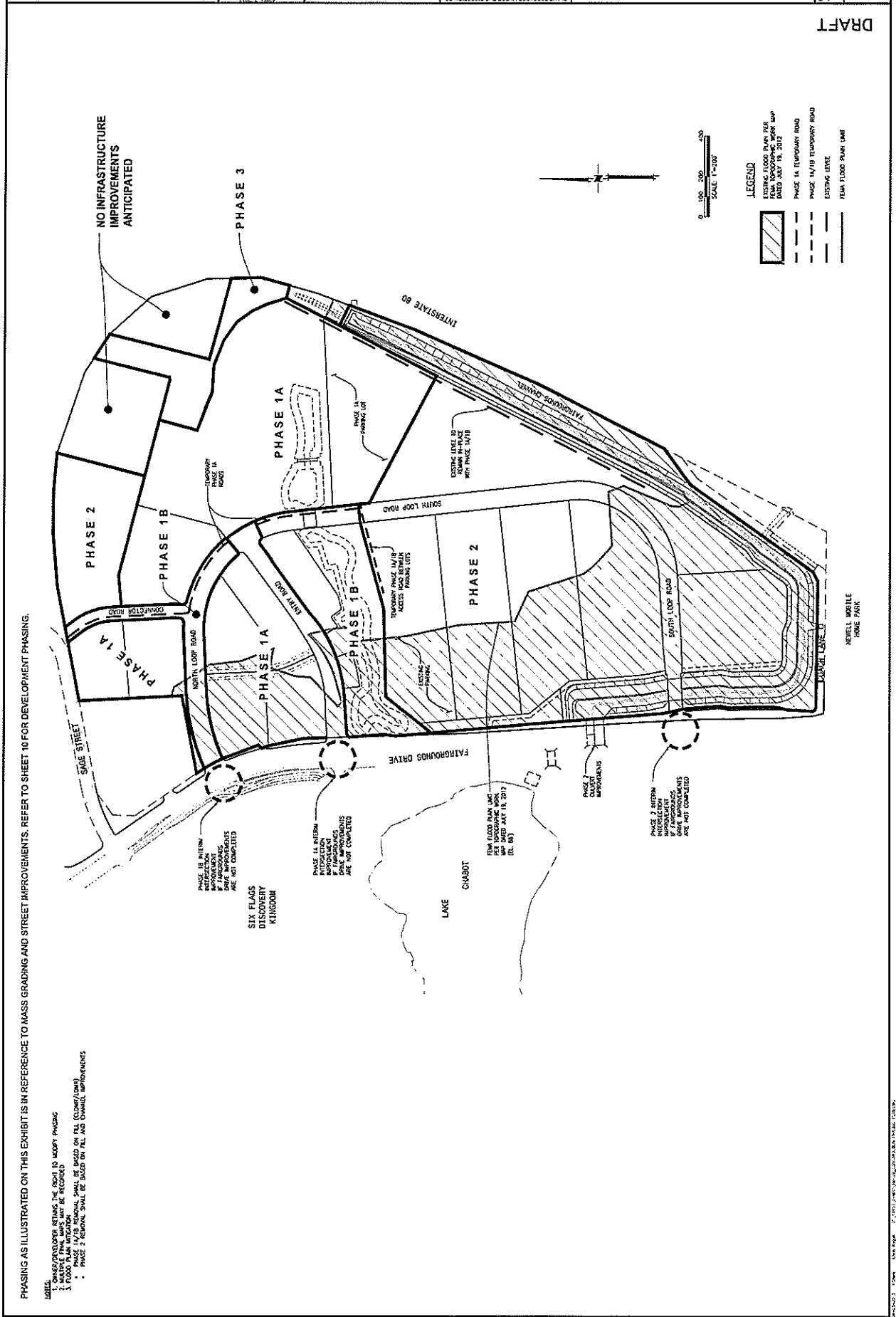
12" CLASS II

Note: Perforated underdrain with clean-out and overflow structure required

TYPICAL BIOTREATMENT SECTION WITHIN PARKWAY

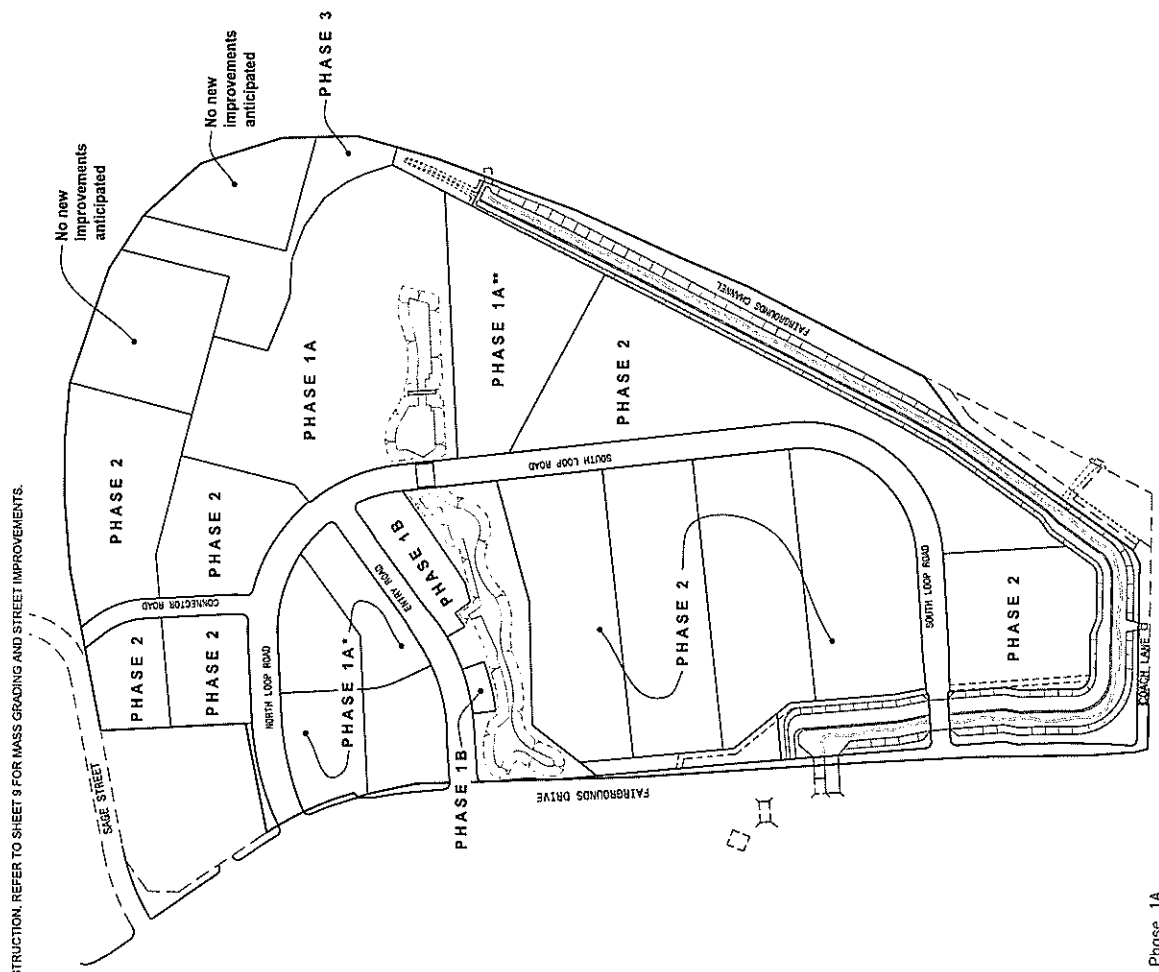
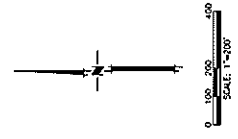
(SIZING TO BE BASED ON COMBINATION FLOW AND VOLUME DESIGN BASIS)





PHASING AS ILLUSTRATED ON THIS EXHIBIT IS IN REFERENCE TO MASS GRADING AND STREET IMPROVEMENTS. REFER TO SHEET 10 FOR DEVELOPMENT PHASING.
 NOTES:
 1. OWNERS/DEVELOPERS RETURNING THE ROAD TO MOBILE PHASING SHALL BE RESPONSIBLE FOR THE DESIGN AND RECORDING OF THE ROAD.
 2. FLOOD PLAIN LIMITATIONS SHALL BE BASED ON ALL (COMPLETION) FLOOD PLAIN LIMITATIONS.
 3. PHASE 1A/B IMPROVEMENTS SHALL BE BASED ON ALL (COMPLETION) FLOOD PLAIN LIMITATIONS.
 4. PHASE 2 IMPROVEMENTS SHALL BE BASED ON ALL (COMPLETION) FLOOD PLAIN LIMITATIONS.

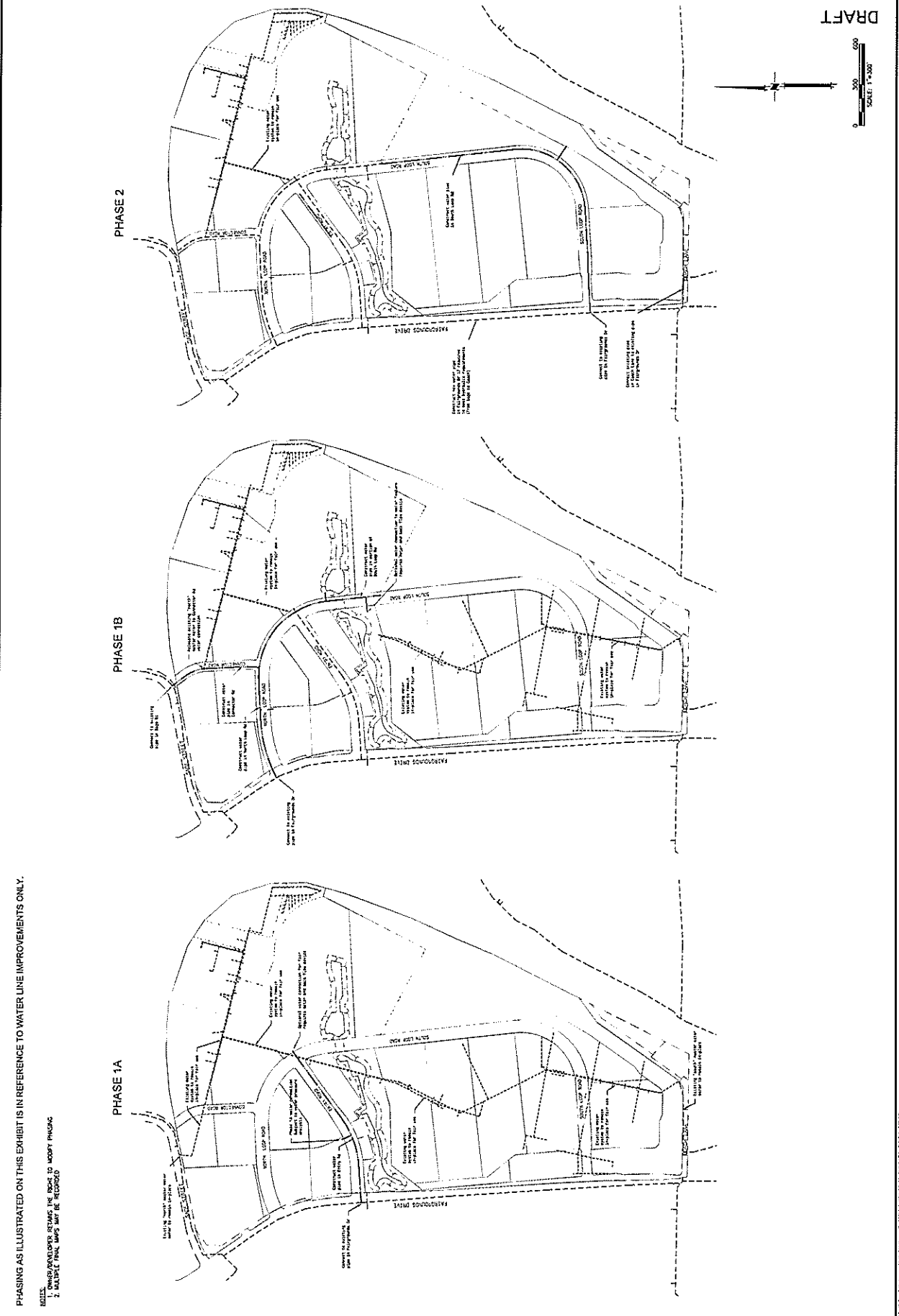
DRAFT



PHASING AS ILLUSTRATED ON THIS EXHIBIT IS IN REFERENCE TO DEVELOPMENT AND BUILDING CONSTRUCTION. REFER TO SHEET 9 FOR MASS GRADING AND STREET IMPROVEMENTS.

- NOTES:
1. GRASS/VEGETATION REMAINS THE SAME TO MOOPY PHASING
 2. INITIAL PLAN MAPS MAY BE RECORDED

* Access and utilities from Entry Road with Phase 1A
 ** Access to be provided by temporary roadway from Entry Road with Phase 1A
 Note: Development of select Phase 1 & 2 parcels may be intensified with Phase 3



PHASING AS ILLUSTRATED ON THIS EXHIBIT IS IN REFERENCE TO WATER LINE IMPROVEMENTS ONLY.

- NOTES:
1. DIMENSIONS/VALUES REMAINS THE SAME TO MODIFY PHASING
 2. MULTIPLE FINAL LINES MAY BE REQUIRED